

# MASSACHUSETTS DEPARTMENT OF TRANSPORTATION HIGHWAY DIVISION

## PLAN AND PROFILE OF BRUCE FREEMAN RAIL TRAIL

### IN THE TOWN OF SUDBURY & CONCORD MIDDLESEX COUNTY

FEDERAL AID PROJECT NO. XXX-XXXX(XXX)X

# 75% SUBMITTAL

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	1	318
PROJECT FILE NO.		608164	
TITLE SHEET & INDEX			

THE COMMONWEALTH OF MASSACHUSETTS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES 2021 EDITION, THE OCTOBER 2017 CONSTRUCTION STANDARD DETAILS, THE 2015 OVERHEAD SIGNAL STRUCTURE AND FOUNDATION STANDARD DRAWINGS, MASSDOT TRAFFIC MANAGEMENT PLANS AND DETAIL DRAWINGS, THE LATEST MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS WITH MASSACHUSETTS AMENDMENTS, THE 1990 STANDARD DRAWINGS FOR SIGNS AND SUPPORTS, THE 1968 STANDARD DRAWINGS FOR TRAFFIC SIGNALS AND HIGHWAY LIGHTING, AND THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, WILL GOVERN.

#### SHARED-USE PATH DESIGN DESIGNATION

##### BRUCE FREEMAN RAIL TRAIL

DESIGN SPEED	18 MPH
FUNCTIONAL CLASSIFICATION	OFF ROAD PEDESTRIAN/BICYCLE FACILITY
* 18 MPH FOR GRADES LESS THAN 2%, 20 MPH FOR GRADES BETWEEN 2% & 4%	

#### DESIGN DESIGNATION (HUDSON RD RTE 27)

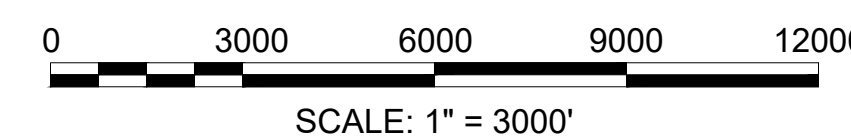
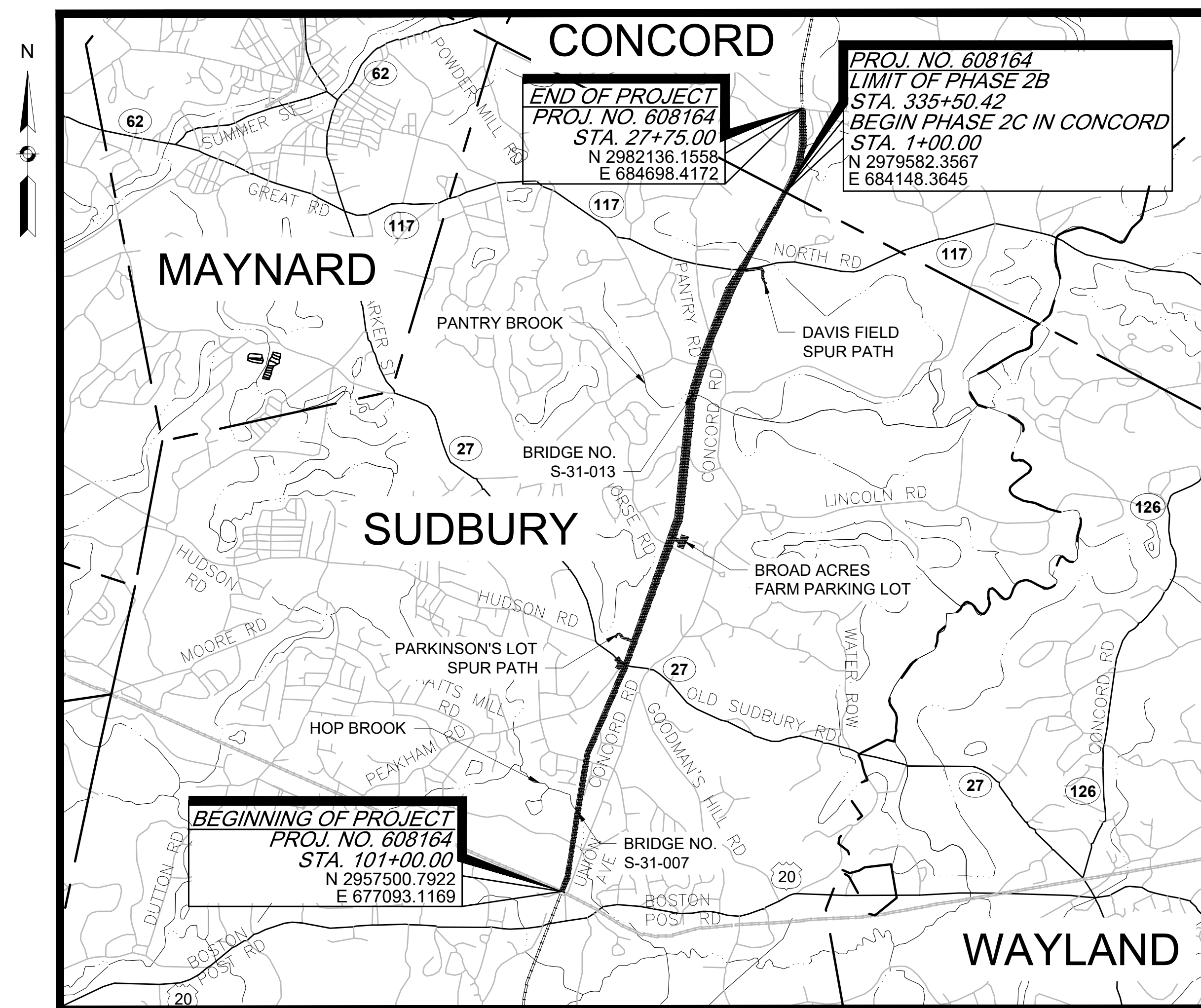
DESIGN SPEED	30 MPH
ADT (2021)	20,404 vpd
ADT (2041)	20,609 vpd
T (PEAK HOUR)	2.6%
T (AVERAGE DAY)	4.7%
FUNCTIONAL CLASSIFICATION	URBAN PRINCIPAL ARTERIAL (EB/WB)

#### DESIGN DESIGNATION (NORTH RD)

DESIGN SPEED	40 MPH
ADT (2021)	12654 vpd
ADT (2041)	12780 vpd
T (PEAK HOUR)	2.9%
T (AVERAGE DAY)	3.2%
FUNCTIONAL CLASSIFICATION	URBAN MINOR ARTERIAL (EB/WB)

#### INDEX

SHEET NO.	DESCRIPTION
1	TITLE SHEET & INDEX
2	GENERAL NOTES
3	LEGEND & ABBREVIATIONS
4-5	KEY PLAN & BORING LOCATIONS
6-8	BORING LOGS
9-13	TYPICAL SECTIONS
14-48	CONSTRUCTION PLANS
49-63	PROFILES
64-95	CURB & BASELINE TIE PLANS
96-124	GRADING PLANS
125-156	PAVEMENT MARKING & SIGNING PLANS
157-159	TRAFFIC SIGN SUMMARY
160-165	TRAFFIC SIGNAL PLANS
166-171	TEMPORARY TRAFFIC CONTROL PLANS
172-203	DRAINAGE & UTILITY PLANS
204	WETLAND REPLICATION PLAN
205-218	CONSTRUCTION DETAILS
219-220	WHEELCHAIR RAMP DETAILS
221-242	BRIDGE PLANS
243-318	CROSS SECTIONS



LENGTH OF PHASE 2B IN SUDBURY = 23,449.63 FEET = 4.441 MILES  
LENGTH OF PHASE 2C IN CONCORD = 2,675.00 FEET = 0.507 MILES

TOWN OF SUDBURY DESIGN CONSULTANT:		
	<b>FUSS &amp; O'NEILL</b>	
190 HIGH STREET BOSTON, MA 02110 800.286.2469 www.fando.com		RECOMMENDED FOR APPROVAL
TOWN OF CONCORD DESIGN CONSULTANT:		
	<b>GPI</b>	CHIEF ENGINEER _____ DATE _____
Engineering Design Planning Construction Management 978.570.2999 GPINET.COM Greenman-Pedersen, Inc. 181 Ballardvale Street, Suite 202 Wilmington, MA 01887		DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION
APPROVED:		
DIVISION ADMINISTRATOR _____		DATE _____

**GENERAL ABBREVIATIONS**

ABAN	ABANDON
ADJ	ADJUST
APPROX	APPROXIMATE
A.C.	ASPHALT CONCRETE
ACCM PIPE	ASPHALT COATED CORRUGATED METAL PIPE
BIT.	BITUMINOUS
BC	BOTTOM OF CURB
BD.	BOUND
BL	BASELINE
BLDG	BUILDING
BM	BENCHMARK
BO	BY OTHERS
BOS	BOTTOM OF SLOPE
BR.	BRIDGE
CC	CEMENT CONCRETE
CCM	CEMENT CONCRETE MASONRY
CEM	CEMENT
CI	CURB INLET
CLF	CHAIN LINK FENCE
CL	CENTERLINE
CO.	COUNTY
CONC	CONCRETE
CONT	CONTINUOUS / CONTINUED
CONST	CONSTRUCTION
CR GR	CROWN GRADE
DIA	DIAMETER
DWY	DRIVEWAY
ELEV (or EL.)	ELEVATION
EMB	EMBANKMENT
EOP	EDGE OF PAVEMENT
EQ	EQUAL
EXIST (or EX)	EXISTING
EXC	EXCAVATION
FDN.	FOUNDATION
FDP	FULL DEPTH PAVEMENT
FLDSTN	FIELDSTONE
GAR	GARAGE
GD	GROUND
GRAN	GRANITE
GRAV	GRAVEL
GRD	GUARD
HMA	HOT MIX ASPHALT
HOR	HORIZONTAL
HWY	HIGHWAY
JCT	JUNCTION
LOAM	LOAM BORROW
LSA	LANDSCAPED AREA
LT	LEFT
MAHWL	MEAN AVERAGE HIGH WATER LINE
MAX	MAXIMUM
MB	MAILBOX
MHB	MASSACHUSETTS HIGHWAY BOUND
MIN	MINIMUM
MOD	MODIFIED
MSE	MECHANICALLY STABILIZED EARTH
NERR	NEW ENGLAND RAILROAD
NIC	NOT IN CONTRACT
NO.	NUMBER
NTS	NOT TO SCALE
O.C.	ON CENTER
O.D.	OUTSIDE DIAMETER
P.G.L.	PROFILE GRADE LINE
PREV	PREVIOUS/PREVIOUSLY
PROJ	PROJECT
PROP	PROPOSED
PSB	PLANTABLE SOIL BORROW
PVMT	PAVEMENT
R&D	REMOVE AND DISCARD
R&R	REMOVE AND RESET
R&S	REMOVE AND STACK
RD	ROAD
RDWY	ROADWAY
REB	REBUILD
REM	REMOVE
REMOD	REMODEL
RET	RETAIN
RET WALL	RETAINING WALL
ROW	RIGHT OF WAY
RR	RAILROAD
RT	RIGHT
SB	STONE BOUND
SHLD	SHOULDER
SHLO/S.H.L.O.	STATE HIGHWAY LAYOUT LINE

**GENERAL ABBREVIATIONS (CONT)**

ST	STREET
STA	STATION
STD	STANDARD
SW	SIDEWALK
TEMP	TEMPORARY
TC	TOP OF CURB
TOS	TOP OF SLOPE
TRANS	TRANSITION
TRM	TURF REINFORCING MAT
TYP	TYPICAL
VAR	VARIES
VERT	VERTICAL
WCR	WHEEL CHAIR RAMP
WP	WORKING POINT
X-SECT	CROSS SECTION

**UTILITY ABBREVIATIONS**

CB	CATCH BASIN
CBCI	CATCH BASIN WITH CURB INLET
CIP	CAST IRON PIPE
CIT	CHANGE IN TYPE
CMP	CORRUGATED METAL PIPE
CSP	CORRUGATED STEEL PIPE
DI	DROP INLET
DIP	DUCTILE IRON PIPE
FES	FLARED END SECTION
F&C	FRAME AND COVER
F&G	FRAME AND GRATE
GG	GAS GATE
GI	GUTTER INLET
GIP	GALVANIZED IRON PIPE
HDPE	HIGH DENSITY POLYETHYLENE PIPE
HDW	HEADWALL
HYD	HYDRANT
INV	INVERT
LB	LEACH BASIN
LP	LIGHT POLE
MH	MANHOLE
MW	MONITORING WELL
OHW	OVERHEAD WIRE
PVC	POLYVINYLCHLORIDE PIPE
PWW	PAVED WATER WAY
RCP	REINFORCED CONCRETE PIPE
SMH	SEWER MANHOLE
TSV&B	TAPPING SLEEVE VALVE & BOX
UP	UTILITY POLE
WG	WATER GATE
WIP	WROUGHT IRON PIPE
WM	WATER METER/WATER MAIN

**ALIGNMENT & GRADING ABBREVIATIONS**

CC	CENTER OF CURVE
HP	HIGH POINT
I.T.	INTERSECTION OF TANGENT
LP	LOW POINT
PC	POINT OF CURVATURE
PCC	POINT OF COMPOUND CURVATURE
PI	POINT OF INTERSECTION
PNT	POINT
POC	POINT ON CURVE
POT	POINT ON TANGENT
PRC	POINT OF REVERSE CURVATURE
PT	POINT OF TANGENCY
∠PT	ANGLE POINT
R	RADIUS OF CURVATURE
T	TANGENT DISTANCE OF CURVE
TAN	TANGENT
25.45	SPOT ELEVATION

**PROFILE ABBREVIATIONS**

AD	ALGEBRAIC DIFFERENCE IN RATES OF GRADE
HSD	HORIZONTAL SIGHT DISTANCE
K	RATE OF VERTICAL CURVATURE
L	LENGTH OF CURVE
PVC	POINT OF VERTICAL CURVATURE
PVCC	POINT OF VERTICAL COMPOUND CURVATURE
PVI	POINT OF VERTICAL INTERSECTION
PVRC	POINT OF VERTICAL REVERSE CURVATURE
PVT	POINT OF VERTICAL TANGENCY
SSD	STOPPING SIGHT DISTANCE
VC	VERTICAL CURVE

**TRAFFIC SIGNAL**

CAB.	CABINET
CCVE	CLOSED CIRCUIT VIDEO EQUIPMENT
DW	STEADY DON'T WALK
FDW	FLASHING DON'T WALK
FR	FLASHING CIRCULAR RED
← FR →	FLASHING RED LEFT ARROW
→ FR →	FLASHING RED RIGHT ARROW
FY	FLASHING CIRCULAR YELLOW
← FY →	FLASHING YELLOW LEFT ARROW
→ FY →	FLASHING YELLOW RIGHT ARROW
G	STEADY CIRCULAR GREEN
← G →	STEADY GREEN LEFT ARROW
→ G →	STEADY GREEN RIGHT ARROW
GSL	STEADY GREEN SLASH LEFT ARROW
GSR	STEADY GREEN SLASH RIGHT ARROW
↑ G ↓	STEADY GREEN VERTICAL ARROW
OL	OVERLAP
PED	PEDESTRIAN
PTZ	PAN, TILT, ZOOM
R	STEADY CIRCULAR RED
← R →	STEADY RED LEFT ARROW
→ R →	STEADY RED RIGHT ARROW
TR SIG	TRAFFIC SIGNAL
TSC	TRAFFIC SIGNAL CONDUIT
W	STEADY WALK
Y	STEADY CIRCULAR YELLOW
← Y →	STEADY YELLOW LEFT ARROW
→ Y →	STEADY YELLOW RIGHT ARROW

SUBBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	2	318
PROJECT FILE NO.		608164	

**GENERAL NOTES**

**GENERAL NOTES:**

- EXISTING CONDITIONS AND TOPOGRAPHICAL INFORMATION FROM AN ACTUAL FIELD SURVEY CONDUCTED BY VHB, INC. IN DECEMBER 2015 THROUGH APRIL 2016. SUPPLEMENTARY FIELD SURVEY WAS CONDUCTED BY GCG ASSOCIATES FROM NOVEMBER 2020 THROUGH DECEMBER 2020.
- THE HORIZONTAL CONTROL IS BASED ON THE MASSACHUSETTS MAINLAND STATE PLANE COORDINATE SYSTEM AND THE NATIONAL GEODETIC SURVEY (NAD83). ALL ELEVATION IS US FEET, REFERENCED TO THE NORTH AMERICA VERTICAL DATUM OF 1988 (NAVD88).
- THE CONTRACTOR SHALL VERIFY ALL EXISTING DIMENSIONS AND GRADES IN THE FIELD BEFORE COMMENCING WORK AND PROMPTLY NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.
- DRAINAGE ELEVATIONS ARE PROVIDED FOR DESIGN PURPOSES ONLY. THE CONTRACTOR SHALL VERIFY BY TEST PIT, THE LOCATIONS OF EXISTING UTILITIES WHICH MAY CONFLICT WITH THE PROPOSED DRAINAGE DESIGN. ANY FIELD ADJUSTMENTS REQUIRED WILL BE MADE AS APPROVED OR DIRECTED BY THE ENGINEER. ONLY AFTER THE CONTRACTOR VERIFIES ELEVATIONS FOR THE CONSTRUCTABILITY OF THE DRAINAGE SYSTEM SHALL ANY STRUCTURES BE ORDERED. ANY FIELD ADJUSTMENTS TO LINE & GRADE UP TO A DEPTH OF 5' SHALL BE INCLUDED IN THE COST OF THE PIPE. PIPE EXCAVATION GREATER THAN 5' WILL BE PAID UNDER CLASS B TRENCH EXCAVATION.
- THE CONTRACTOR SHALL VERIFY BY TEST PIT, THE LOCATIONS OF EXISTING UTILITIES WHICH MAY CONFLICT WITH PROPOSED CONDUIT AND SIGNAL EQUIPMENT. ANY FIELD ADJUSTMENTS REQUIRED WILL BE MADE AS APPROVED OR DIRECTED BY THE ENGINEER.
- WHERE AN EXISTING UTILITY IS FOUND TO CONFLICT WITH THE PROPOSED WORK, THE LOCATION, ELEVATION AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED WITHOUT DELAY BY THE CONTRACTOR, AND THE INFORMATION FURNISHED TO THE ENGINEER FOR RESOLUTION OF THE CONFLICT.
- THE CONTRACTOR SHALL ALTER THE MASONRY OF THE TOP SECTION OF ALL EXISTING DRAINAGE AND SEWER STRUCTURES AS NECESSARY FOR CHANGES IN GRADE, AND RESET ALL WATER AND DRAINAGE FRAMES, GRATES AND BOXES TO THE PROPOSED FINISH SURFACE GRADE. REQUIRED NEW MASONRY SHALL BE CLAY BRICK.
- THE CONTRACTOR SHALL MAKE ALL ARRANGEMENTS FOR THE ALTERATION AND ADJUSTMENT OF GAS, ELECTRIC, TELEPHONE AND ANY OTHER PRIVATE UTILITIES BY THE UTILITY COMPANIES.
- EXISTING UTILITY POLES WILL BE RELOCATED BY OTHERS IF REQUIRED.
- PRIOR TO ONSET OF TREE REMOVAL ACTIVITIES, THE CONTRACTOR, RESIDENT ENGINEER AND MASSDOT LANDSCAPE ARCHITECT AND TOWN REPRESENTATIVE SHALL WALK SITE TO IDENTIFY TREES TO BE REMOVED.
- AREAS OUTSIDE THE LIMITS OF PROPOSED WORK DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO THEIR ORIGINAL CONDITION AT NO EXPENSE TO THE OWNER.
- THE TERM "PROPOSED" (PROP) MEANS WORK TO BE CONSTRUCTED USING NEW MATERIALS OR, WHERE APPLICABLE, RE-USING EXISTING MATERIALS IDENTIFIED AS "REMOVE AND RESET" (R&R).
- JOINTS BETWEEN NEW ASPHALT CONCRETE ROADWAY PAVEMENT AND SAWCUT EXISTING PAVEMENT SHALL BE SEALED WITH BITUMEN AND BACKSANDDED.
- AFTER MILLING OPERATIONS AND PRIOR TO PAVING THE SUPERPAVE INTERMEDIATE OR SURFACES COURSES THE ENGINEER SHALL EVALUATE THE MILLED SURFACE AND SHALL APPLY THE APPROPRIATE REPAIR METHOD IF REQUIRED.
- EXISTING SIGNS WITHIN THE PROJECT LIMITS SHALL BE RETAINED UNLESS INDICATED OTHERWISE ON THE DRAWINGS.
- IF SUITABLE, ALL EXISTING GRANITE CURB & EDGING SHALL BE RE-USED IN THE PROPOSED WORK, EXCEPT CURVED STONES OF A DIFFERENT RADIUS THAN PROPOSED CURB.
- ALL PROPOSED HOT MIX ASPHALT CURB SHALL BE MASSDOT TYPE 3.
- ALL EXISTING STATE, COUNTY, CITY, AND TOWN LOCATION LINES AND PRIVATE PROPERTY LINES HAVE BEEN ESTABLISHED FROM AVAILABLE INFORMATION AND THEIR EXACT LOCATIONS ARE NOT GUARANTEED.
- ALL PROPOSED BOUNDS SHALL BE PLACED BY A LICENSED PROFESSIONAL SURVEYOR. THE CONTRACTOR SHALL EXERCISE DUE CARE WHEN WORKING AROUND ALL PROPERTY BOUNDS WHICH ARE TO REMAIN. SHOULD ANY DAMAGE TO A BOUND RESULT FROM THE ACTIONS OF THE CONTRACTOR, THE CONTRACTOR SHALL HAVE THE BOUND REPLACED AND/OR REALIGNED BY A LICENSED PROFESSIONAL SURVEYOR AS DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST.
- DISPOSAL OF ALL SURPLUS MATERIAL SHALL BE AS APPROVED BY THE ENGINEER AND OWNER.
- LATERAL DRAIN PIPES SHALL BE INSTALLED WITH A PITCH OF 0.01 FOOT PER FOOT (MINIMUM) UNLESS NOTED OTHERWISE ON THE PLANS.

**GENERAL SYMBOLS**

EXISTING	PROPOSED	DESCRIPTION
		JERSEY BARRIER
		CATCH BASIN
		CATCH BASIN CURB INLET
		FLAG POLE
		GAS PUMP
		MAIL BOX
		POST SQUARE
		POST CIRCULAR
		WELL
		ELECTRIC HANDHOLE
		FENCE GATE POST
		GAS GATE
		BORING HOLE
		MONITORING WELL
		TEST PIT
		HYDRANT
		LIGHT POLE
		COUNTY BOUND
		GPS POINT
		CABLE MANHOLE
		DRAINAGE MANHOLE
		ELECTRIC MANHOLE
		GAS MANHOLE
		MISC MANHOLE
		SEWER MANHOLE
		TELEPHONE MANHOLE
		WATER MANHOLE
		MASSACHUSETTS HIGHWAY BOUND
		MONUMENT
		STONE BOUND
		TOWN OR CITY BOUND
		TRAVERSE OR TRIANGULATION STATION
		TROLLEY POLE OR GUY POLE
		TRANSMISSION POLE
		UTILITY POLE W/ FIREBOX
		UTILITY POLE WITH DOUBLE LIGHT
		UTILITY POLE W / 1 LIGHT
		UTILITY POLE
		BUSH
		TREE
		STUMP
		SWAMP / MARSH
		WATER GATE
		PARKING METER
		OVERHEAD CABLE/WIRE
		CURBING
		CONTOURS (ON-THE-GROUND SURVEY DATA)
		CONTOURS (PHOTOGRAMMETRIC DATA)
		UNDERGROUND DRAIN PIPE (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND ELECTRIC DUCT (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND GAS MAIN (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND SEWER MAIN (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND TELEPHONE DUCT (DOUBLE LINE 24 INCH AND OVER)
		UNDERGROUND WATER MAIN (DOUBLE LINE 24 INCH AND OVER)
		BALANCED STONE WALL
		GUARD RAIL - STEEL POSTS
		GUARD RAIL - WOOD POSTS
		CHAIN LINK OR METAL FENCE
		WOOD FENCE
		EROSION CONTROL
		DRAINAGE SWALE
		TREE LINE
		SAWCUT LINE
		TOP OR BOTTOM OF SLOPE
		LIMIT OF EDGE OF PAVEMENT OR COLD PLANE AND OVERLAY
		BANK OF RIVER OR STREAM
		BORDER OF WETLAND
		100 FT WETLAND BUFFER
		200 FT RIVERFRONT BUFFER
		STATE HIGHWAY LAYOUT
		TOWN OR CITY LAYOUT
		COUNTY LAYOUT
		RAILROAD SIDELINE
		TOWN OR CITY BOUNDARY LINE
		PROPERTY LINE OR APPROXIMATE PROPERTY LINE
		EASEMENT

**TRAFFIC SYMBOLS**

EXISTING	PROPOSED	DESCRIPTION
		CONTROLLER PHASE ACTUATED
		TRAFFIC SIGNAL HEAD (SIZE AS NOTED)
		WIRE LOOP DETECTOR (6' x 6' TYP UNLESS OTHERWISE SPECIFIED)
		VIDEO DETECTION CAMERA
		MICROWAVE DETECTOR
		PEDESTRIAN PUSH BUTTON, SIGN (DIRECTIONAL ARROW AS SHOWN) AND SADDLE
		EMERGENCY PREEMPTION CONFIRMATION STROBE LIGHT
		VEHICULAR SIGNAL HEAD
		VEHICULAR SIGNAL HEAD, OPTICALLY PROGRAMMED
		FLASHING BEACON
		PEDESTRIAN SIGNAL HEAD, (TYPE AS NOTED OR AS SPECIFIED)
		RAILROAD SIGNAL
		SIGNAL POST AND BASE (ALPHA-NUMERIC DESIGNATION NOTED)
		MAST ARM, SHAFT AND BASE (ARM LENGTH AS NOTED)
		HIGH MAST POLE OR TOWER
		SIGN AND POST
		SIGN AND POST (2 POSTS)
		MAST ARM WITH LUMINAIRE
		OPTICAL PRE-EMPTION DETECTOR
		CONTROL CABINET, GROUND MOUNTED
		CONTROL CABINET, POLE MOUNTED
		FLASHING BEACON CONTROL AND METER PEDESTAL
		LOAD CENTER ASSEMBLY
		PULL BOX 12"x12" (OR AS NOTED)
		ELECTRIC HANDHOLE 12"x24" (OR AS NOTED)
		TRAFFIC SIGNAL CONDUIT

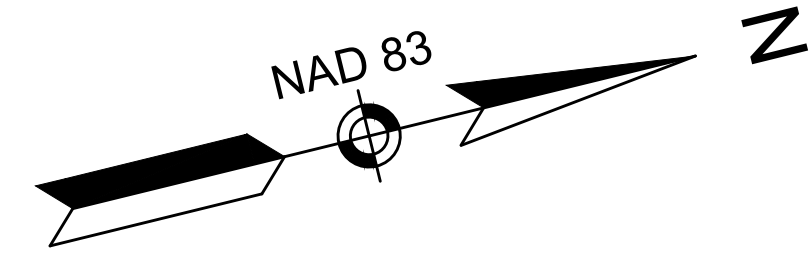
**PAVEMENT MARKINGS SYMBOLS**

EXISTING	PROPOSED	DESCRIPTION
		PAVEMENT ARROW - WHITE
		LEGEND "ONLY" - WHITE
		STOP LINE
		CROSSWALK
		SOLID WHITE LINE
		SOLID YELLOW LINE
		BROKEN WHITE LINE
		BROKEN YELLOW LINE
		DOTTED WHITE LINE
		DOTTED YELLOW LINE
		DOTTED WHITE LINE EXTENSION
		DOTTED YELLOW LINE EXTENSION
		DOUBLE WHITE LINE
		DOUBLE YELLOW LINE
		LONG DASHED YELLOW LINE

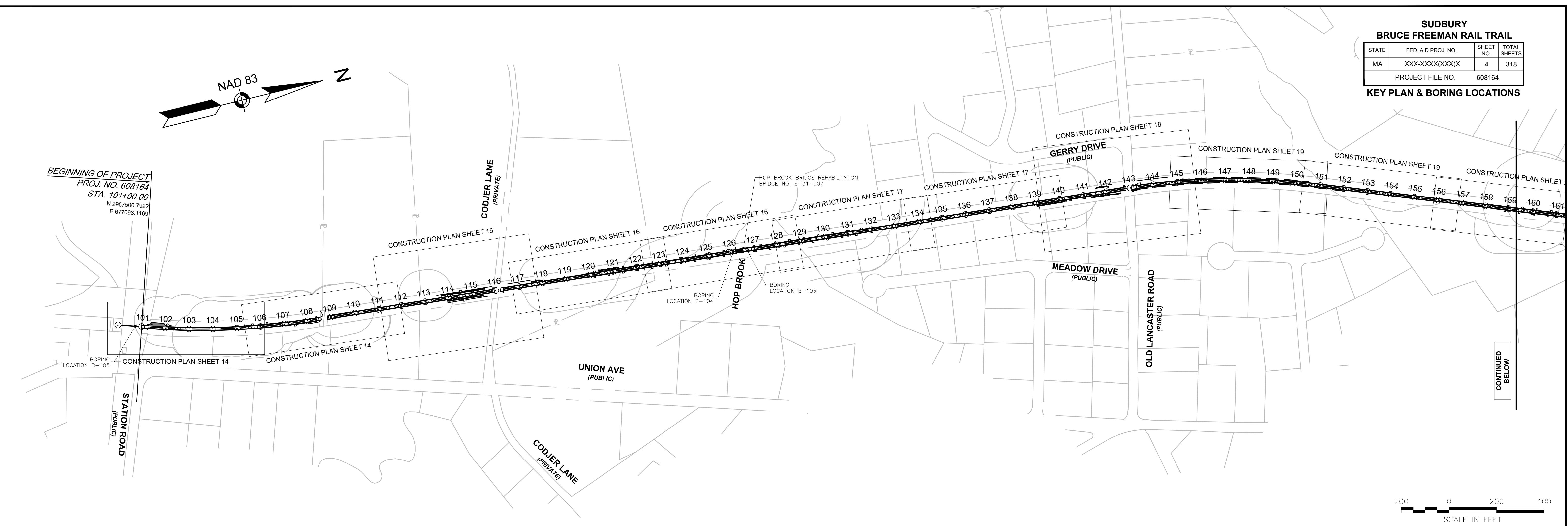
**SUBURRY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	4	318
PROJECT FILE NO.		608164	

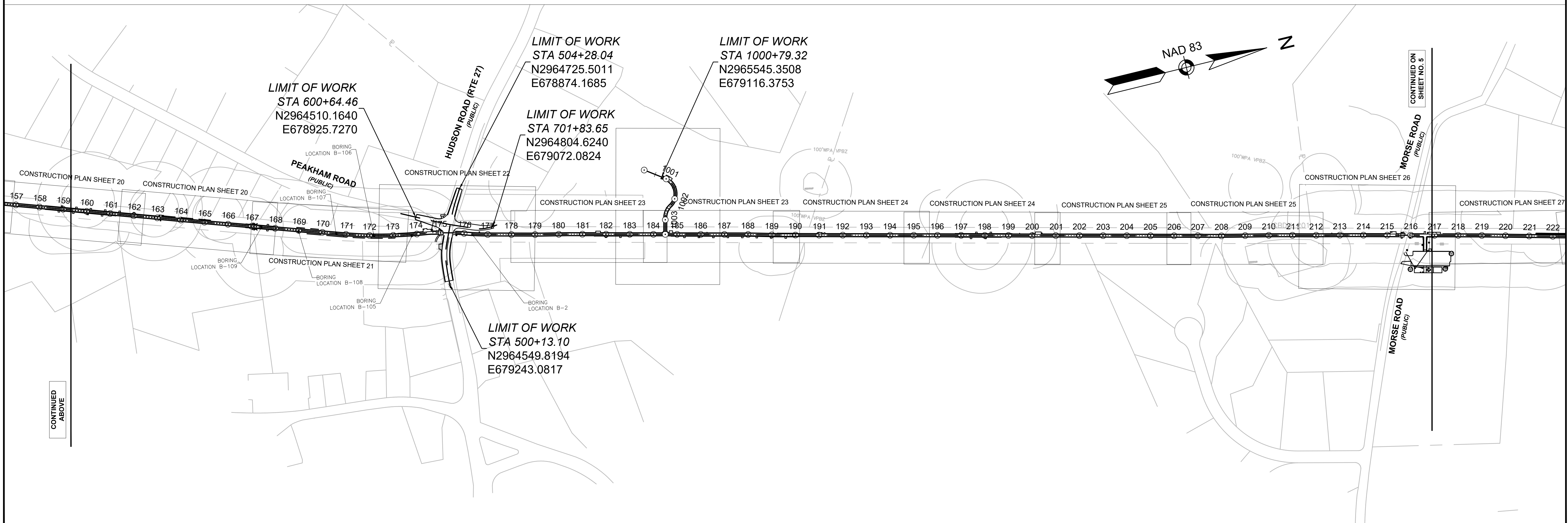
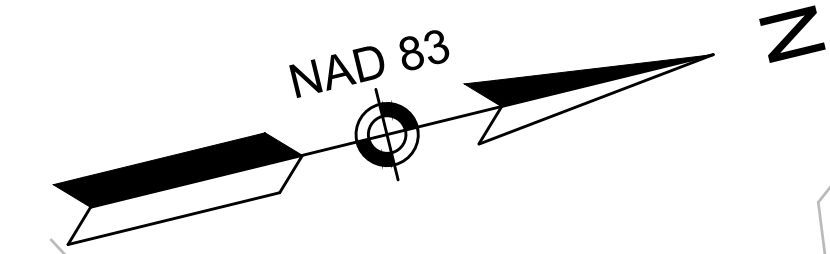
**KEY PLAN & BORING LOCATIONS**



BEGINNING OF PROJECT  
PROJ. NO. 608164  
STA. 101+00.00  
N 2957500.7922  
E 677093.1169



CONTINUED  
BELOW



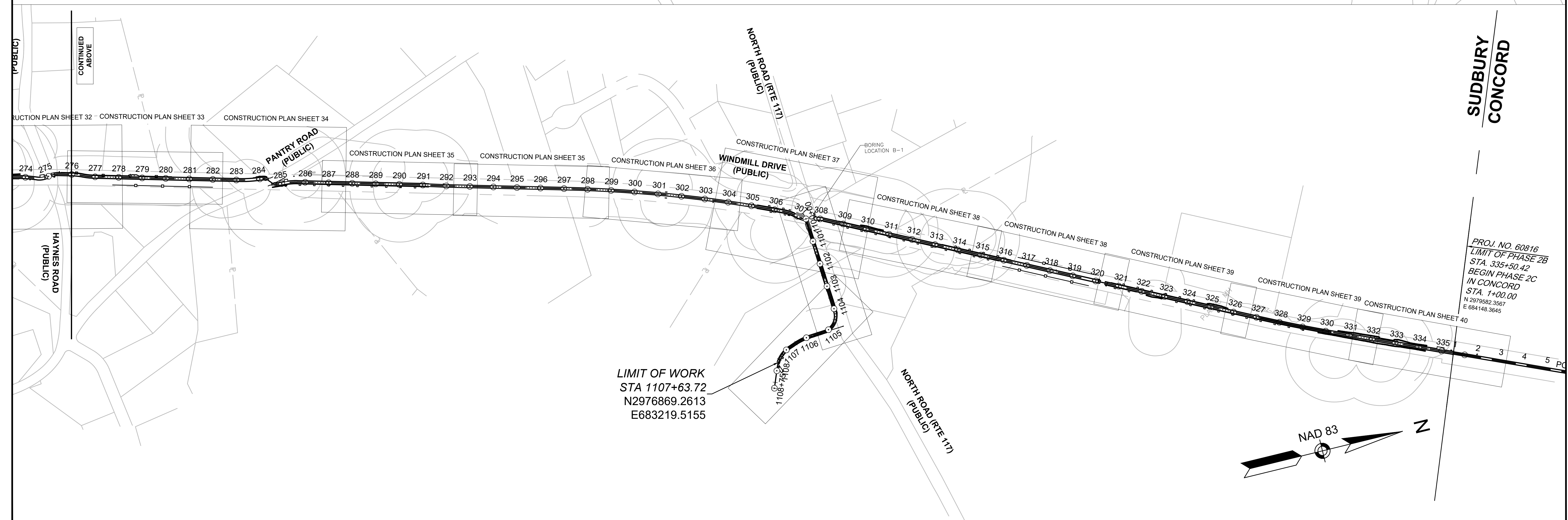
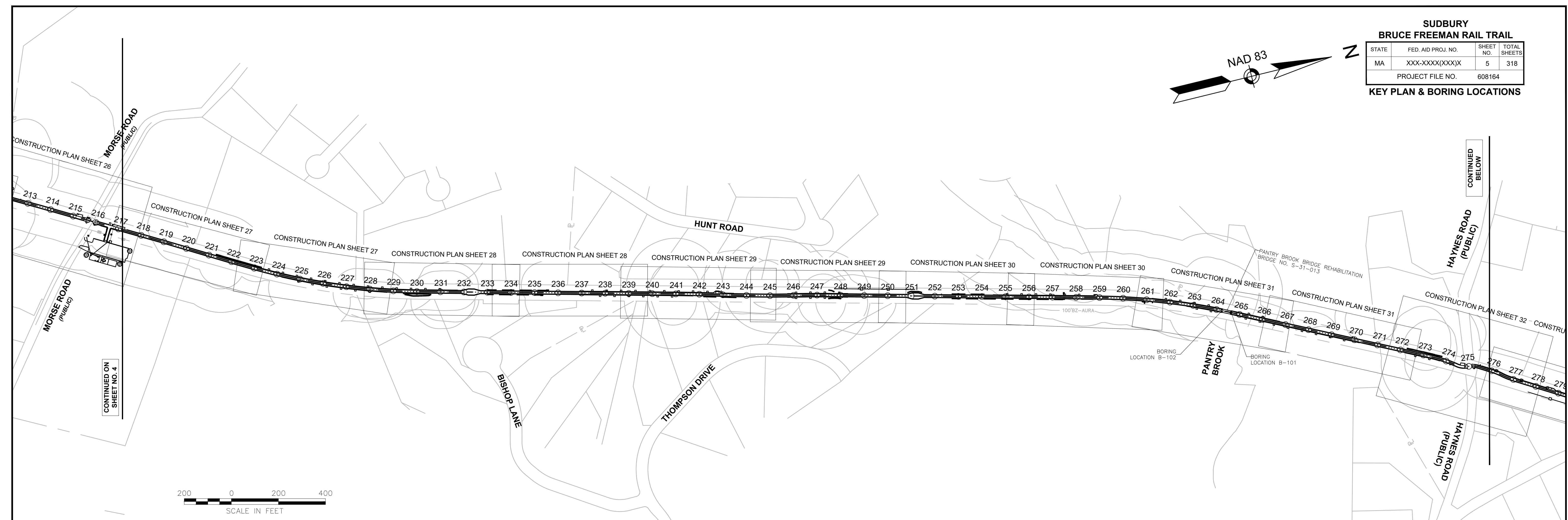
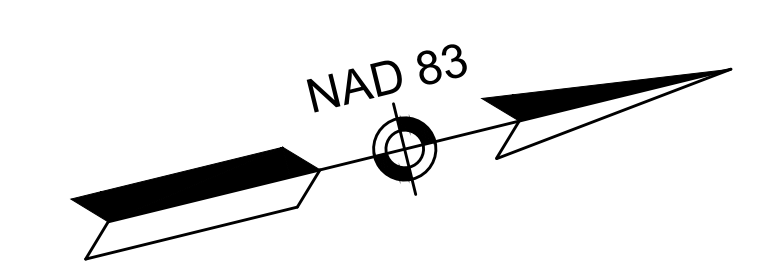
CONTINUED  
ABOVE

CONTINUED ON  
SHEET NO. 5

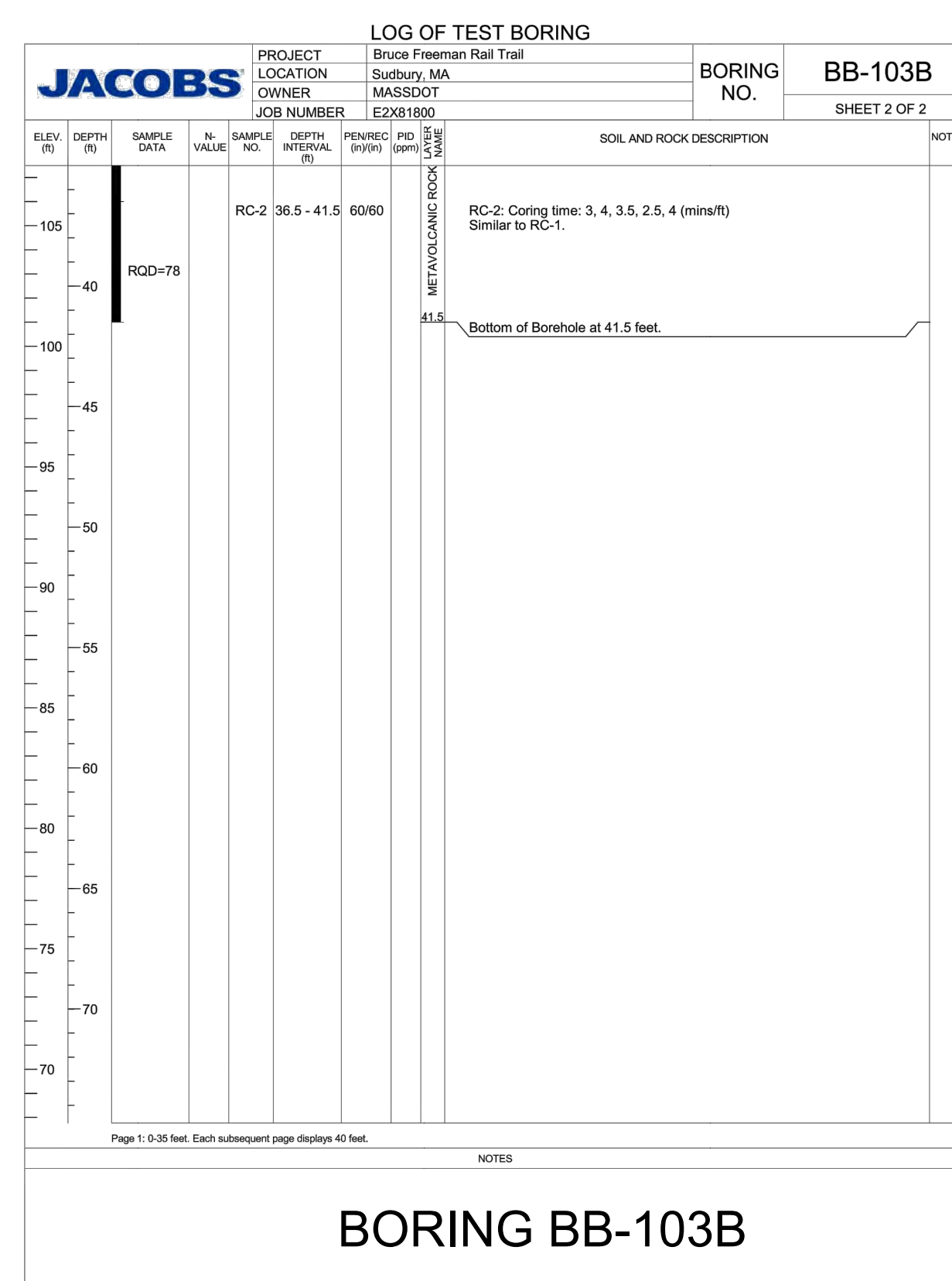
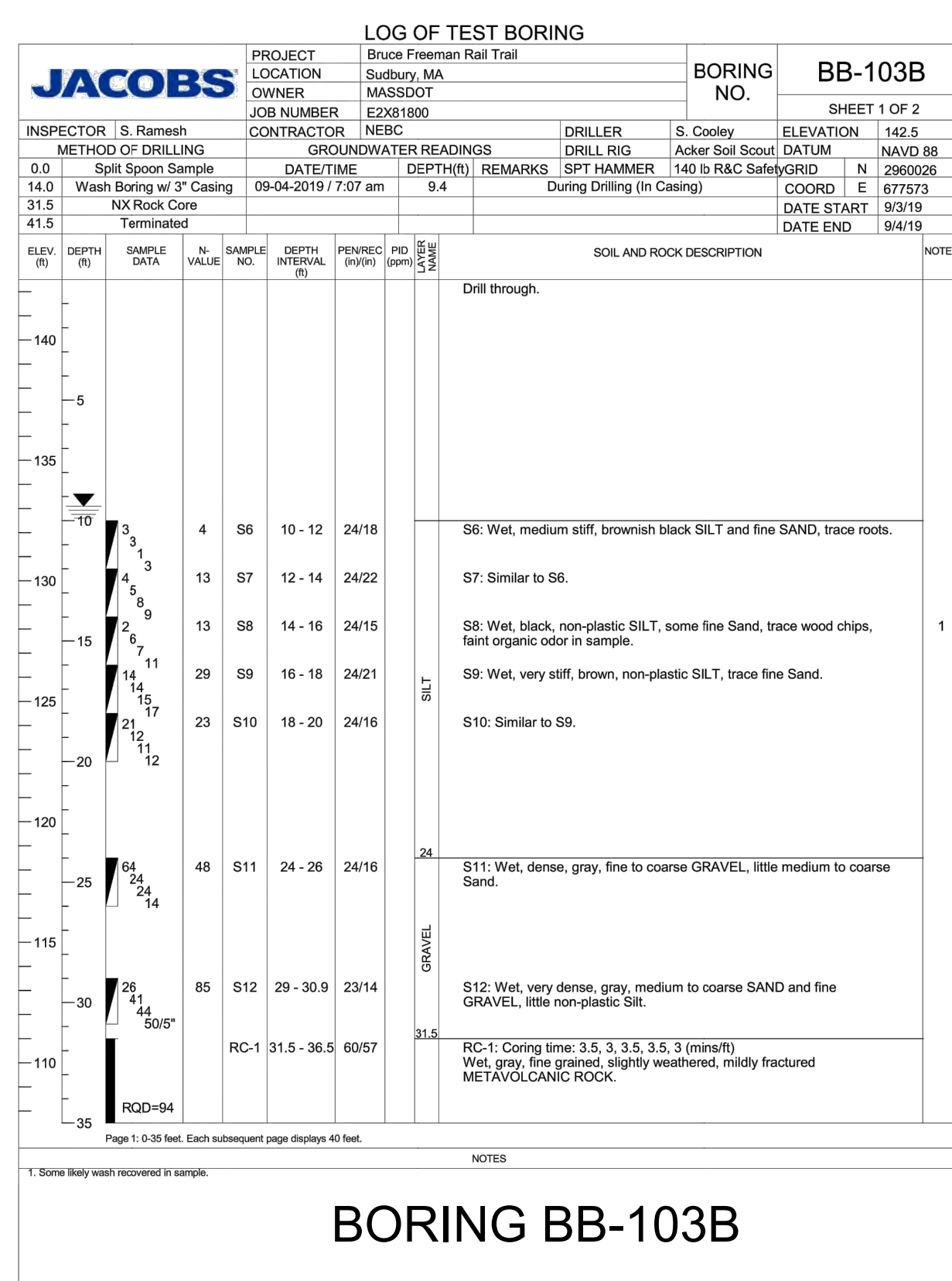
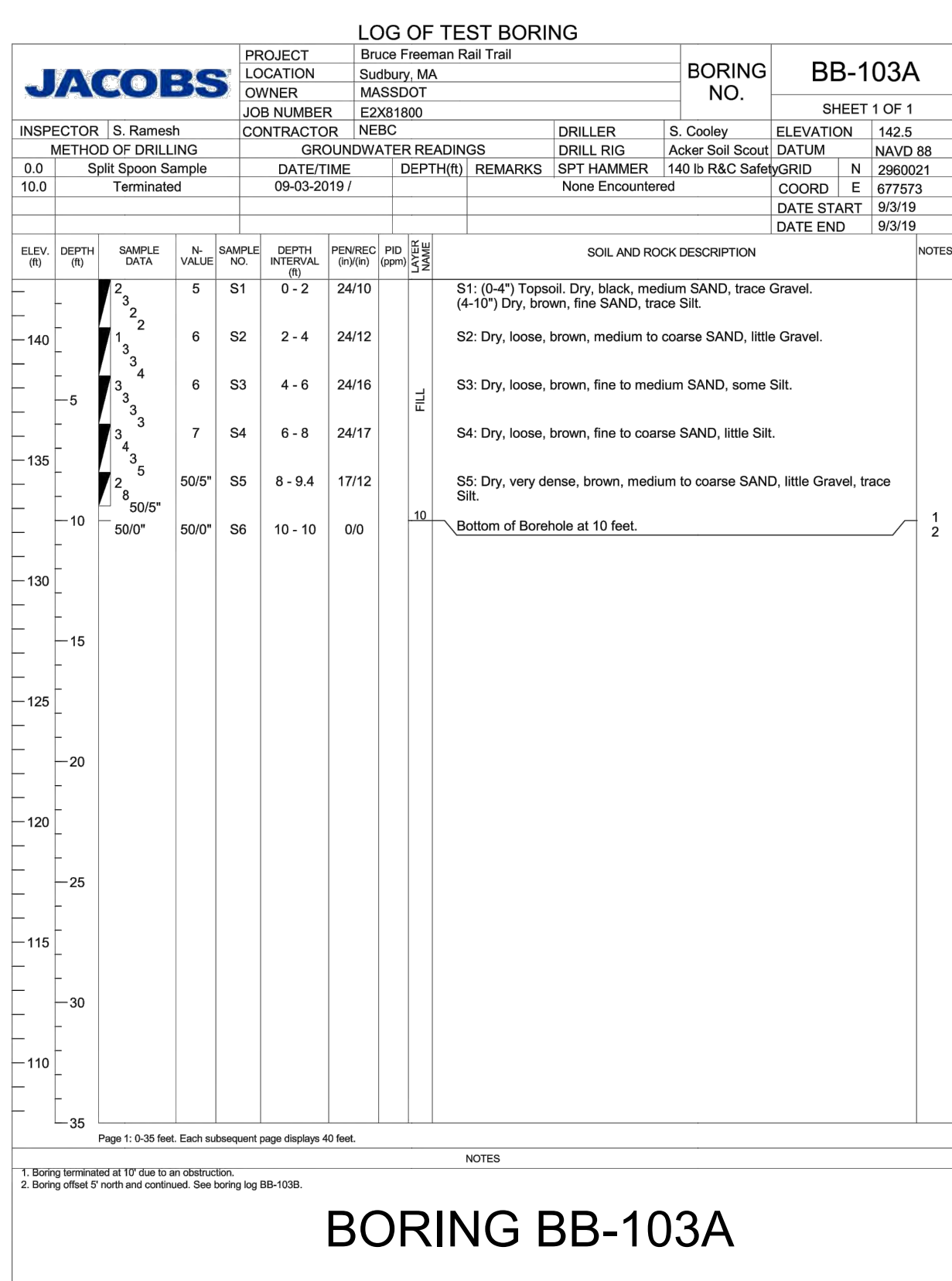
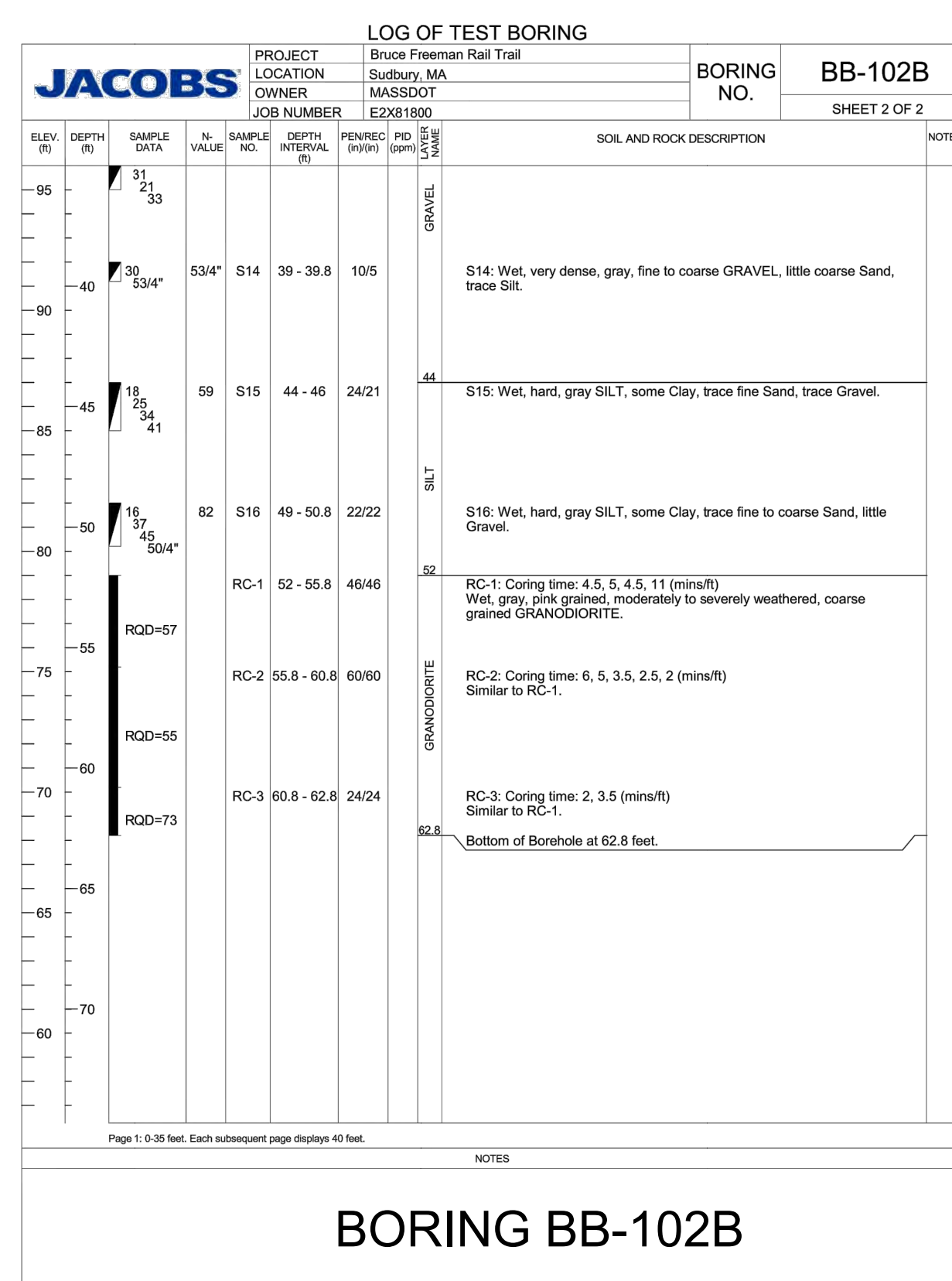
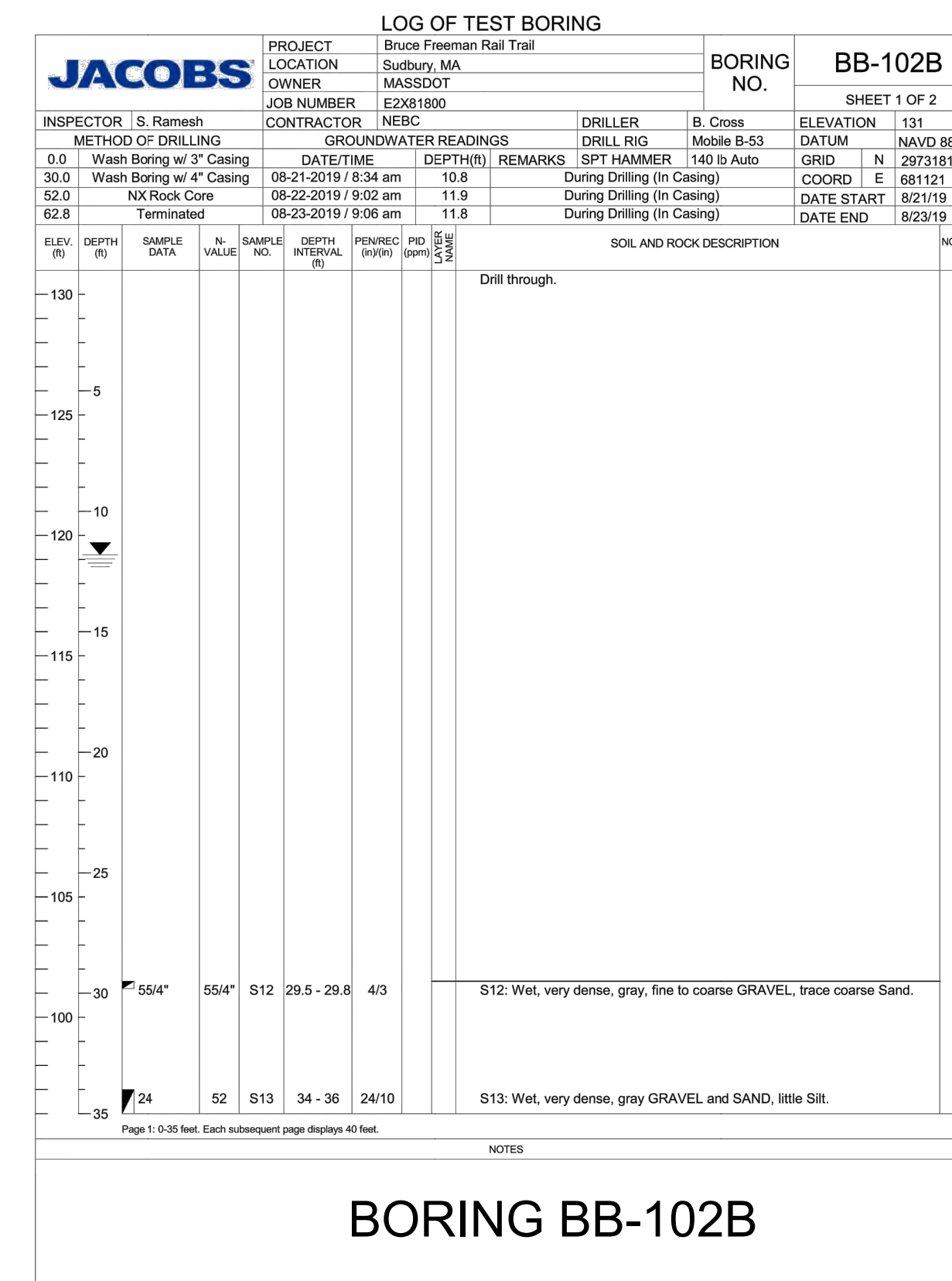
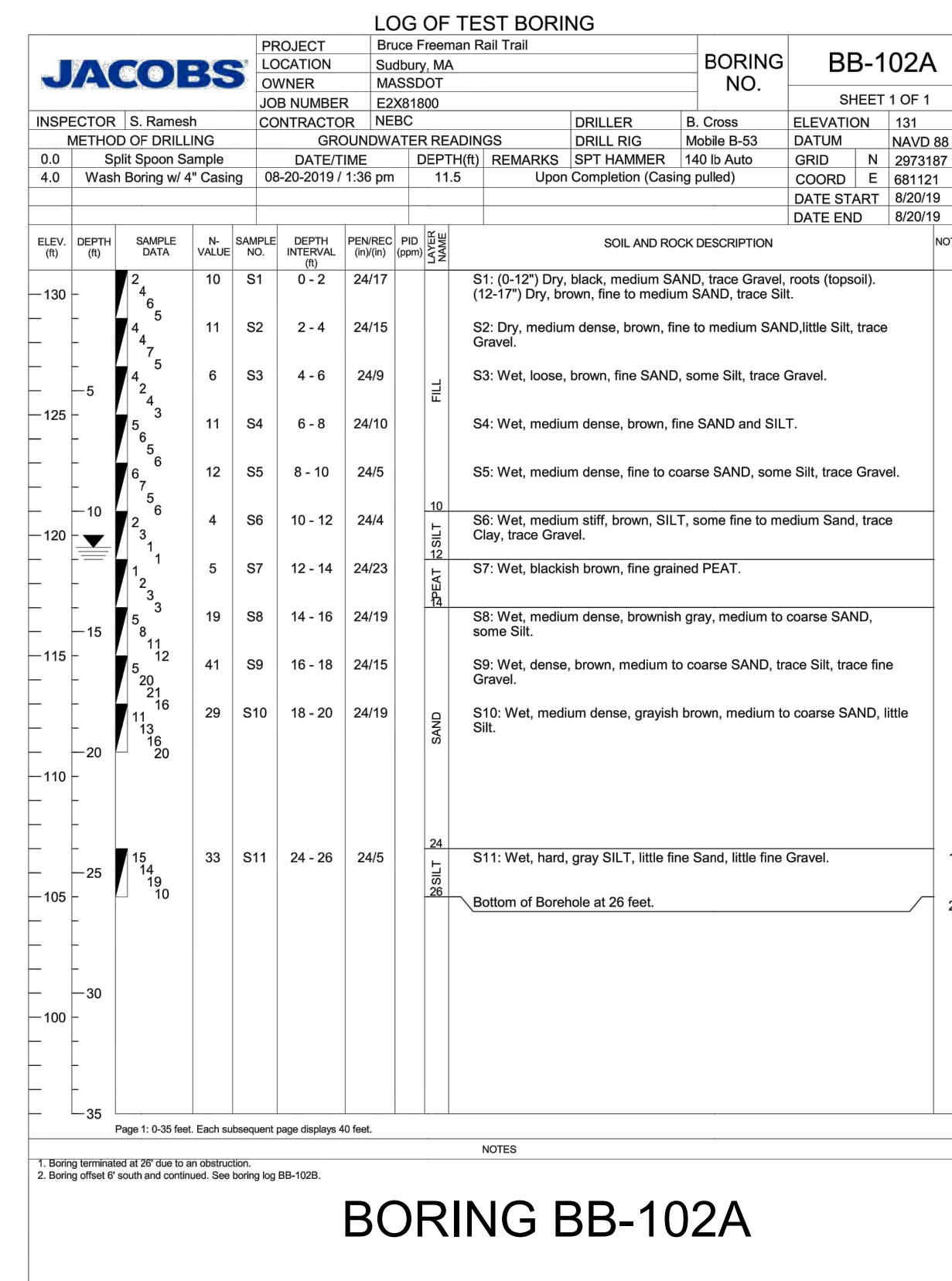
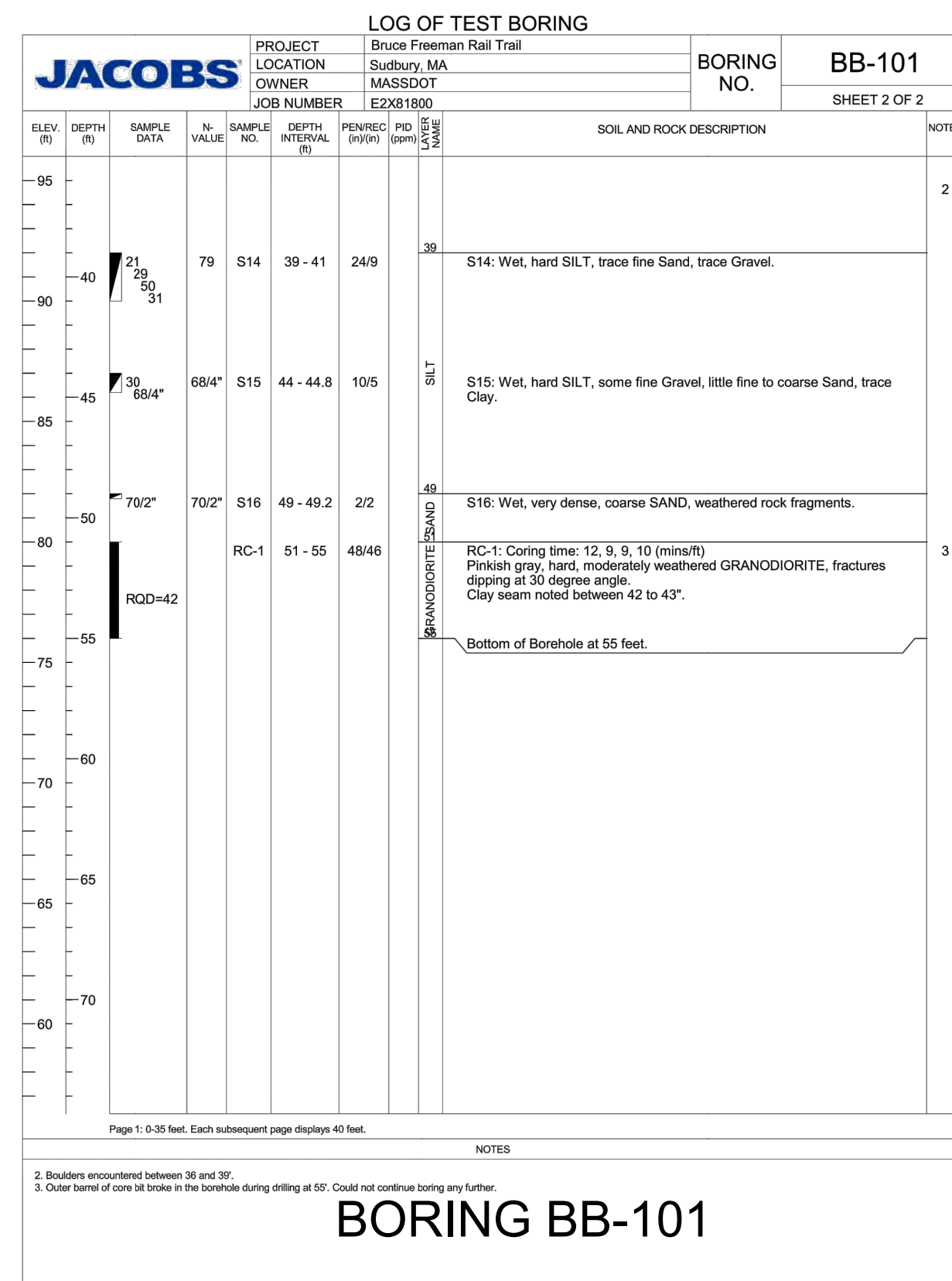
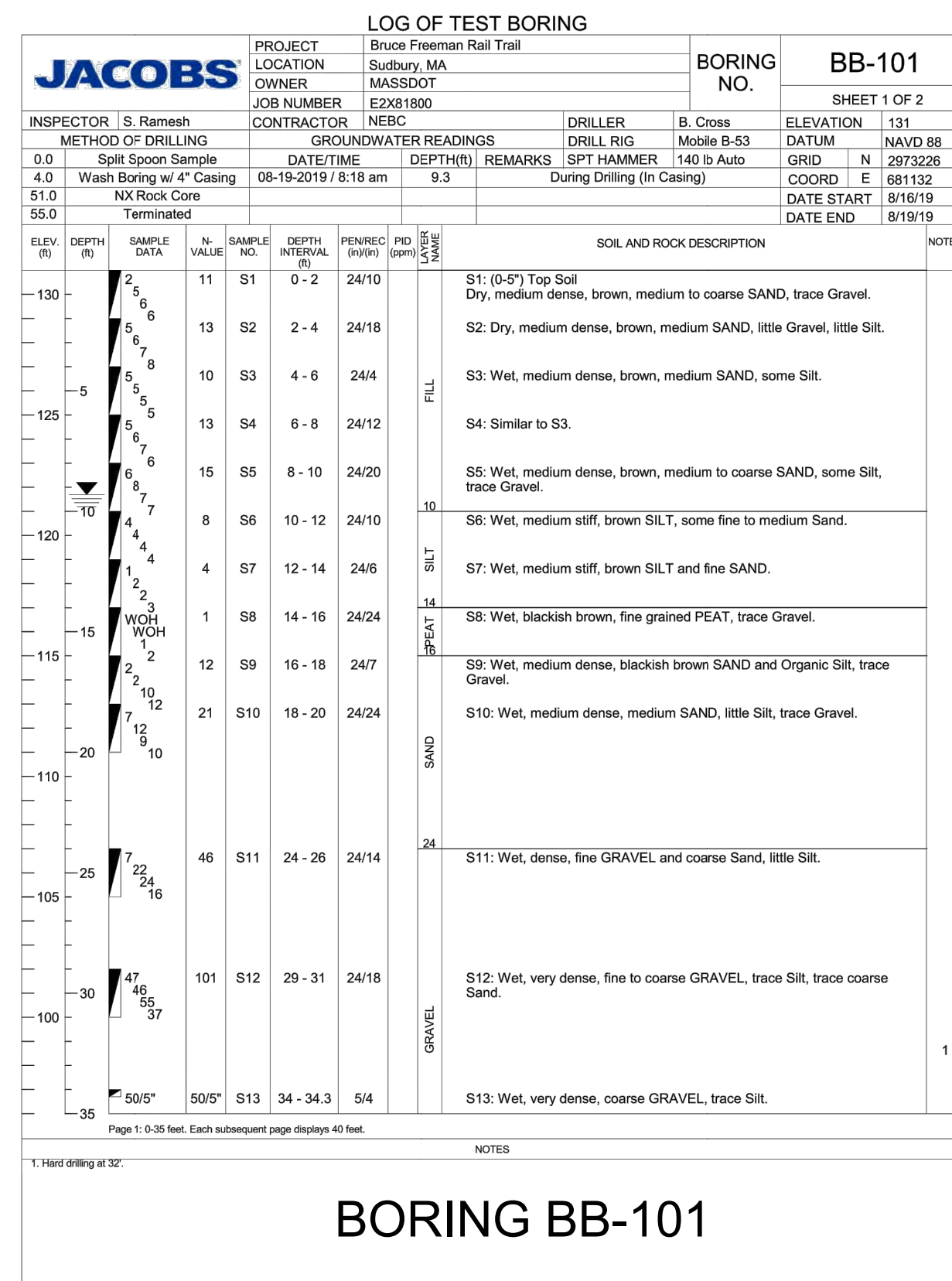
**SUBURRY  
BRUCE FREEMAN RAIL TRAIL**

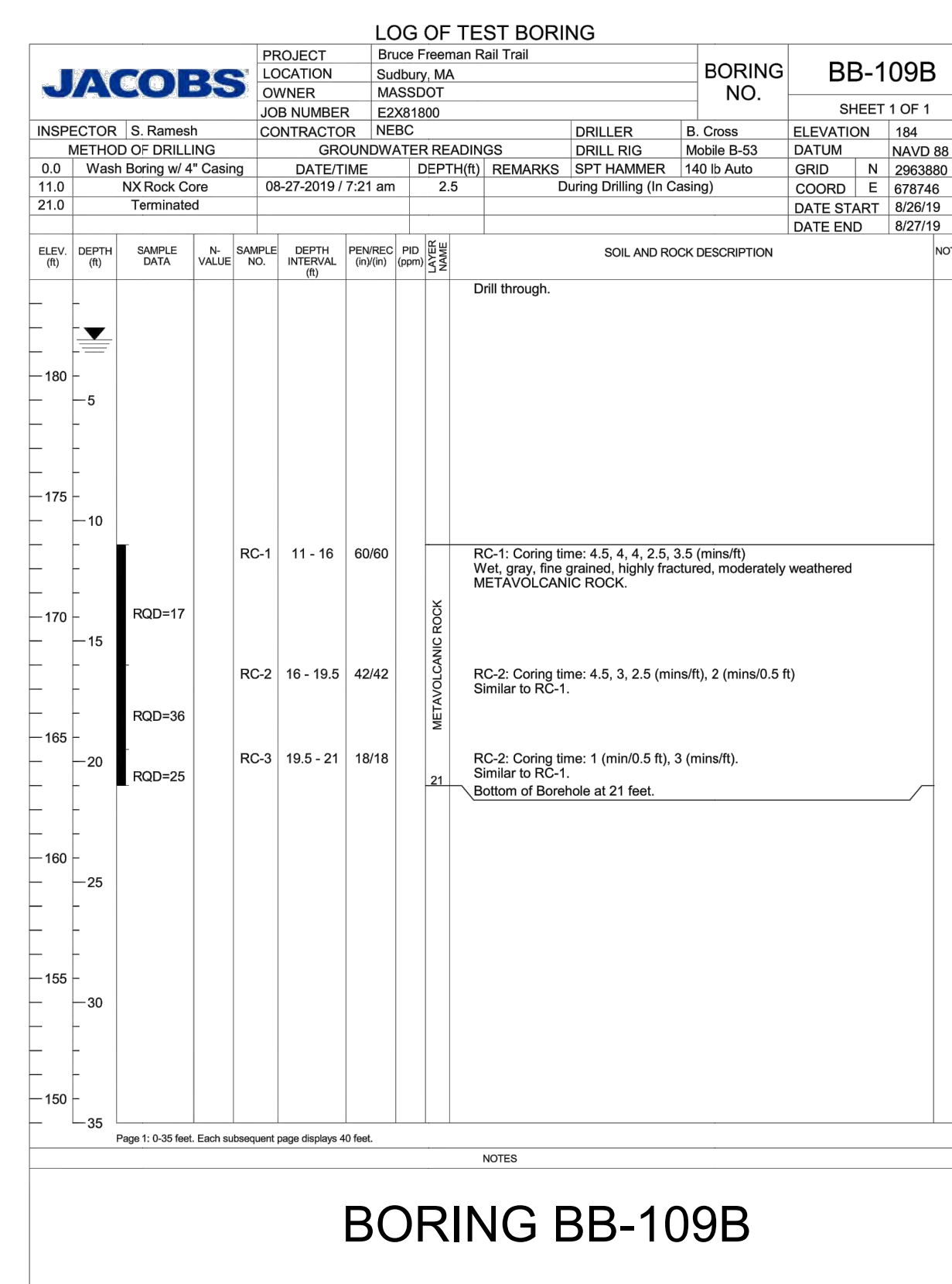
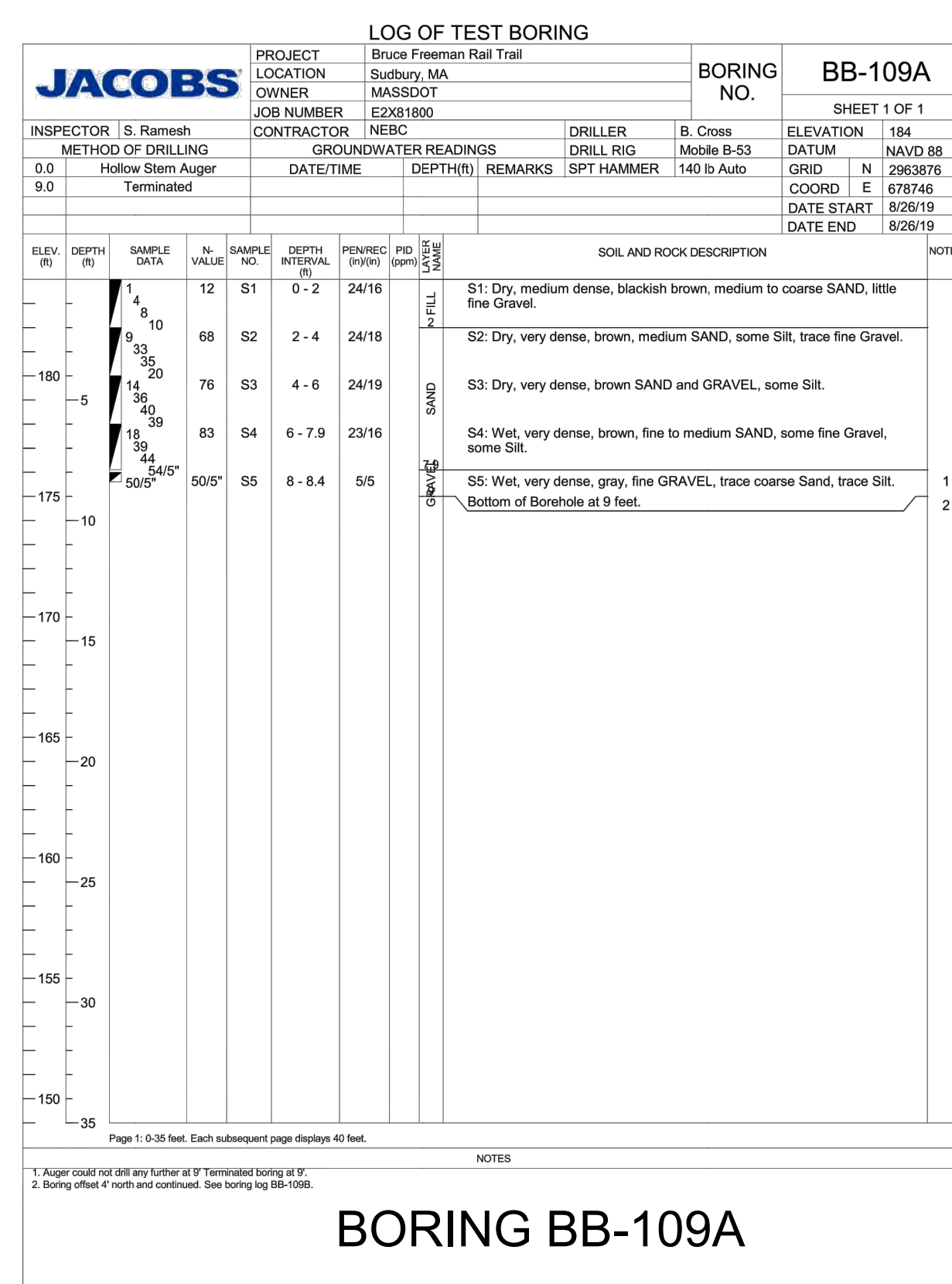
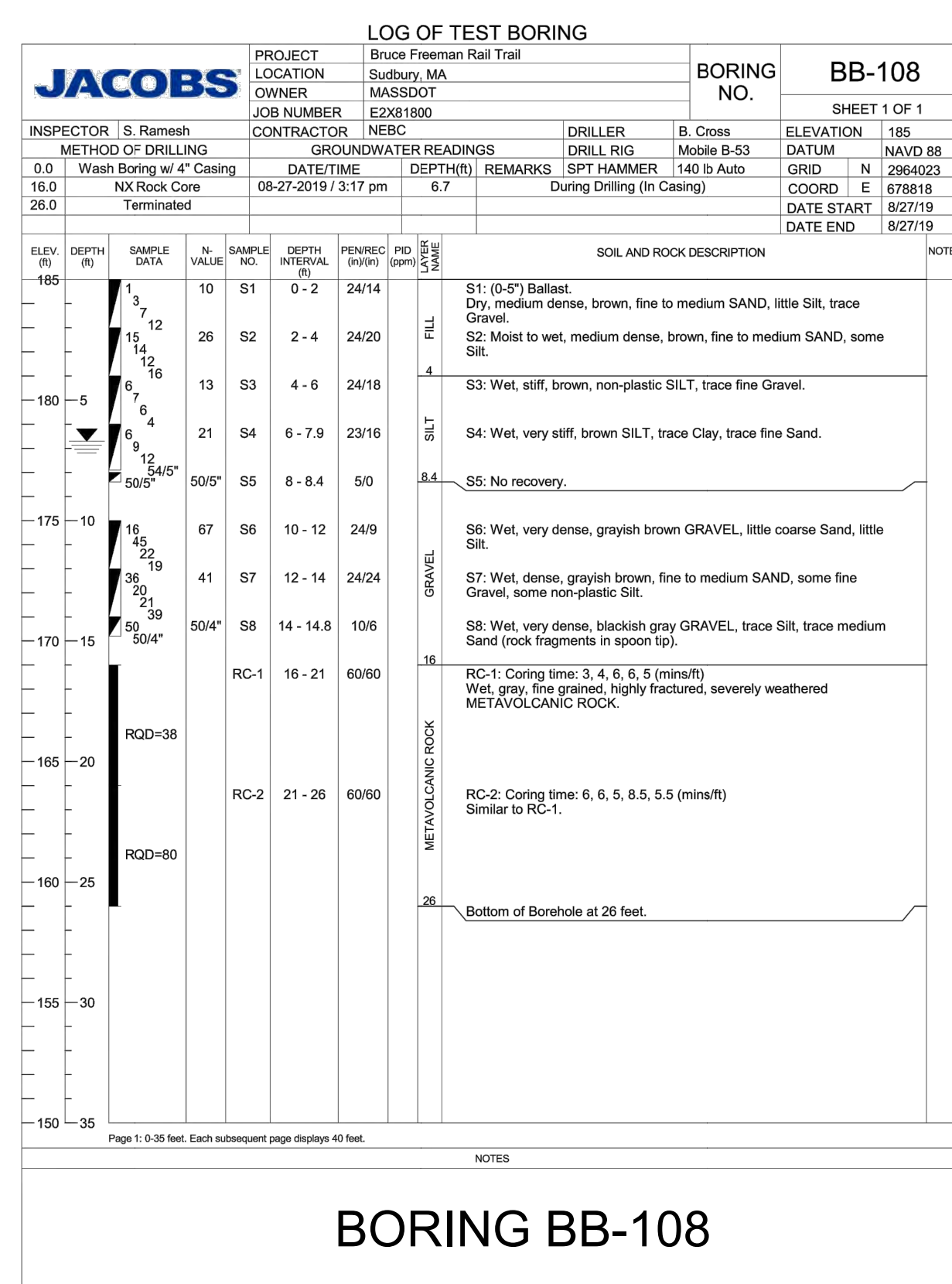
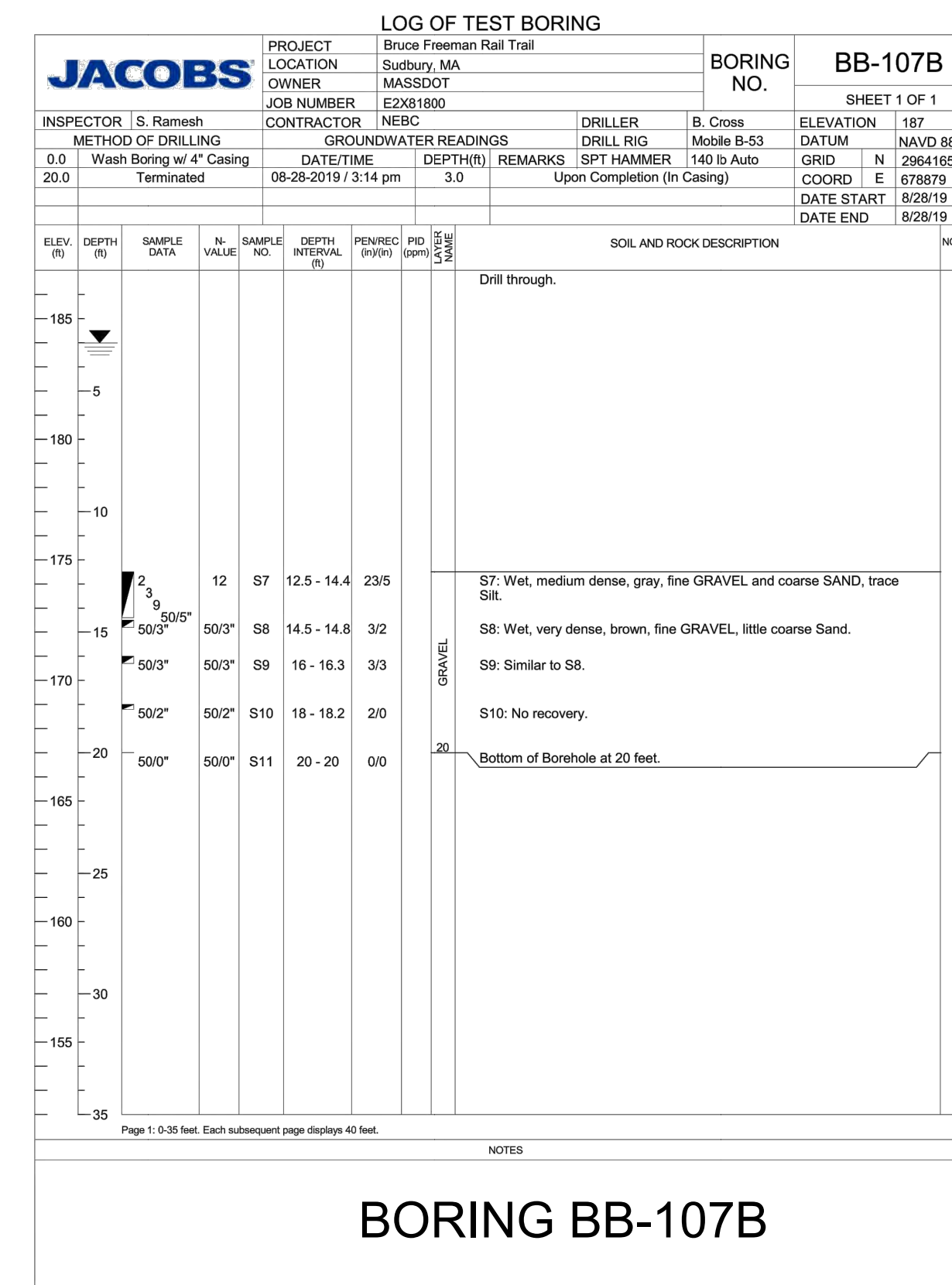
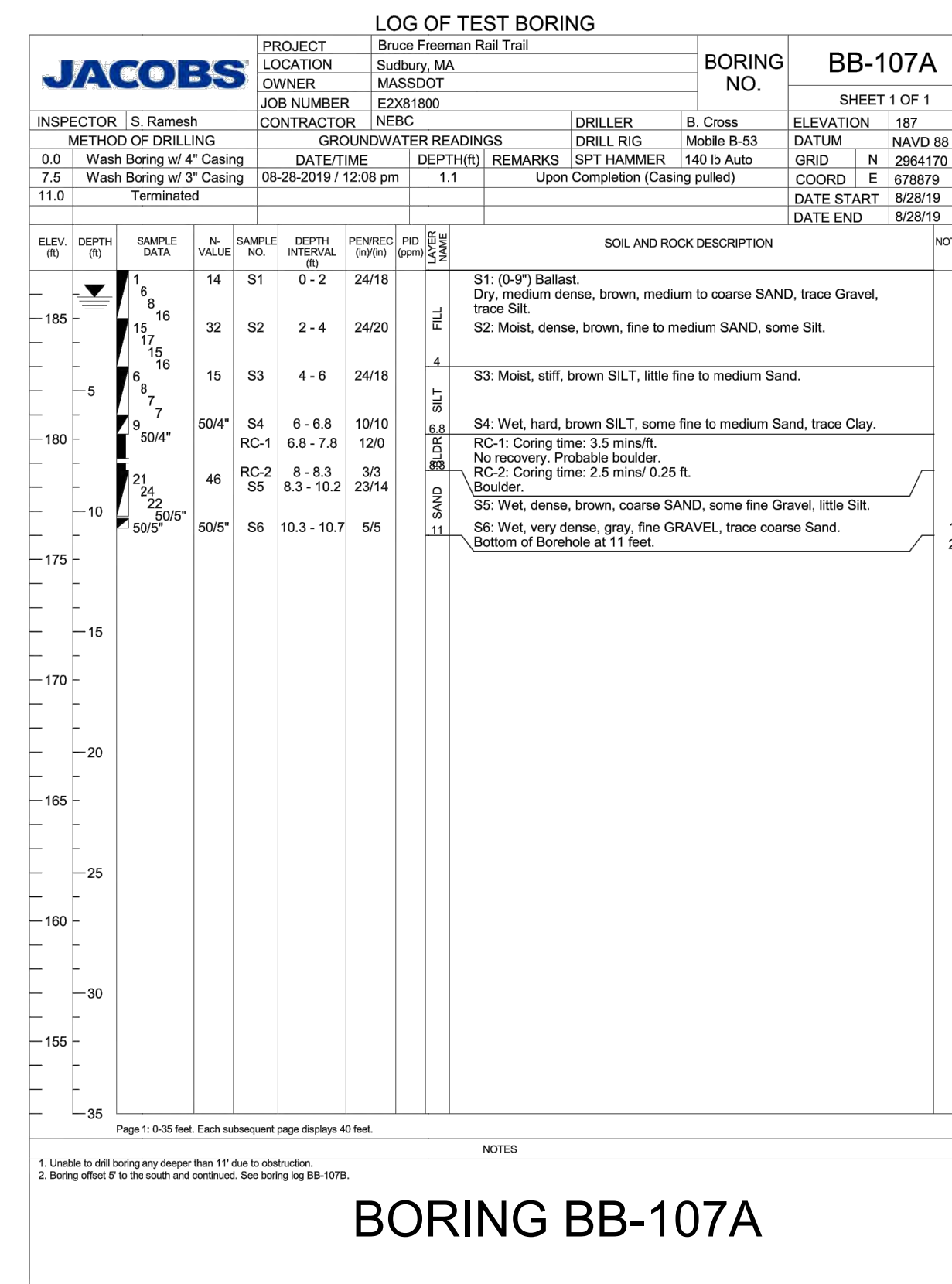
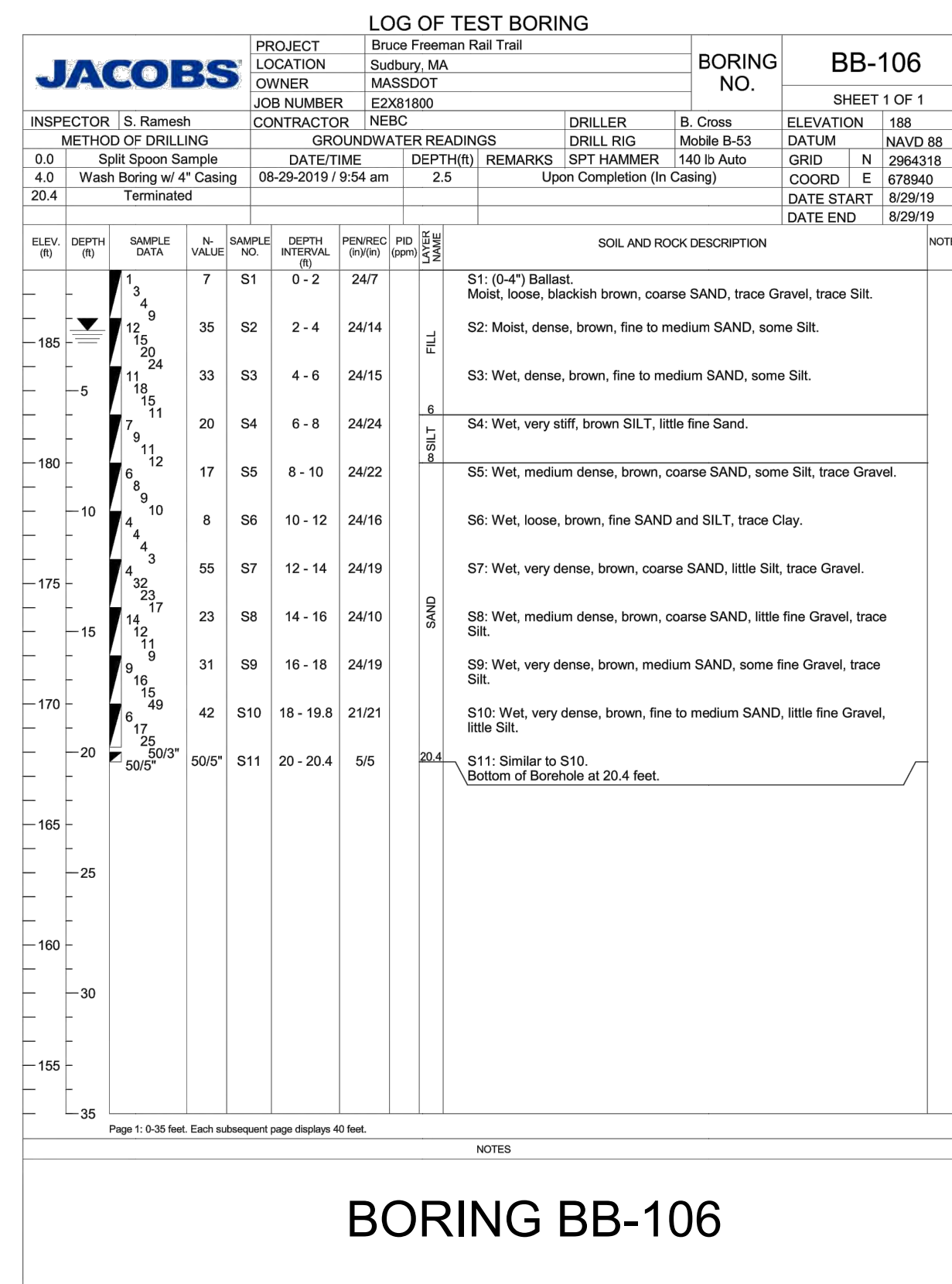
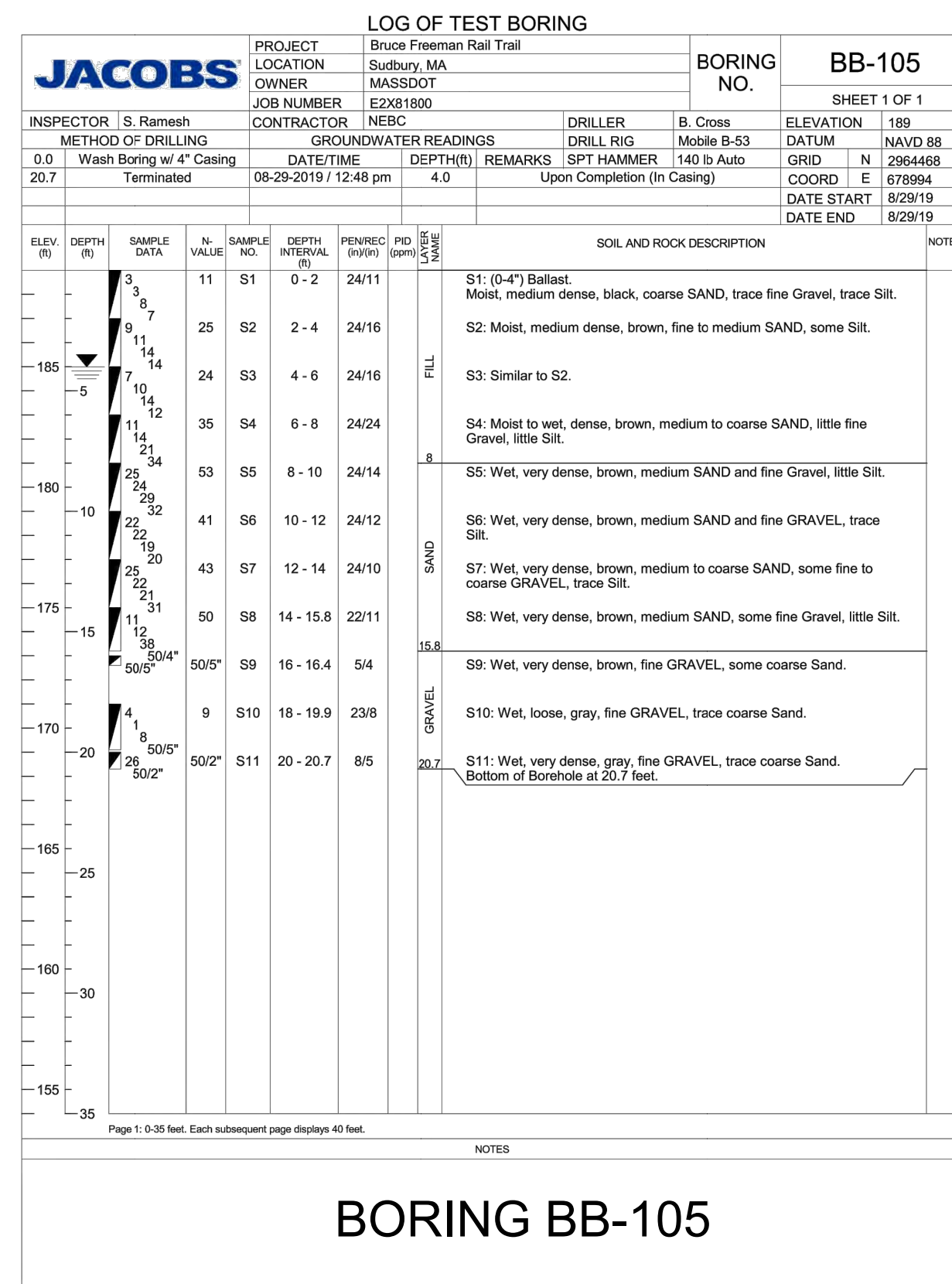
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	5	318
PROJECT FILE NO. 608164			

**KEY PLAN & BORING LOCATIONS**



PROJ. NO. 60816  
 LIMIT OF PHASE 2B  
 STA. 335+50.42  
 BEGIN PHASE 2C  
 IN CONCORD  
 STA. 1+00.00  
 N 2979582.3567  
 E 684148.3645





<b>Client:</b> Fuss & O'Neill		<b>SEABOARD DRILLING, INC.</b>		<b>Test Boring/ Monitor Well ID:</b> B-1			
<b>Location:</b> North Road Sudbury		649 Meadow St., Chicopee, MA 01013					
<b>Project:</b> Bruce Freeman Rail Trail		<b>DRILLING/SOIL LOG</b>		<b>Sheet No.</b> 1 of 1			
<b>Contractor:</b> Seaboard Drilling, Inc.							
<b>Type</b>	<b>Casing</b>	<b>SS</b>	<b>Core Barrel</b>	<b>Hammer (Weight-lb./fall-30")</b>	<b>Start:</b> 3/26/2021		
	HSA		N/A	140/30 300/24	3/26/2021		
<b>O.D. Inch</b>	<b>Rig Type: Mobile B-53</b>						
<b>I.D. Inch</b>	<b>Driller: Dale Griffin</b>						
	8-1/2"						
	4-1/4"						
<b>Depth (ft.) Range</b>	<b>Blows</b>	<b>Sample No.</b>	<b>Recovery</b>	<b>FIELD CLASSIFICATIONS AND REMARKS</b>			
0-2'	2-3-4-3	S-1	9"	fine to coarse SAND			
5-7'	2-5-2-13	S-2	14"	fine to coarse SAND to fine Sand, trace Silt (wet)			
10-12'	13-13-9-12	S-3	12"	Similar to S-2 above			
15-17'	woh-3-4	S-4	24"	fine SAND & Silt			
20-22'	8-25-35-50/3	S-5	18"	weathered rock			
25-27'	50/3*	S-6	0	Auger refusal @ 23' End of Boring @ 23'			
SAMPLE PENETRATION RESISTANCE - 140 lb. Wt. Falling 30" on 2" O.D. sampler							
<b>Density (# Hammer Blows)</b>		<b>Cohesive Consistence (# Hammer Blows)</b>			<b>PROPORTIONS</b>		
0-4	Very Loose	0-2	Very Soft	3-4	Soft	Trace	0 to 10%
5-9	Loose	5-8	Medium-Stiff	9-15	Stiff	Little	10 to 20%
10-29	Medium-Dense	16-30	Very Stiff	31+	Hard	Some	20 to 35%
30-49	Dense					and	30 to 50%
50+	Very Dense						

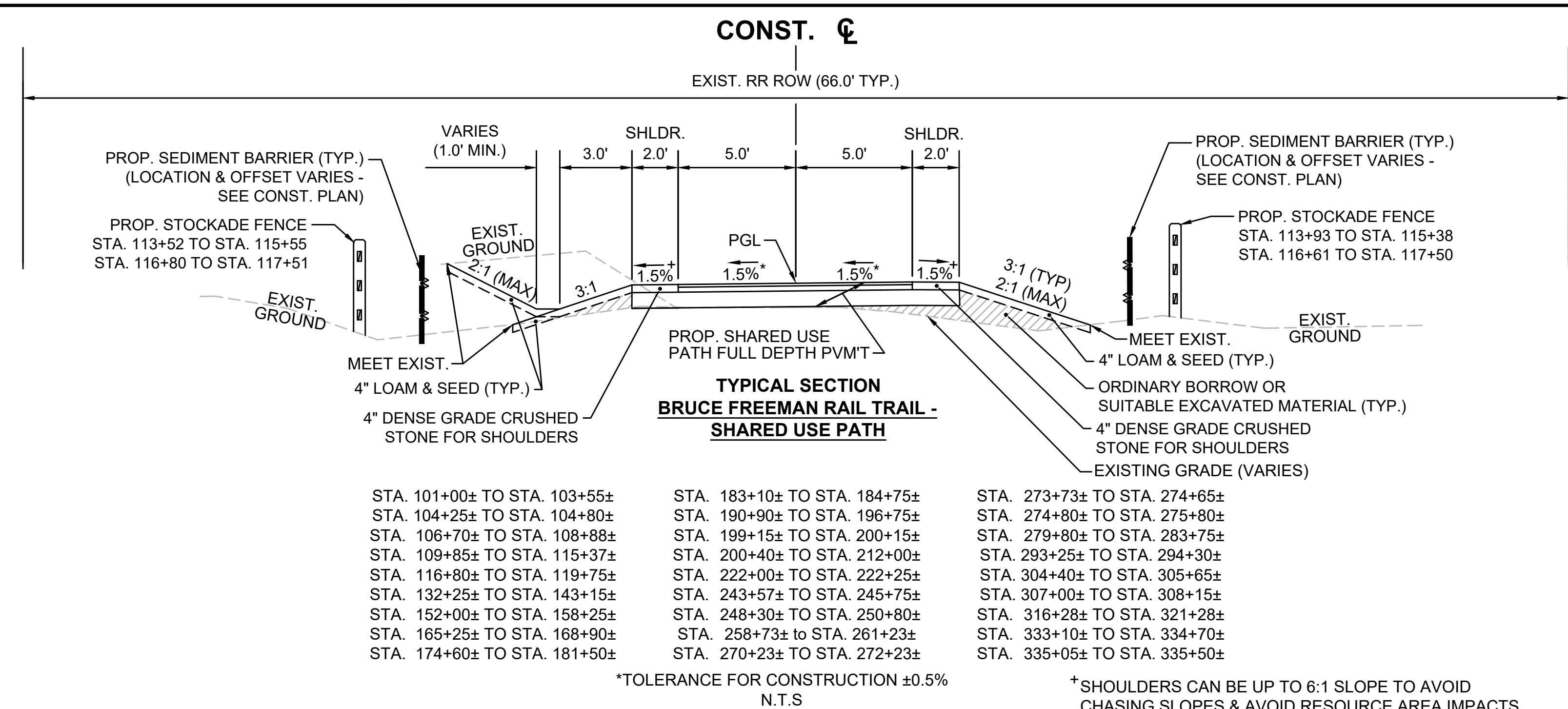
<b>Client:</b> Fuss & O'Neill		<b>SEABOARD DRILLING, INC.</b>		<b>Test Boring/ Monitor Well ID:</b> B-2			
<b>Location:</b> Peakham/Hudson Sudbury Ma		649 Meadow St., Chicopee, MA 01013					
<b>Project:</b> Bruce Freeman Rail Trail		<b>DRILLING/SOIL LOG</b>		<b>Sheet No.</b> 1 of 1			
<b>Contractor:</b> Seaboard Drilling, Inc.							
<b>Type</b>	<b>Casing</b>	<b>SS</b>	<b>Core Barrel</b>	<b>Hammer (Weight-lb./fall-30")</b>	<b>Start:</b> 3/26/2021		
	HSA		N/A	140/30 300/24	3/26/2021		
<b>O.D. Inch</b>	<b>Rig Type: Mobile B-53</b>						
<b>I.D. Inch</b>	<b>Driller: Dale Griffin</b>						
	8-1/2"						
	4-1/4"						
<b>Depth (ft.) Range</b>	<b>Blows</b>	<b>Sample No.</b>	<b>Recovery</b>	<b>FIELD CLASSIFICATIONS AND REMARKS</b>			
0-2'	3-2-10-11	S-1	18"	fine to coarse SAND, littlr Gravel, wood			
5-7'	8-43-50-50/4	S-2	20"	fine to coarse SAND and Gravel			
10-12'	15-20-50/4*	S-3	8"	Similar to S-2 above (wet)			
15-17'	50/2*	S-4	0	Auger refusal @ 15.5' very dense from 12-15.5' End of Boring @ 15.5'			
SAMPLE PENETRATION RESISTANCE - 140 lb. Wt. Falling 30" on 2" O.D. sampler							
<b>Density (# Hammer Blows)</b>		<b>Cohesive Consistence (# Hammer Blows)</b>			<b>PROPORTIONS</b>		
0-4	Very Loose	0-2	Very Soft	3-4	Soft	Trace	0 to 10%
5-9	Loose	5-8	Medium-Stiff	9-15	Stiff	Little	10 to 20%
10-29	Medium-Dense	16-30	Very Stiff	31+	Hard	Some	20 to 35%
30-49	Dense					and	30 to 50%
50+	Very Dense						

MAST ARM & STRAIN POLE LOCATION KEY						
NO.	STATION/OFFSET	INTERSECTION	MAST ARM SIZE	SOIL BORING NO.	SOIL TYPE	FOUNDATION DEPTH
1	502+56/ 32.9 RT	HUDSON/ PEAKHAM	45'-0"	B-2	LOOSE TO DENSE DRY SAND	12'-0"
2	307+80 / 9.8 LT	BFRT/ NORTH ROAD	25'-0"	B-1	LOOSE DRY SAND	7'-6"

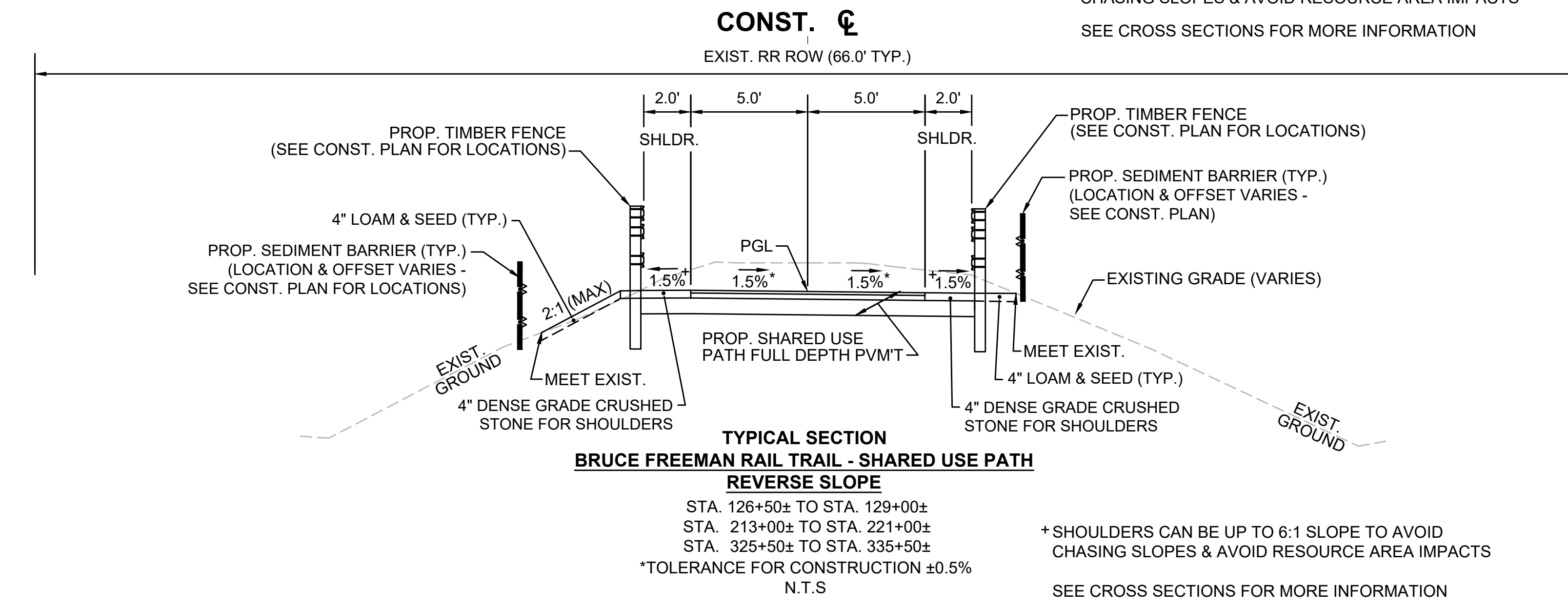
**\*FOR BIDDING PURPOSES ONLY.  
ACTUAL FOUNDATION DEPTH TO  
BE DETERMINED BY THE  
CONTRACTORS STRUCTURAL  
ENGINEER/SUPPLIER AND  
APPROVED THROUGH STAMPED  
SHOP DRAWINGS.**



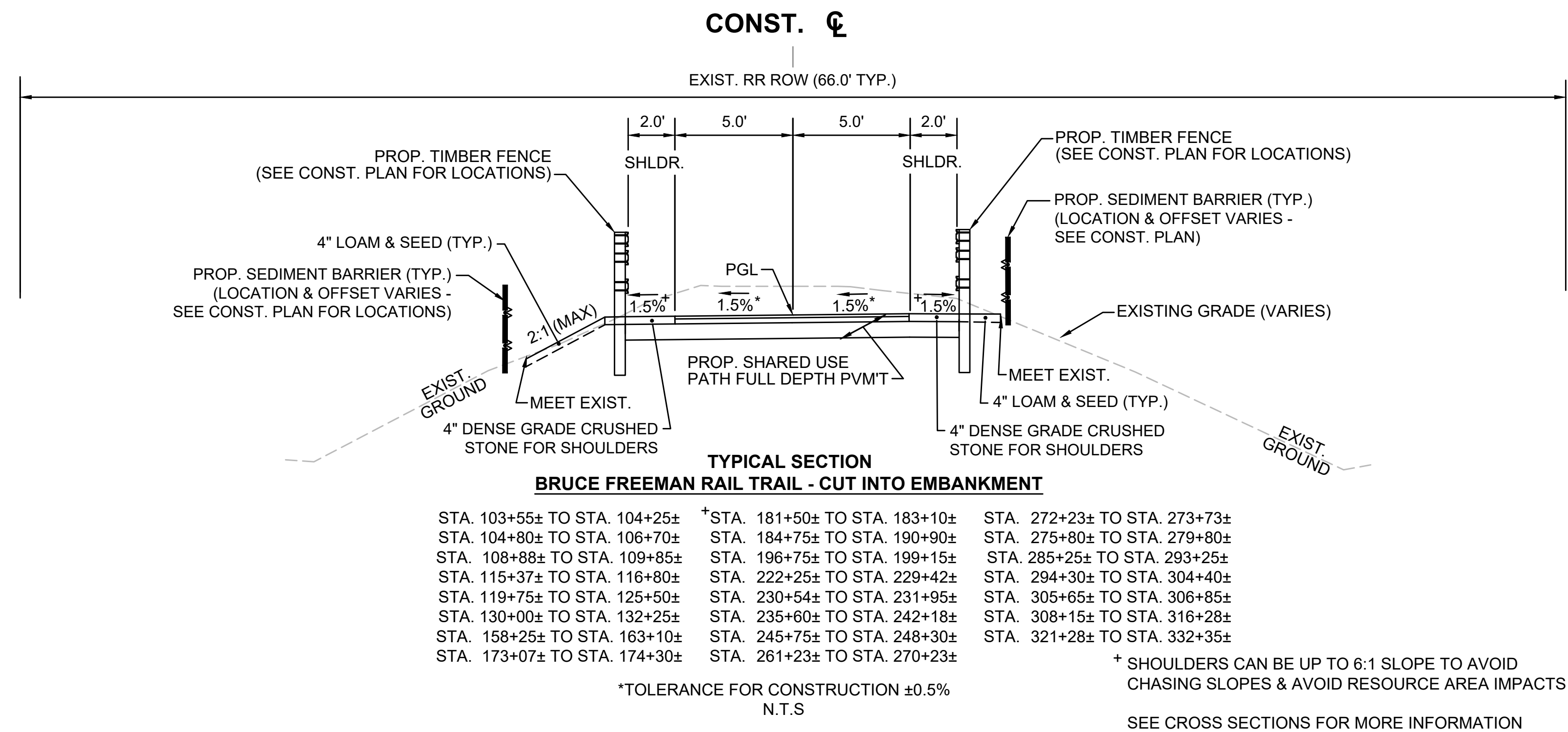
SUBBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	9	318
PROJECT FILE NO.		608164	



- |                              |                              |                              |
|------------------------------|------------------------------|------------------------------|
| STA. 101+00± TO STA. 103+55± | STA. 183+10± TO STA. 184+75± | STA. 273+73± TO STA. 274+65± |
| STA. 104+25± TO STA. 104+80± | STA. 190+90± TO STA. 196+75± | STA. 274+80± TO STA. 275+80± |
| STA. 106+70± TO STA. 108+88± | STA. 199+15± TO STA. 200+15± | STA. 279+80± TO STA. 283+75± |
| STA. 109+85± TO STA. 115+37± | STA. 200+40± TO STA. 212+00± | STA. 293+25± TO STA. 294+30± |
| STA. 116+80± TO STA. 119+75± | STA. 222+00± TO STA. 222+25± | STA. 304+40± TO STA. 305+65± |
| STA. 132+25± TO STA. 143+15± | STA. 243+57± TO STA. 245+75± | STA. 307+00± TO STA. 308+15± |
| STA. 152+00± TO STA. 158+25± | STA. 248+30± TO STA. 250+80± | STA. 316+28± TO STA. 321+28± |
| STA. 165+25± TO STA. 168+90± | STA. 258+73± TO STA. 261+23± | STA. 333+10± TO STA. 334+70± |
| STA. 174+60± TO STA. 181+50± | STA. 270+23± TO STA. 272+23± | STA. 335+05± TO STA. 335+50± |



- STA. 126+50± TO STA. 129+00±  
 STA. 213+00± TO STA. 221+00±  
 STA. 325+50± TO STA. 335+50±



- |                              |                              |                              |
|------------------------------|------------------------------|------------------------------|
| STA. 103+55± TO STA. 104+25± | STA. 181+50± TO STA. 183+10± | STA. 272+23± TO STA. 273+73± |
| STA. 104+80± TO STA. 106+70± | STA. 184+75± TO STA. 190+90± | STA. 275+80± TO STA. 279+80± |
| STA. 108+88± TO STA. 109+85± | STA. 196+75± TO STA. 199+15± | STA. 285+25± TO STA. 293+25± |
| STA. 115+37± TO STA. 116+80± | STA. 222+25± TO STA. 229+42± | STA. 294+30± TO STA. 304+40± |
| STA. 119+75± TO STA. 125+50± | STA. 230+54± TO STA. 231+95± | STA. 305+65± TO STA. 306+85± |
| STA. 130+00± TO STA. 132+25± | STA. 235+60± TO STA. 242+18± | STA. 308+15± TO STA. 316+28± |
| STA. 158+25± TO STA. 163+10± | STA. 245+75± TO STA. 248+30± | STA. 321+28± TO STA. 332+35± |
| STA. 173+07± TO STA. 174+30± | STA. 261+23± TO STA. 270+23± |                              |

### PAVEMENT NOTES:

- PROPOSED SHARED-USE PATH**  
 SURFACE: 1-1/2" SUPERPAVE SURFACE COURSE - 9.5 (SSC - 9.5)  
 INTERMEDIATE: 2-1/2" SUPERPAVE INTERMEDIATE COURSE - 19.0 (SIC - 19.0)  
 SUBBASE: 4" to 8" GRAVEL BORROW, TYPE b (FOR LEVELING) OVER RESHAPED EXISTING RAILROAD BALLAST
- PROPOSED PARKING LOT FULL DEPTH PAVEMENT**  
 SURFACE: 1-1/2" SUPERPAVE SURFACE COURSE 9.5 OVER  
 BASE: 2-1/2" SUPERPAVE INTERMEDIATE COURSE 19.0 - OVER  
 FOUNDATION: 8" GRAVEL BORROW, TYPE b
- PROPOSED FULL DEPTH CONSTRUCTION (LESS THAN 4.00' WIDE) - PEAKHAM ROAD & HUDSON ROAD**  
 SURFACE: 1-3/4" SUPERPAVE SURFACE COURSE - 12.5 POLYMER  
 INTERMEDIATE: 2-1/2" SUPERPAVE INTERMEDIATE COURSE - 19.0 OVER  
 BASE: 6" CEMENT CONCRETE BASE COURSE - 4000psi, 610, 3/4" OVER  
 SUBBASE: 8" GRAVEL BORROW, TYPE b.
- PROPOSED PAVEMENT MILLING & OVERLAY - PEAKHAM ROAD & HUDSON ROAD**  
 1-3/4" PAVEMENT MILLING  
 1-3/4" SUPERPAVE SURFACE COURSE - 12.5 POLYMER
- PROPOSED HOT MIX ASPHALT SIDEWALK & DRIVEWAY**  
 SURFACE: 1-1/2" SUPERPAVE SURFACE COURSE 9.5 OVER  
 2-1/2" SUPERPAVE INTERMEDIATE COURSE 19.0  
 FOUNDATION: 8" GRAVEL BORROW, TYPE b
- PROPOSED CEMENT CONCRETE REST AREA, CONCRETE WALK & WHEELCHAIR RAMP**  
 SURFACE: 6" CEMENT CONCRETE AIR ENTRAINED 4000 PSI, 3/4", 610  
 SUBBASE: 8" GRAVEL BORROW, TYPE b

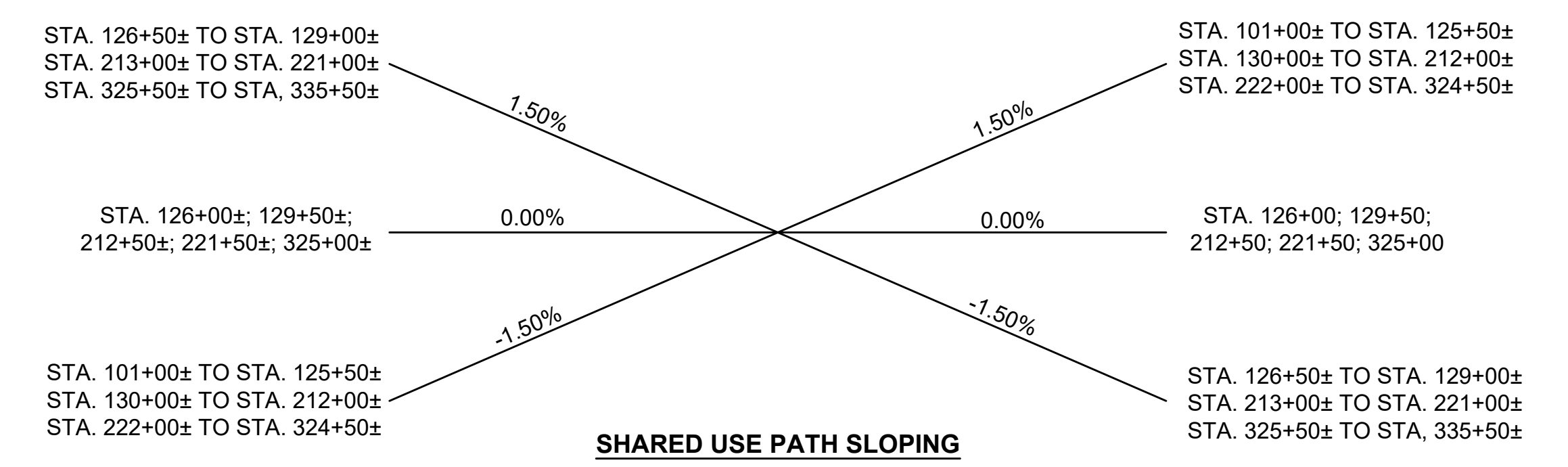
**NOTES:** EXIST. GRAVEL/BALLAST SUBGROUND MATERIAL DETERMINED BY THE ENGINEER TO BE SUITABLE SHALL REMAIN. THE DEPTH OF THE GRAVEL BORROW WILL BE AS REQUIRED BASED ON THE PROPOSED SUB-BASE ELEVATIONS.

AFTER REMOVAL OF STEEL RAILS AND WOOD TIMBER, ROUGH GRADE AND COMPACT SUBGROUND AREA. THEN PLACE AND COMPACT GRAVEL BORROW SUB-BASE MATERIAL IN MULTIPLE LIFTS.

ASPHALT EMULSION FOR TACK COAT AND HMA JOINT SEALANT SHALL BE APPLIED PER SECTION 450 QA OF THE SPECIAL PROVISIONS.

HMA FOR PATCHING SHALL BE USED FOR ALL PERMANENT, PARTIAL, AND FULL DEPTH PAVEMENT REPAIRS OF UNSOUND PAVEMENT PER SECTION 450 IN AREAS OUTSIDE OF PROPOSED FULL DEPTH RECLAMATION OR RECONSTRUCTION ROADWAY AREAS.

HMA FOR MISCELLANEOUS WORK SHALL BE USED FOR ALL TEMPORARY CONSTRUCTION, TAPER RAMPS, CURB CUT RAMPS, TEMPORARY TRENCH REPAIR, ETC.



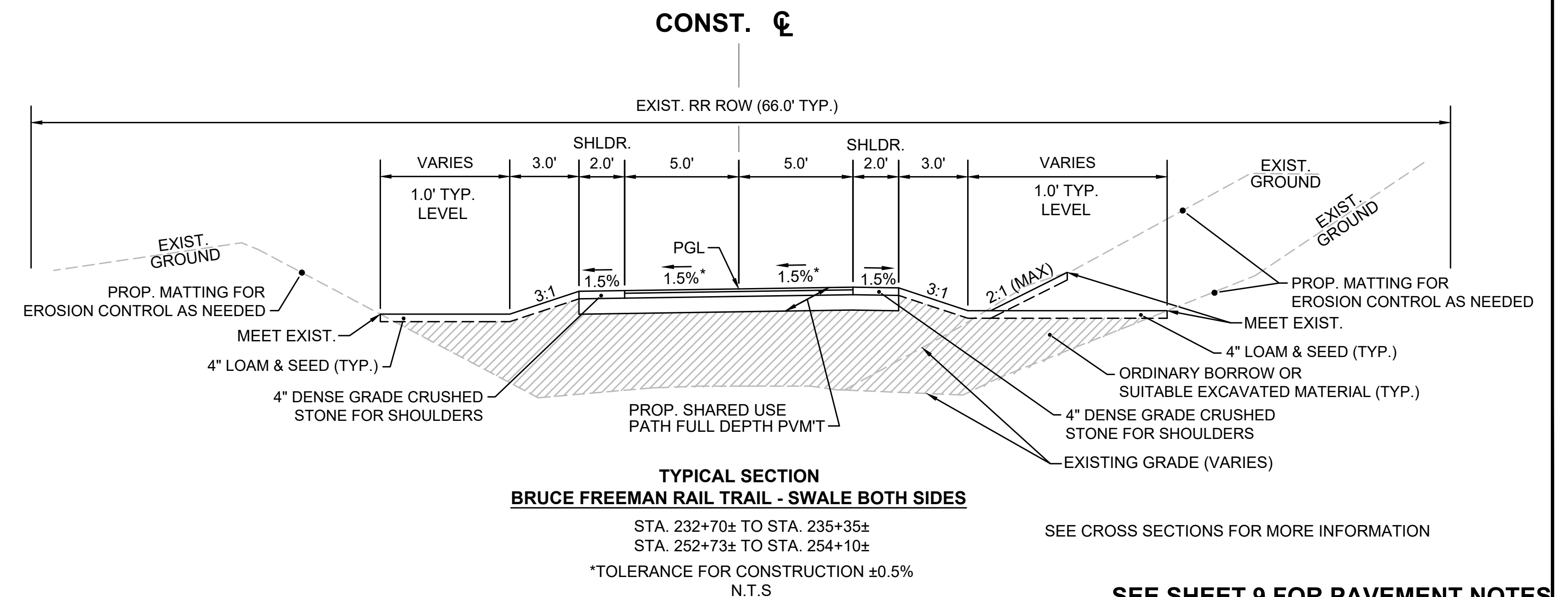
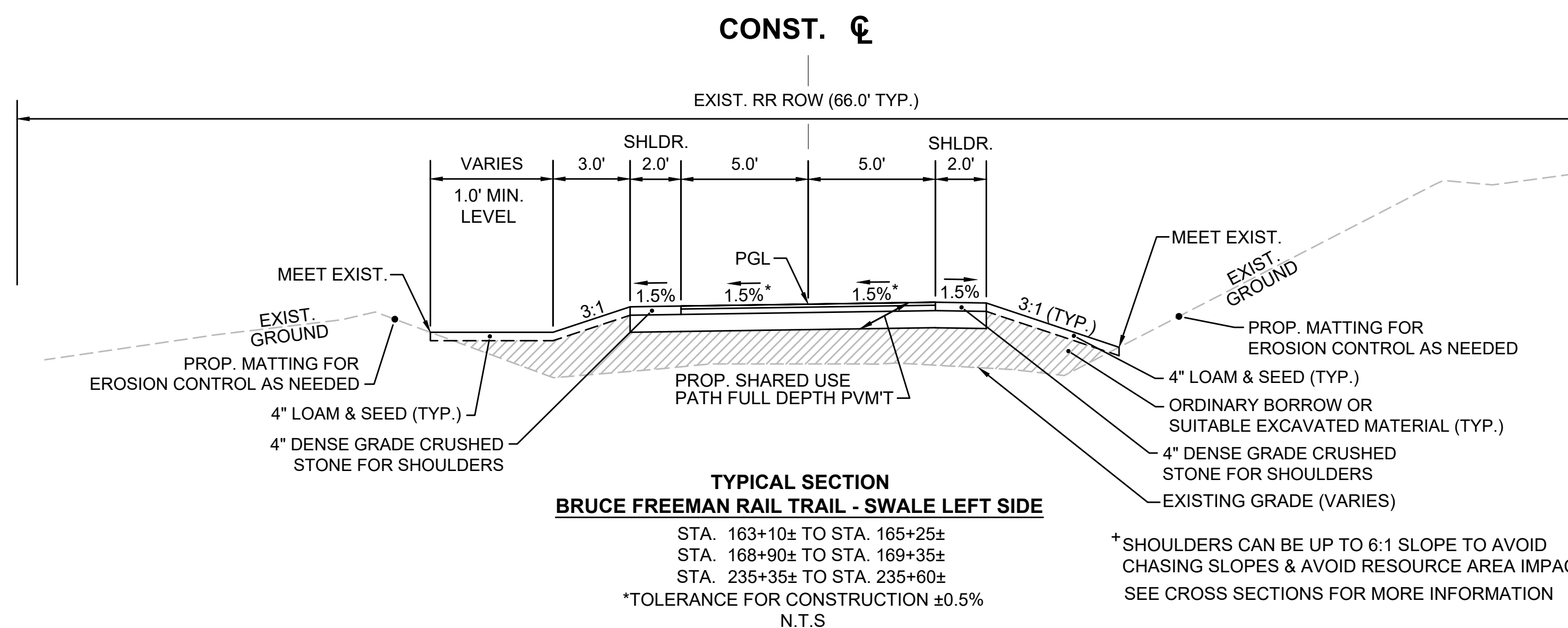
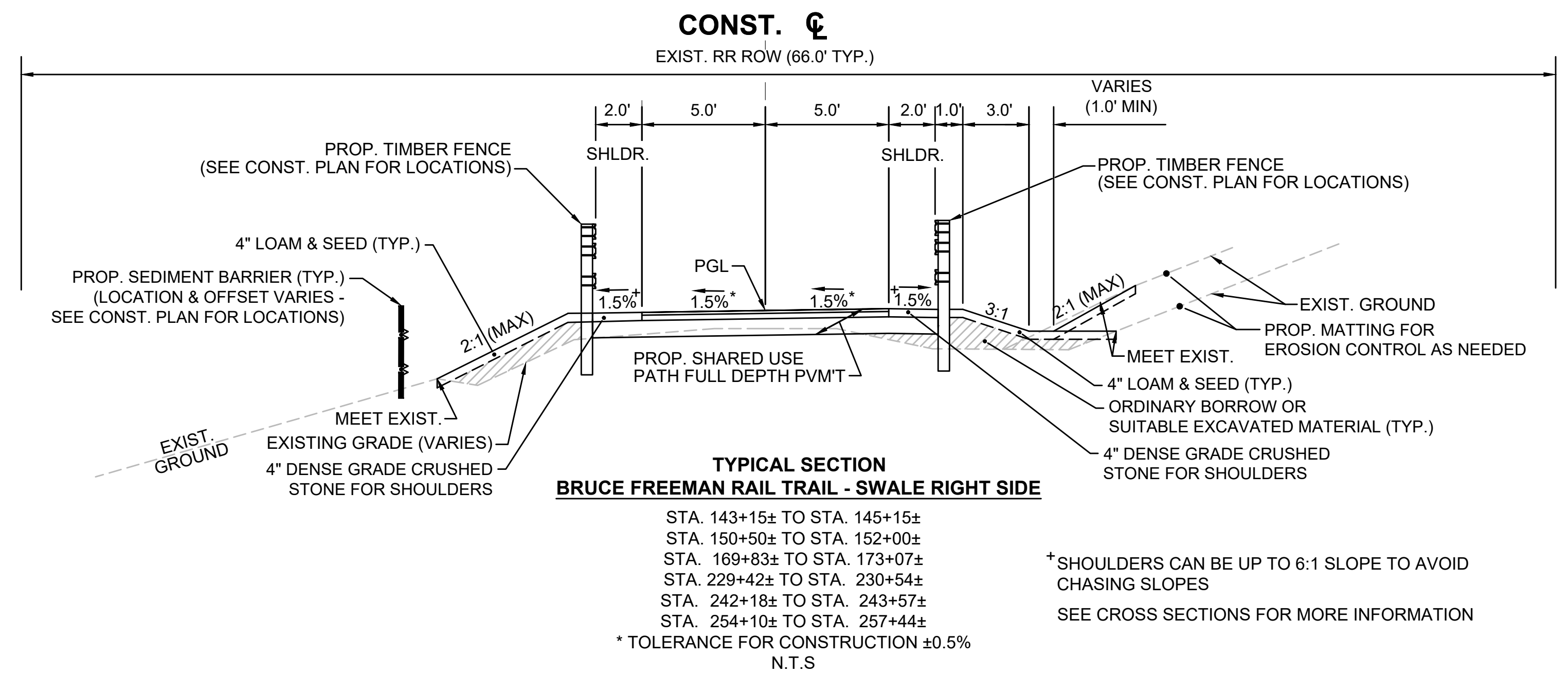
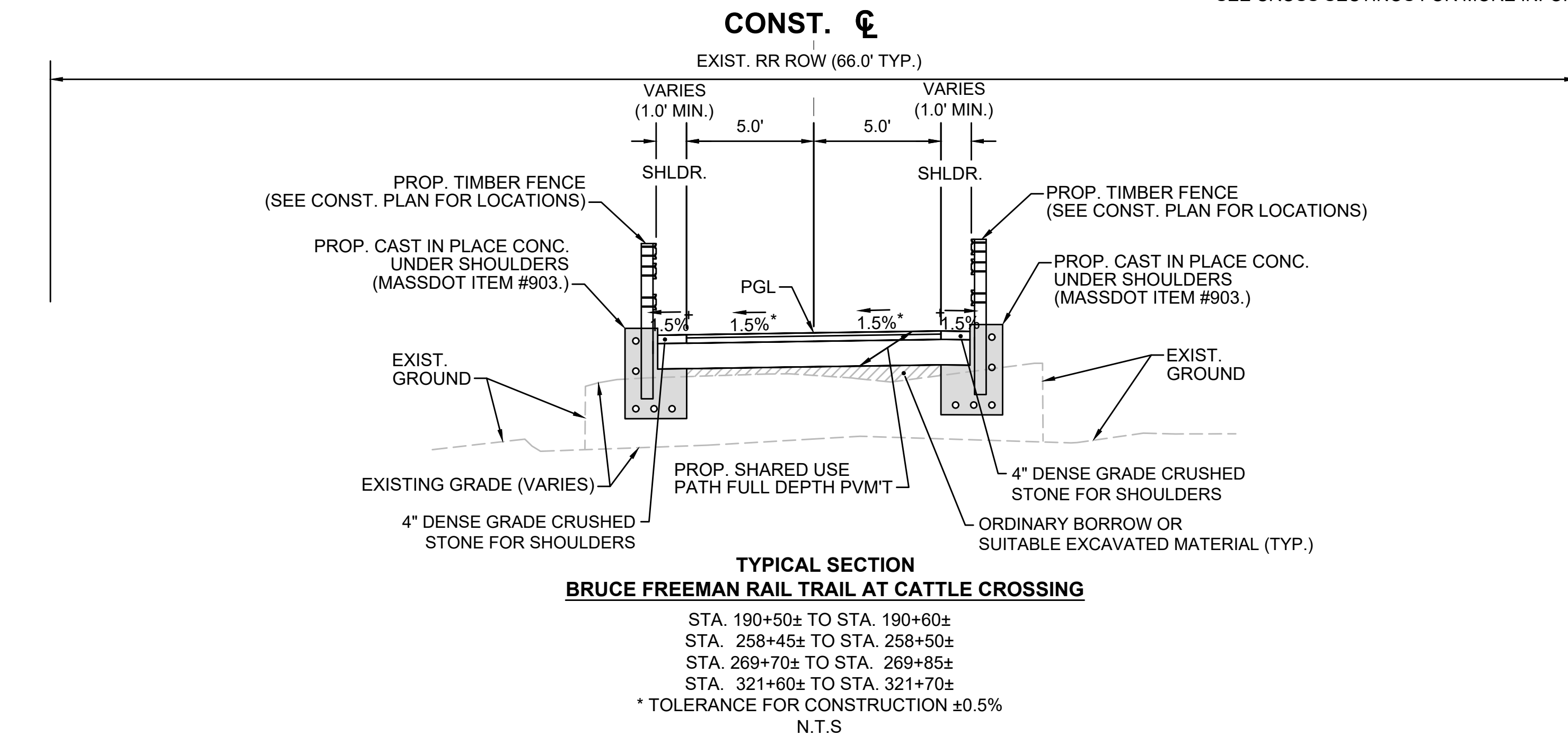
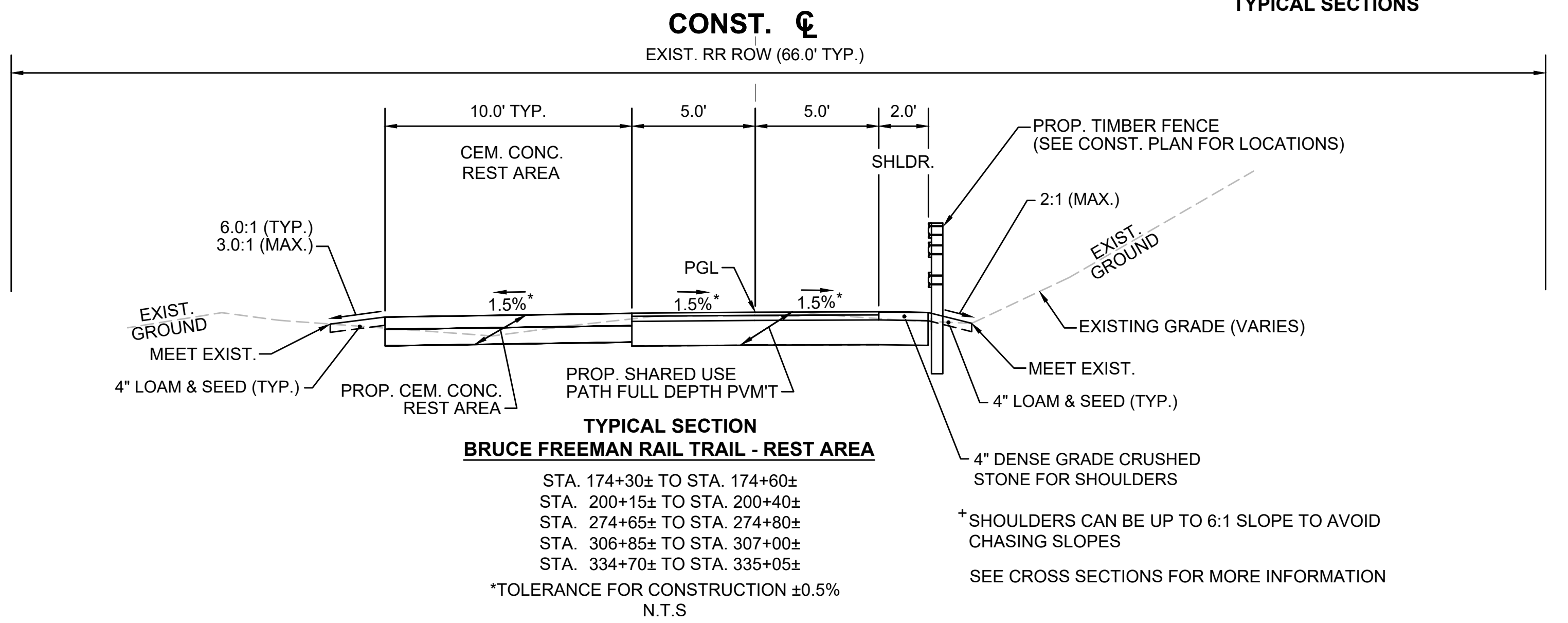
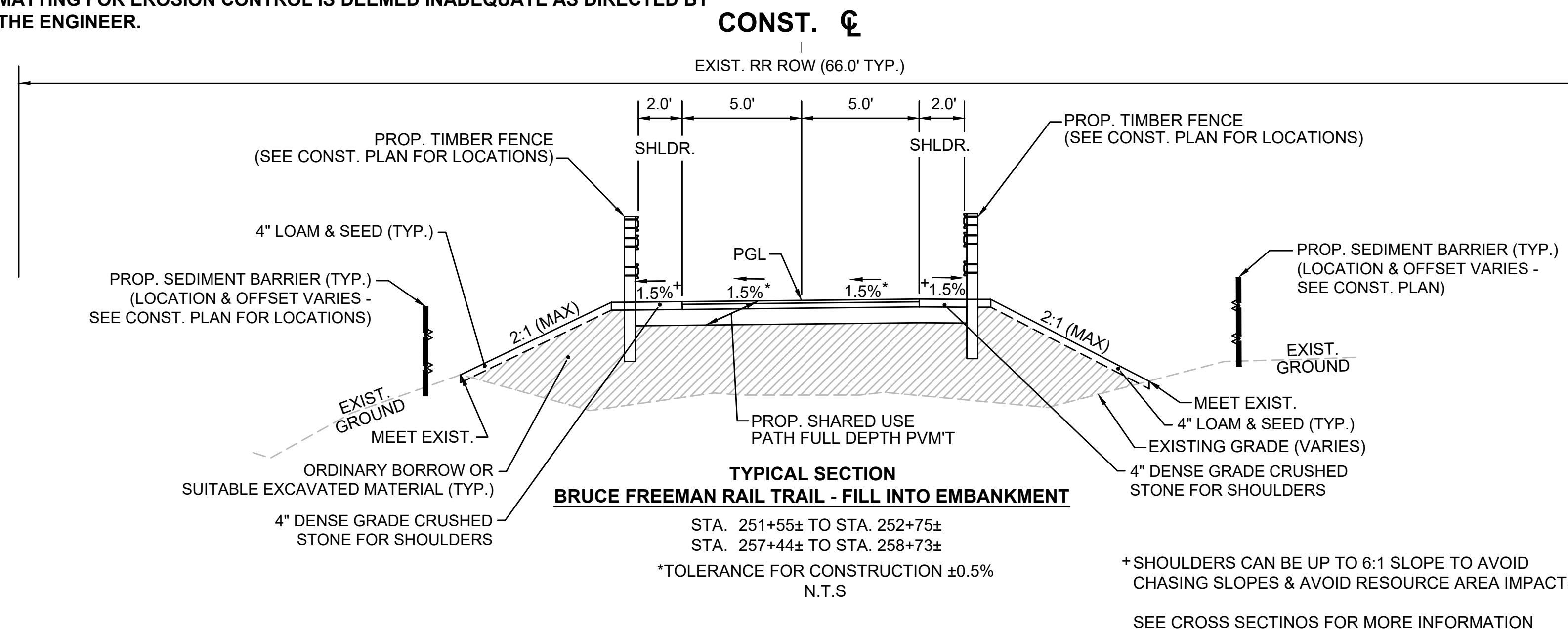
**NOTE:**  
 1. THE SECTIONS OF PROPOSED ROADWAY NOT COVERED IN THE RANGE OF STATIONS ASSOCIATED WITH THE TYPICAL SECTIONS ARE EITHER IN TRANSITION OR ARE LOCATED AT INTERSECTIONS AND THEREFOR HAVE NOT BEEN SHOWN. REFER TO CROSS SECTION SHEETS FOR MORE DETAILS.

**NOTES:**

- MATTING FOR EROSION CONTROL SHALL BE INSTALLED ON EXISTING SIDE SLOPES THAT ARE 2:1 OR STEEPER THAT ARE NOT WITHIN JUTE MESH WATERWAY AREAS.
- MODIFIED ROCK FILL SHALL BE USED AS NEEDED ON ANY CUT OR FILL SLOPES STEEPER THAN 2:1 WHERE POOR SOIL CONDITIONS EXIST AND MATTING FOR EROSION CONTROL IS DEEMED INADEQUATE AS DIRECTED BY THE ENGINEER.

SUBBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	10	318
PROJECT FILE NO.		608164	

**TYPICAL SECTIONS**

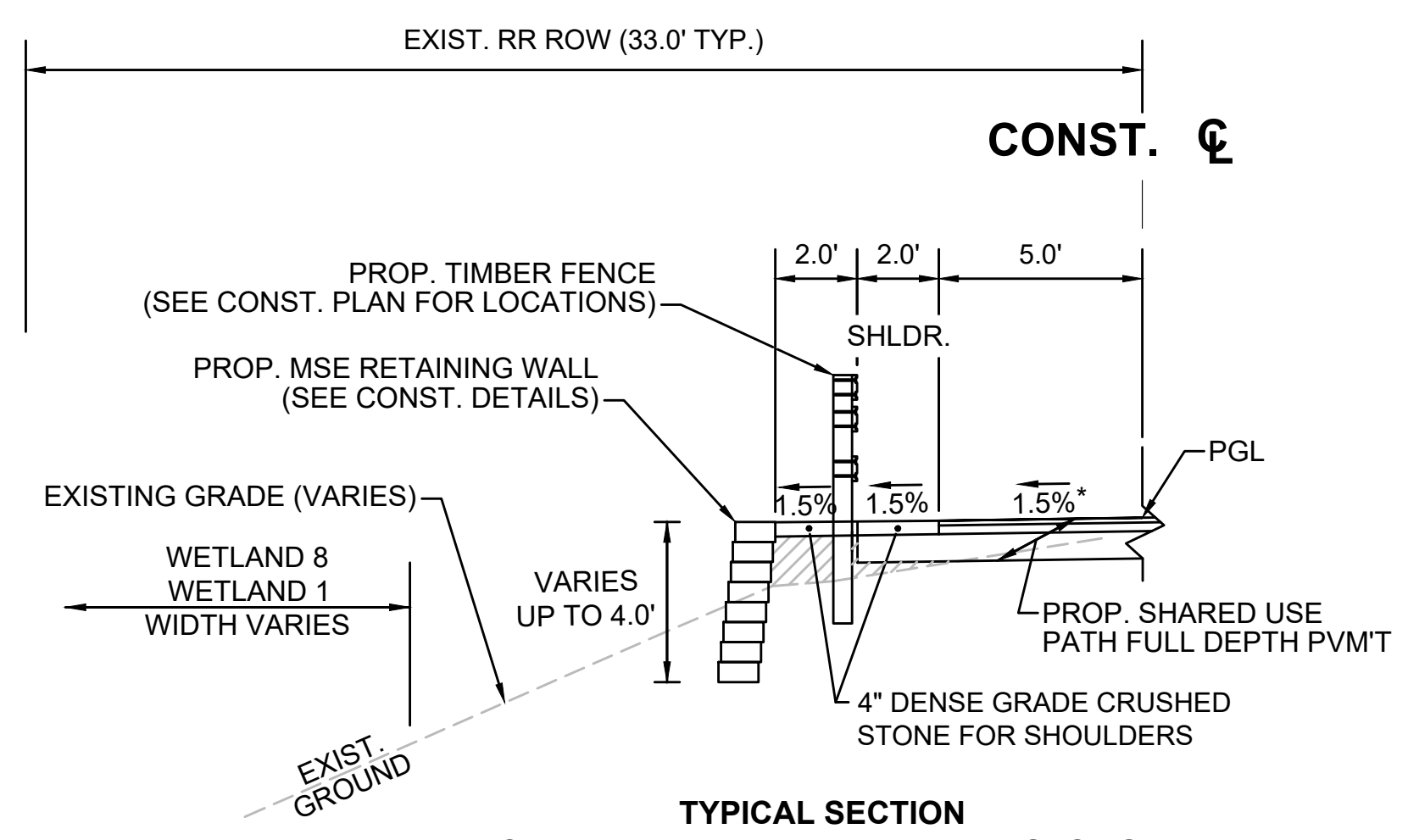


**SEE SHEET 9 FOR PAVEMENT NOTES**

**SUBURY  
BRUCE FREEMAN RAIL TRAIL**

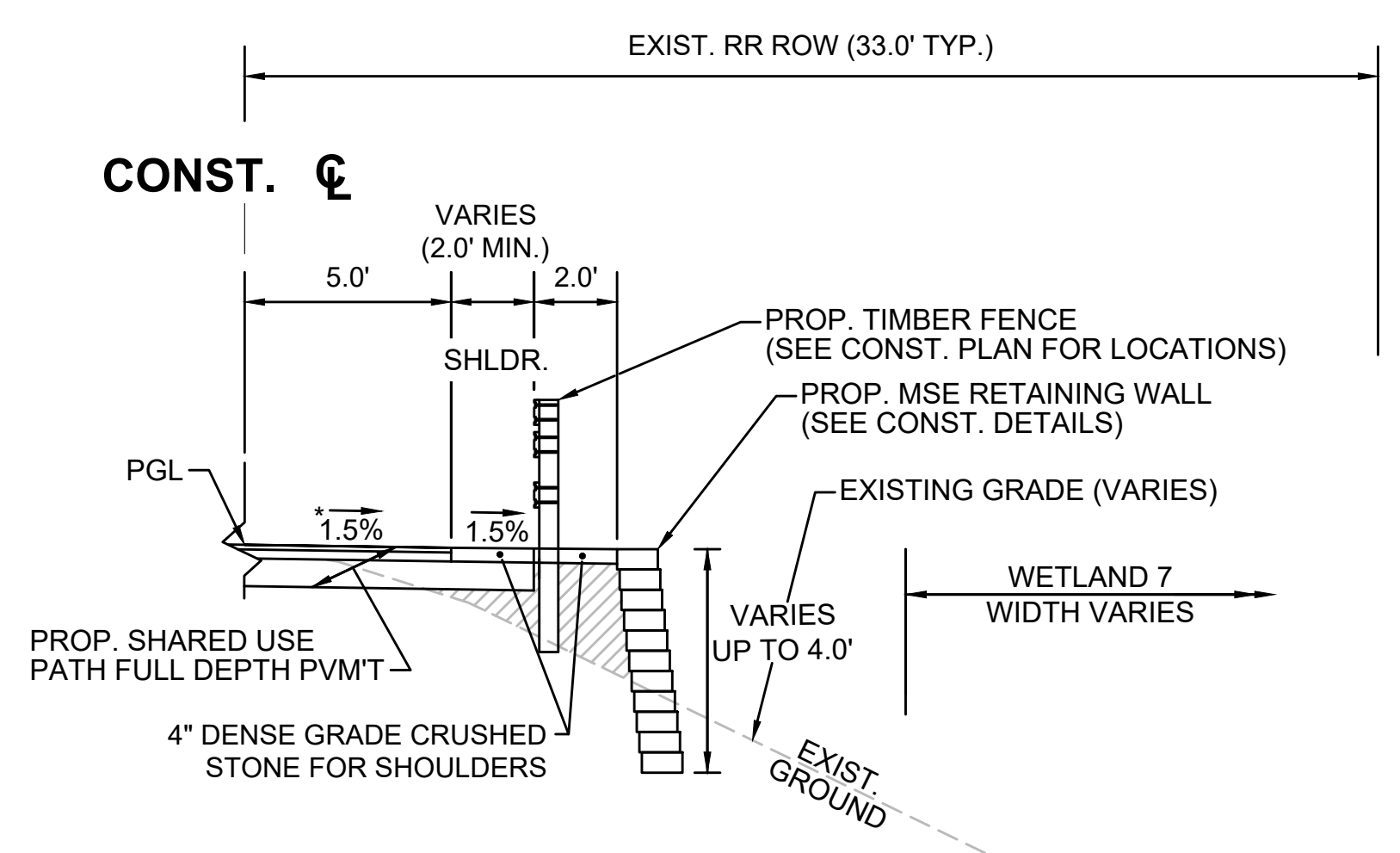
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	11	318
PROJECT FILE NO.		608164	

**TYPICAL SECTIONS**



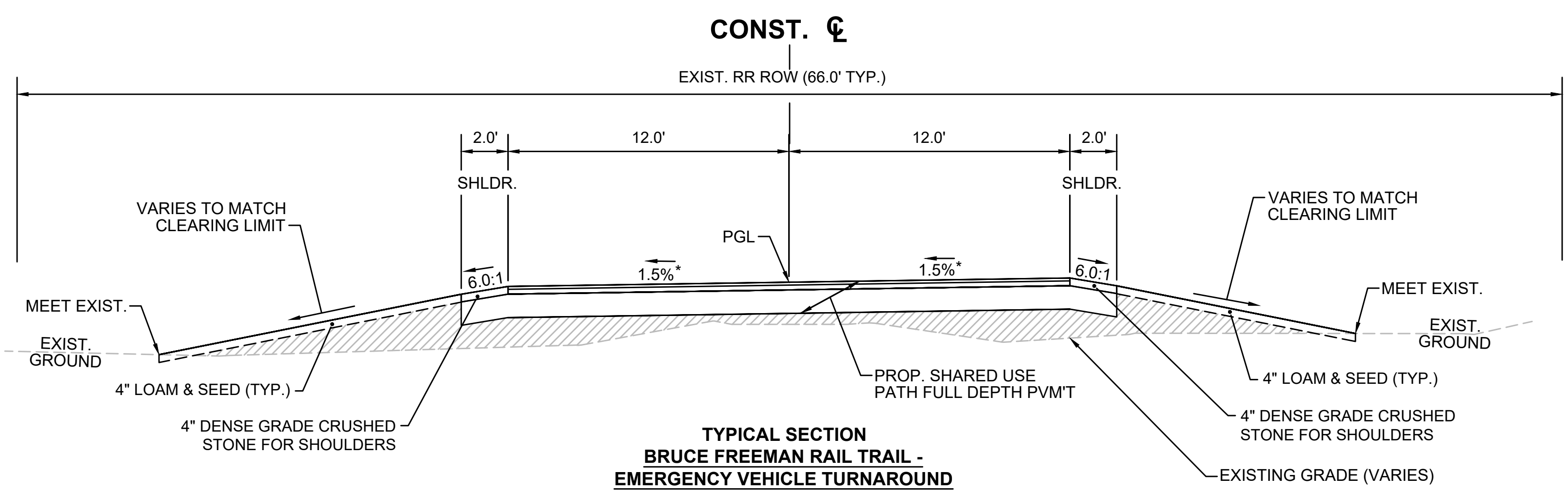
**TYPICAL SECTION  
BRUCE FREEMAN RAIL TRAIL - WALL SECTION LT**  
 STA. 283+85± TO STA. 284+30±  
 STA. 283+85± TO STA. 284+30±  
 STA. 332+35± TO STA. 332+90±  
 \*TOLERANCE FOR CONSTRUCTION ±0.5%  
 N.T.S.

SEE CROSS SECTIONS FOR MORE INFORMATION  
 SEE GRADING PLANS FOR ELEVATIONS

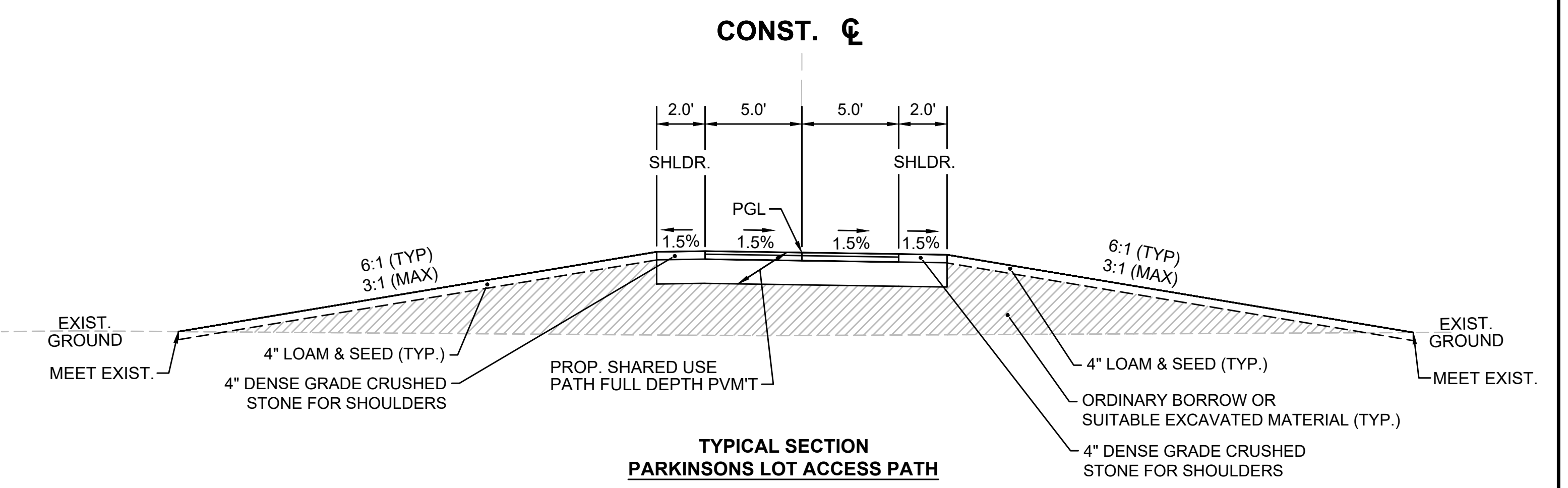


**TYPICAL SECTION  
BRUCE FREEMAN RAIL TRAIL - WALL SECTION RT**  
 STA. 284+65± TO STA. 285+25±  
 \*TOLERANCE FOR CONSTRUCTION ±0.5%  
 N.T.S.

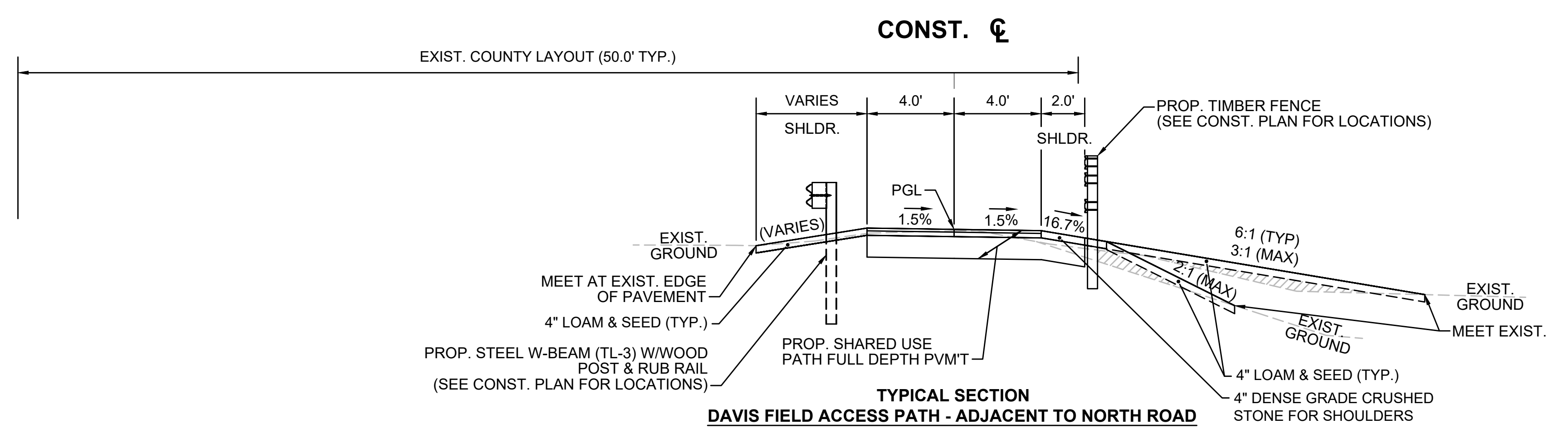
SEE CROSS SECTIONS FOR MORE INFORMATION  
 SEE GRADING PLANS FOR ELEVATIONS



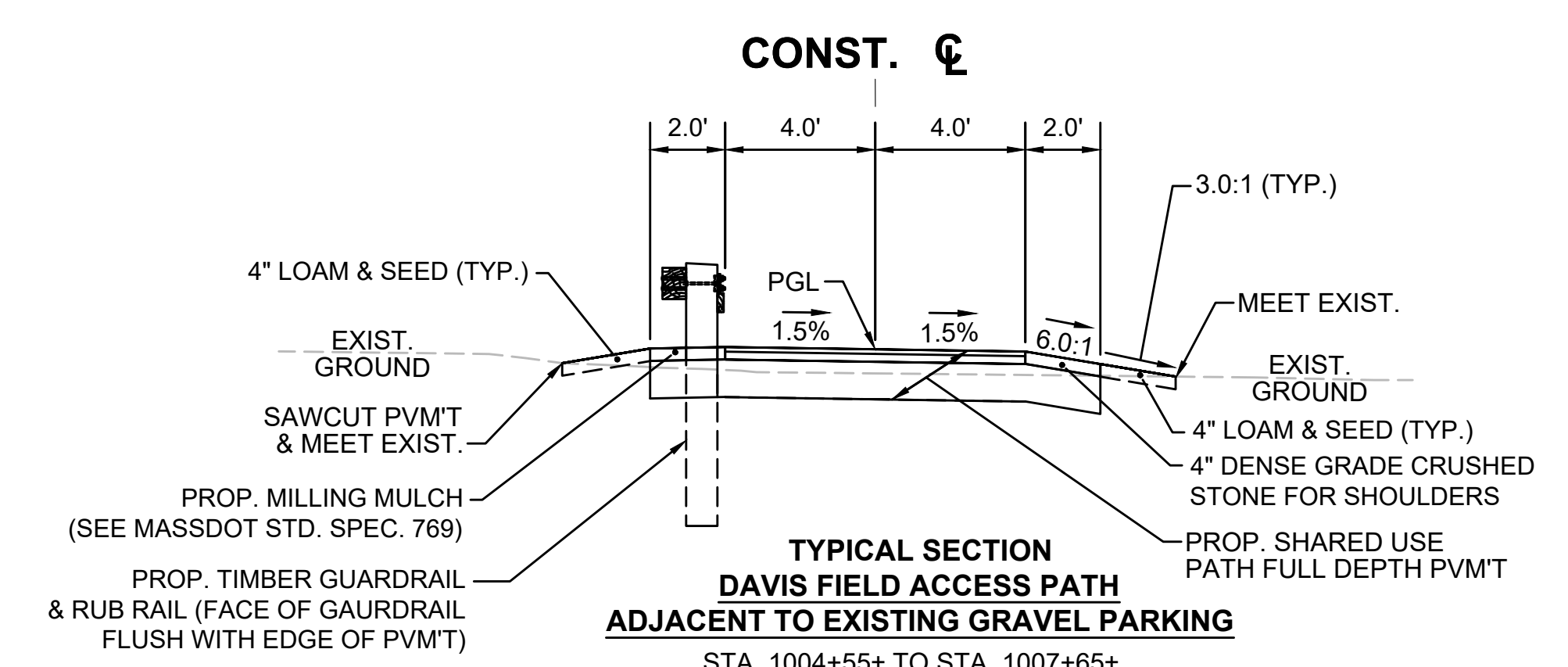
**TYPICAL SECTION  
BRUCE FREEMAN RAIL TRAIL -  
EMERGENCY VEHICLE TURNAROUND**  
 STA. 231+95± TO STA. 232+70±  
 STA. 250+80± TO STA. 251+55±  
 \*TOLERANCE FOR CONSTRUCTION ±0.5%  
 N.T.S.



**TYPICAL SECTION  
PARKINSONS LOT ACCESS PATH**  
 STA. 1010+00± TO STA. 1012+80±  
 \*TOLERANCE FOR CONSTRUCTION ±0.5%  
 N.T.S.



**TYPICAL SECTION  
DAVIS FIELD ACCESS PATH - ADJACENT TO NORTH ROAD**  
 STA. 1000+00± TO STA. 1004+55±  
 \*TOLERANCE FOR CONSTRUCTION ±0.5%  
 N.T.S.

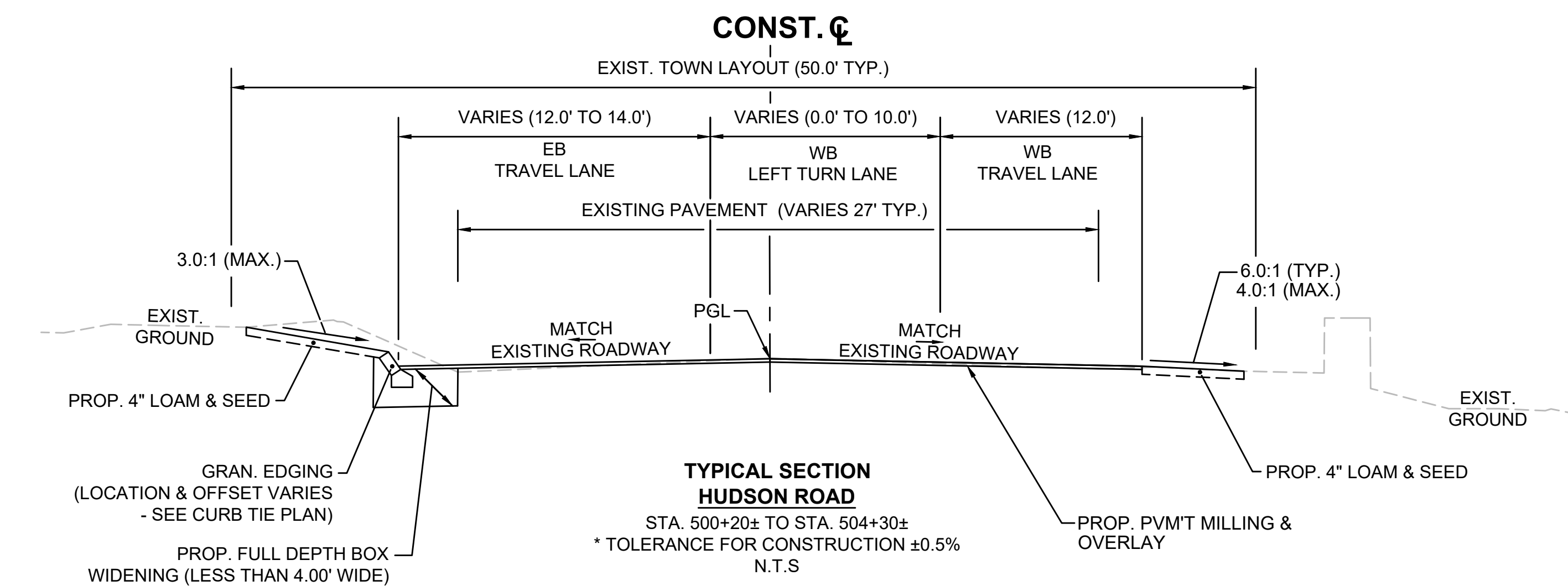
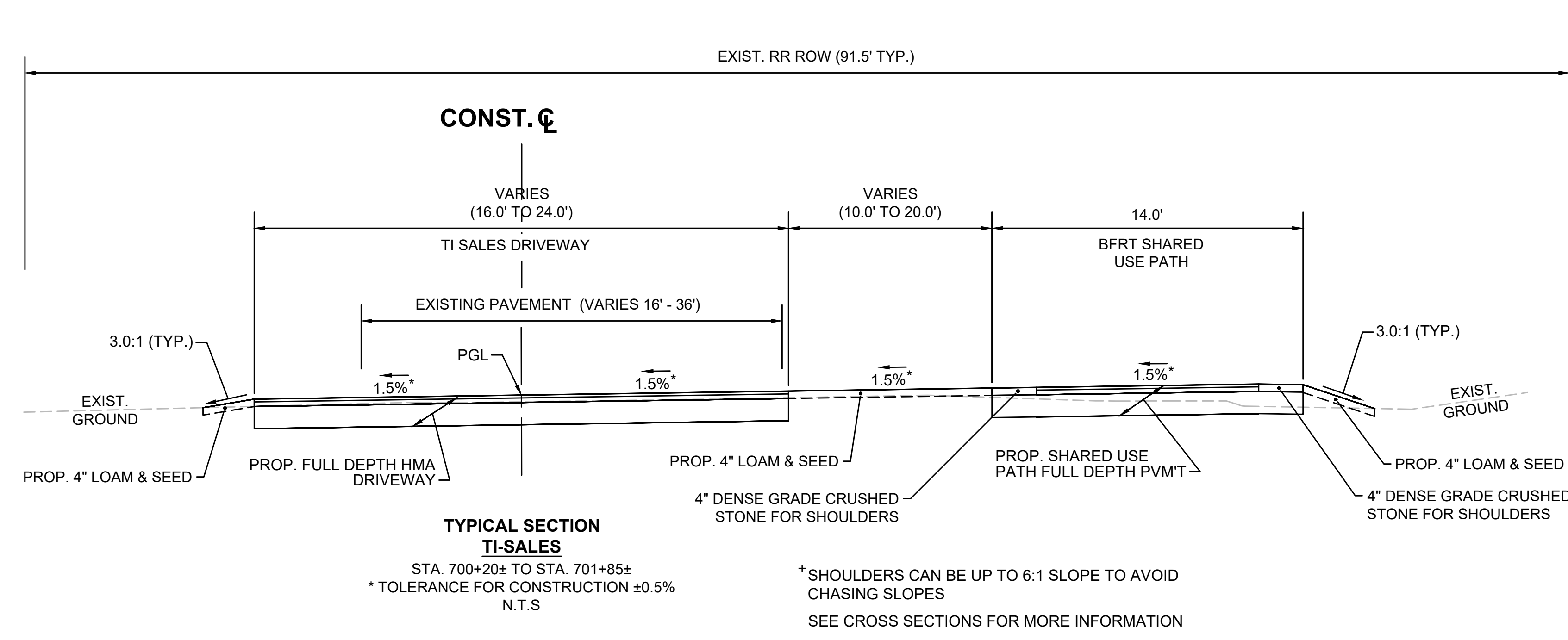
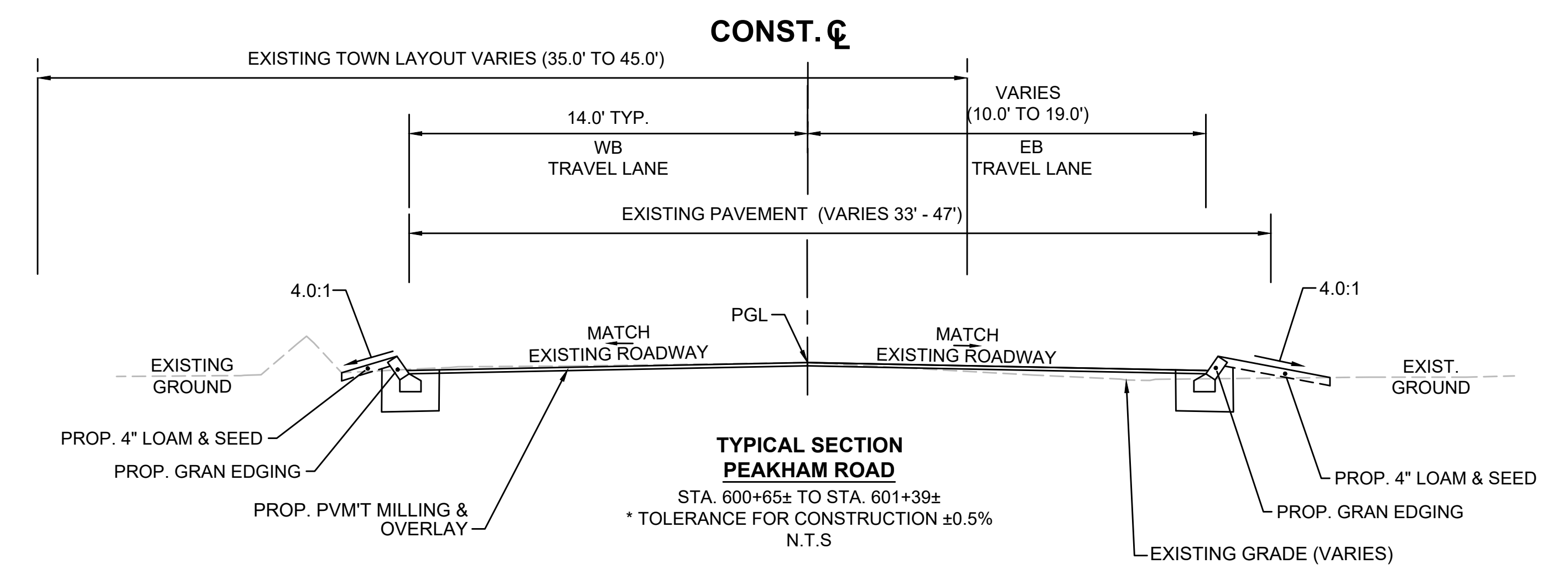
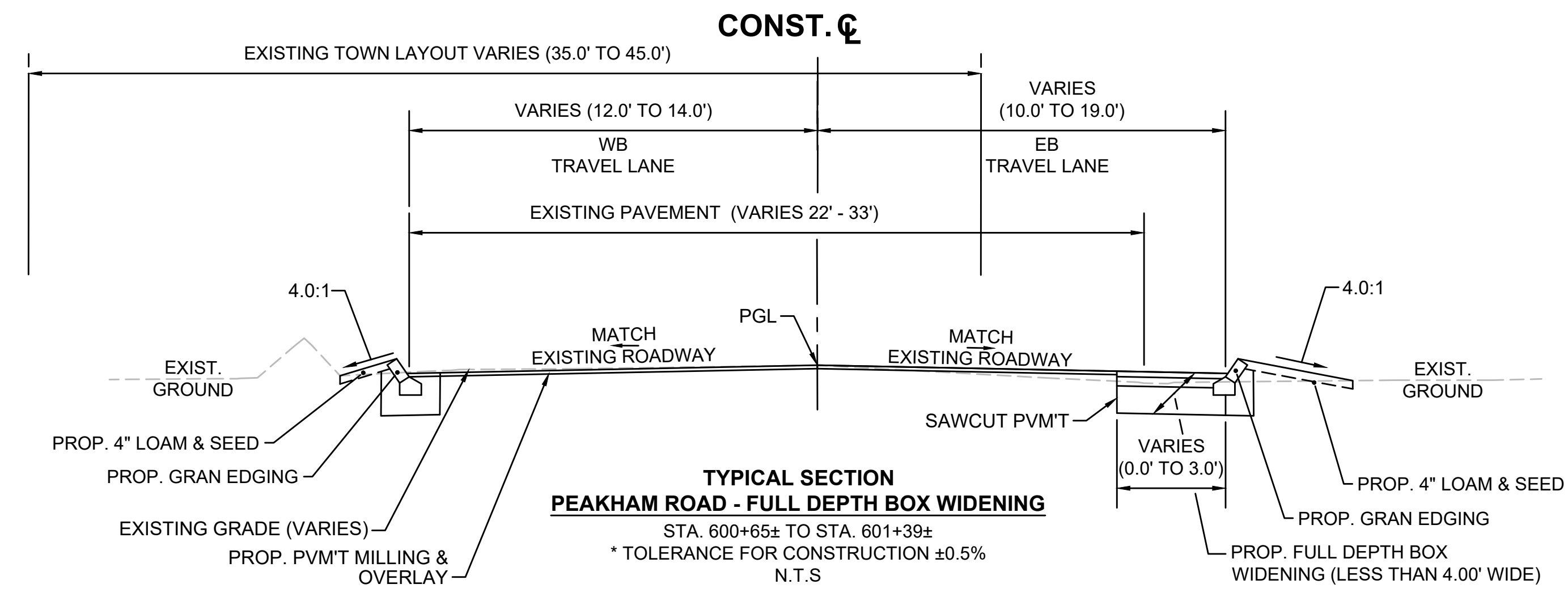
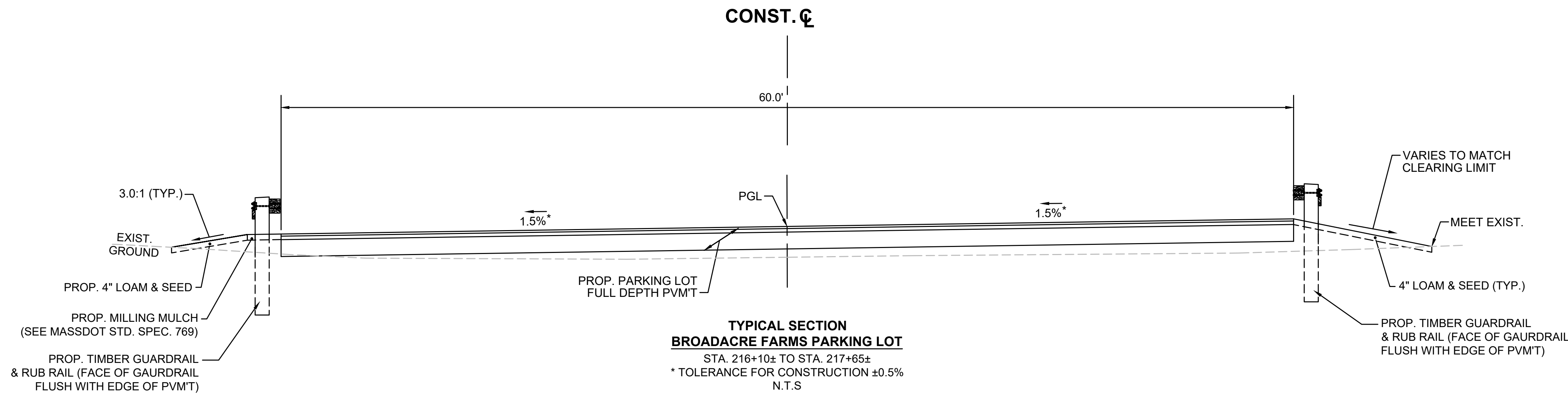


**TYPICAL SECTION  
DAVIS FIELD ACCESS PATH  
ADJACENT TO EXISTING GRAVEL PARKING**  
 STA. 1004+55± TO STA. 1007+65±  
 \*TOLERANCE FOR CONSTRUCTION ±0.5%  
 N.T.S.

SEE SHEET 9 FOR PAVEMENT NOTES

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	12	318
PROJECT FILE NO.		608164	

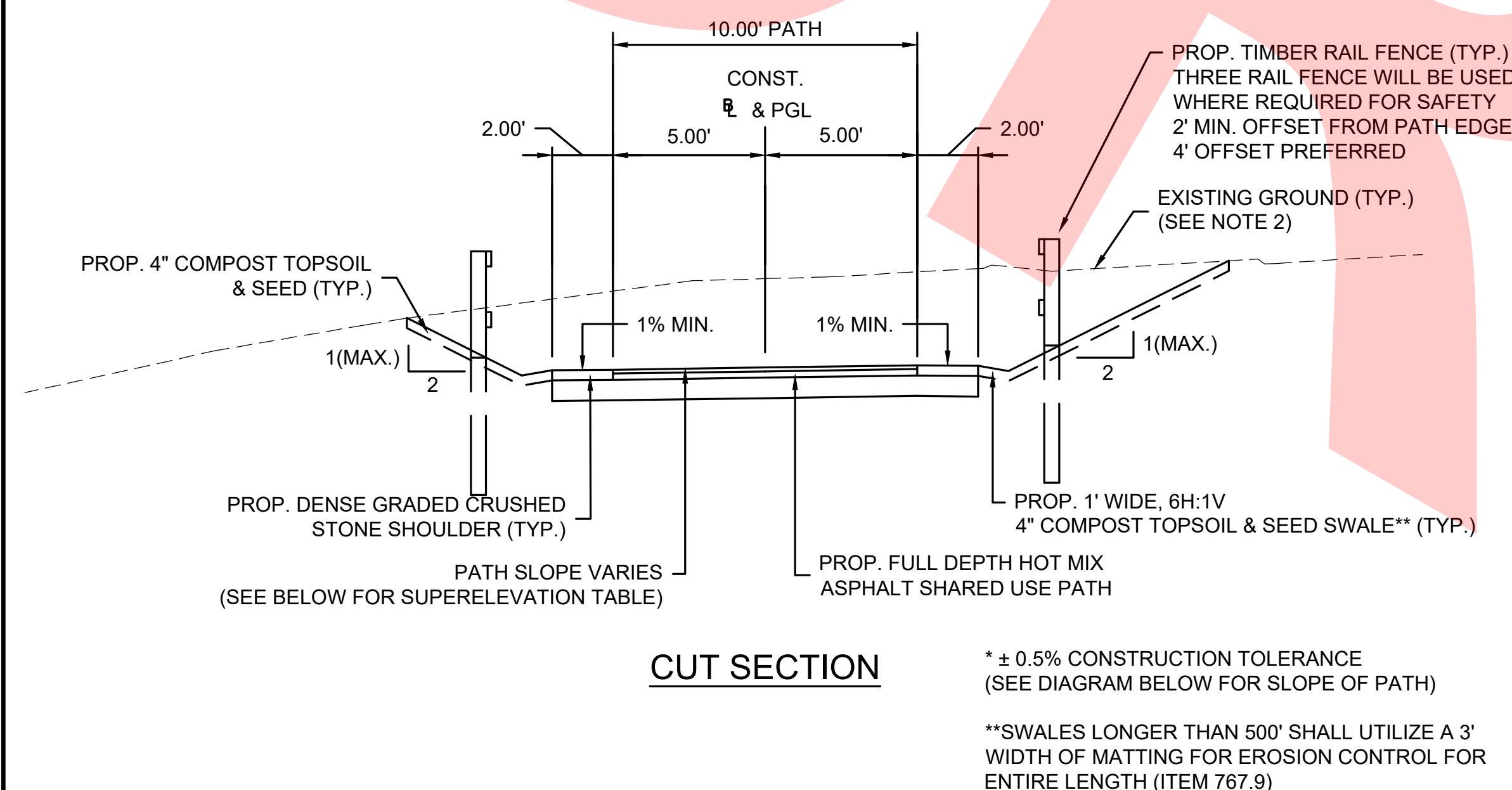
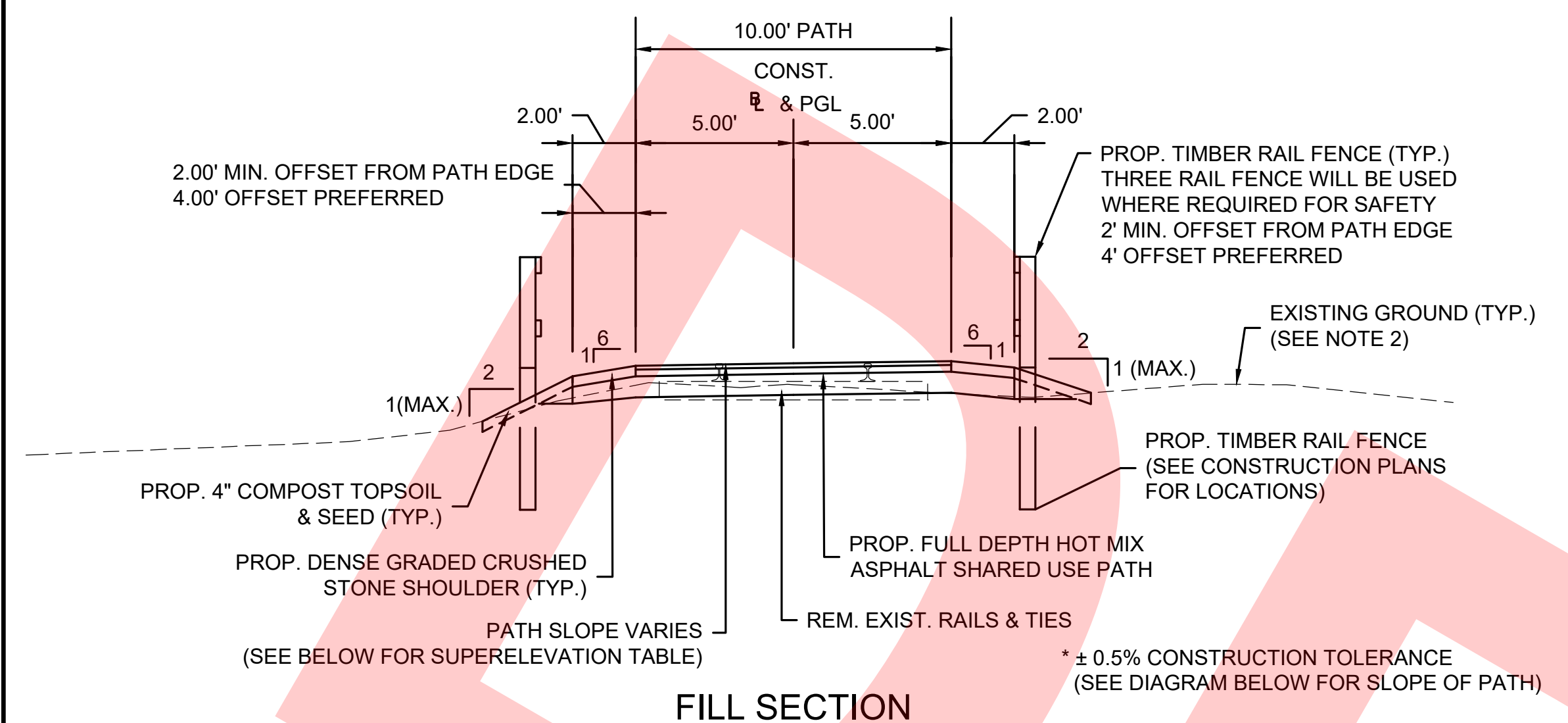
**TYPICAL SECTIONS**



SEE SHEET 9 FOR PAVEMENT NOTES

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	-	1	32
PROJECT FILE NO.		608164	

TYPICAL SECTIONS

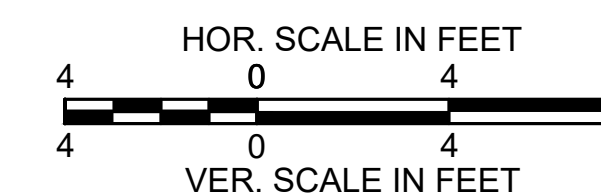
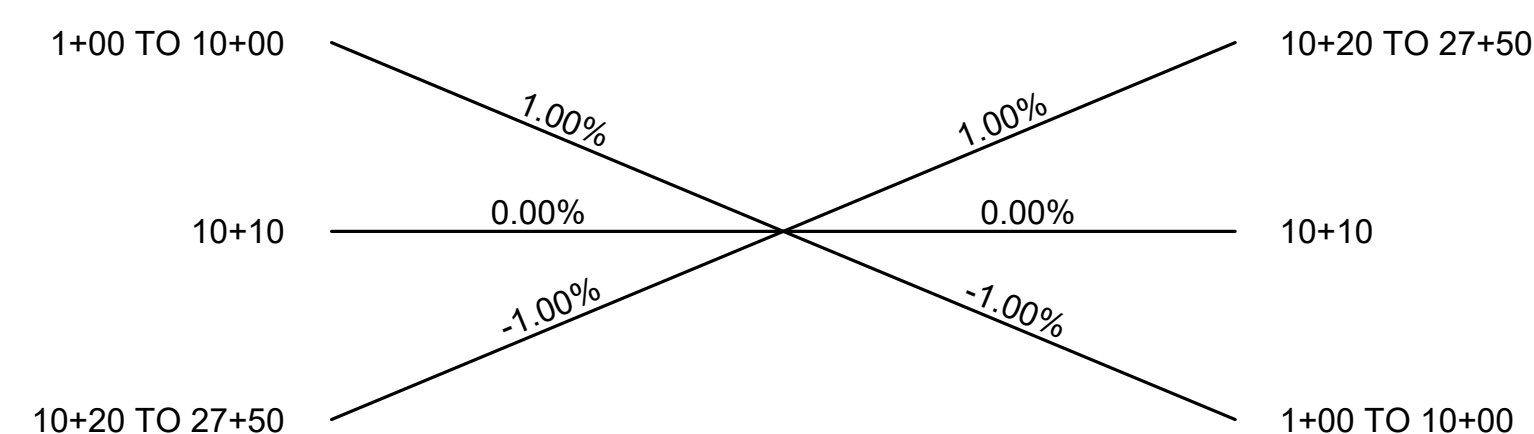


**TYPICAL SECTION - 10' WIDE PATH**

SCALE 1" = 4'  
STA. 1+00± - STA. 27+50±

**NOTES:**

- THE SECTIONS OF PROPOSED ROADWAY NOT COVERED IN THE RANGE OF STATIONS ASSOCIATED WITH THE TYPICAL SECTIONS ARE EITHER IN TRANSITION OR ARE LOCATED AT INTERSECTIONS AND THEREFORE HAVE NOT BEEN SHOWN. REFER TO CROSS SECTION SHEETS FOR MORE DETAILS.
- CONTRACTOR SHALL REMOVE EXISTING ORGANIC MATERIAL (ABOVE RAILROAD BALLAST) THAT IS UNSUITABLE FOR REUSE AND DISPOSE OF OUTSIDE OF THE PROJECT LIMITS. IT IS ASSUMED THAT ALL OTHER EXCAVATED SUITABLE MATERIAL WILL REMAIN WITHIN THE PROJECT LIMITS. SEE SPECIAL PROVISIONS FOR DETAILS.



FENCING DETAILS

TIMBER FENCE (TWO-RAIL) 103+55 RT TO STA. 104+25 RT  
 TIMBER FENCE (TWO-RAIL) 104+80 LT TO STA. 106+70 LT

HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

DRAINAGE DETAILS

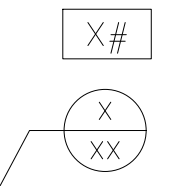
NONE

NOTES:

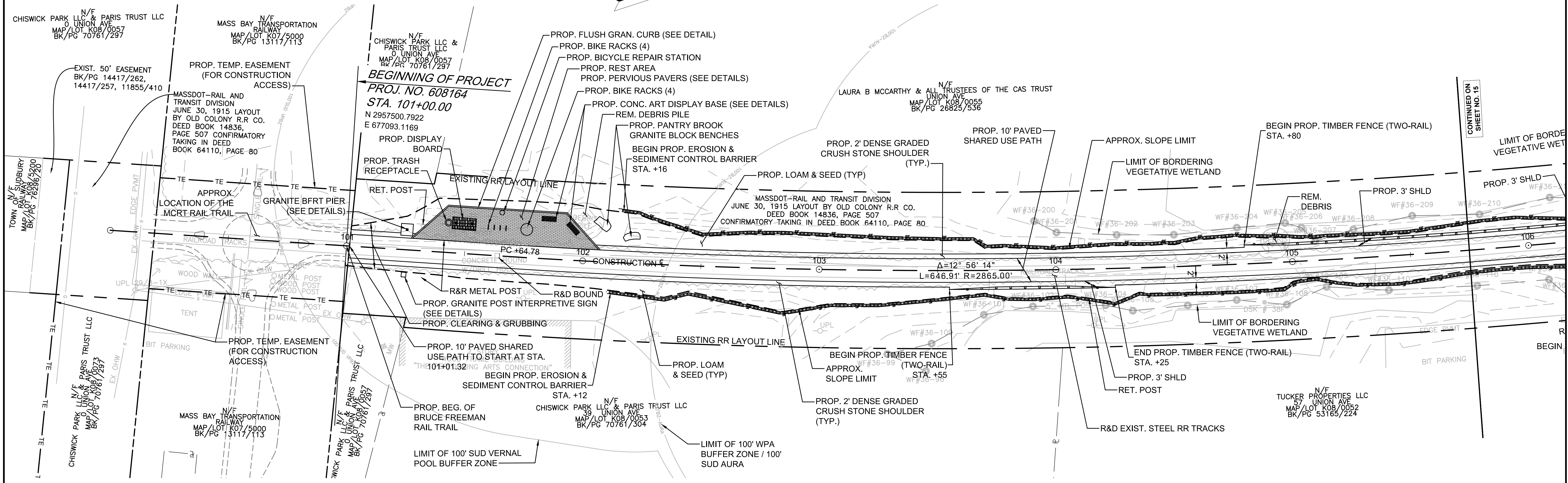
1. REMOVE AND DISCARD ALL EXISTING RAILS LOCATED WITHIN THE PROJECT LIMITS. COST FOR R&D SHALL BE PAID UNDER ITEM 129.5 TRACK EXCAVATION.
2. SEE TYPICAL SECTIONS FOR FORMATION OF SHARED USE PATH SHOULDERS AND MATERIAL SPECIFICATIONS
3. ALL EXISTING STRUCTURES WITHIN CURB RAMP AREAS SHALL BE ADJUSTED TO BE FLUSH WITH CURB RAMP SURFACE.
4. ALL TREES WITHIN SLOPE LIMITS/CLEARING LIMITS SHALL BE REMOVED UNLESS OTHERWISE NOTED ON PLANS. PAYMENT FOR TREE REMOVALS IN THESE AREAS SHALL BE PAID FOR UNDER CLEARING & GRUBBING ITEM PER STANDARD SPECIFICATIONS.
5. ALL TREES OUTSIDE OF SLOPE LIMITS/CLEARING LIMITS SHALL BE RETAINED.

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL #  
 PLANT QUANTITY AND SPECIES

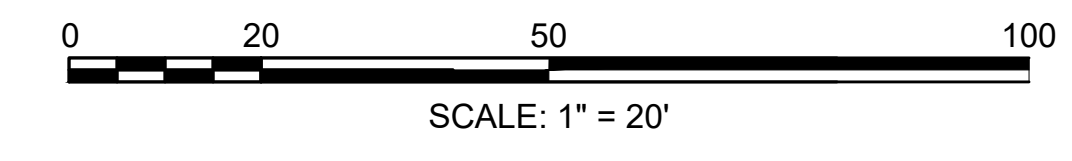


SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	14	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			



PROJECT SUMMARY PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<u>TREES</u>				
AR	ACER RUBRUM 'OCTOBER GLORY'	OCTOBER GLORY MAPLE	17	2-2.5" CAL.
AS	ACER SACCHARUM	LEGACY SUGAR MAPLE	27	2-2.5" CAL.
QR	QUERCUS RUBRA	NORTHERN RED OAK	11	2-2.5" CAL.
QA	QUERCUS ALBA	WHITE OAK	9	2-2.5" CAL.
TA	TILIA AMERICANA	AMERICAN LINDEN	99	2"-2.5" CAL.
PS	PINUS STROBUS	WHITE PINE	87	5-6" HT.
PG	PICEA GLAUCA	WHITE SPRUCE	29	4-5" HT.
MA	MALUS 'IVORY SPEAR'	IVORY SPEAR CRABAPPLE	3	2.5"-3" CAL.
<u>SHRUBS</u>				
IG	ILEX GLABRA	INKBERRY	72	2-3' HT.
PV	PRUNUS VERGINIANA	CHOKEBERRY	36	2-3' HT.
CLA	CLETHRA ALNIFOLIA	SUMMERSWEET	63	18"-24" HT.
ILX	ILEX VERTICILLATA	WINTERBERRY	279	18"-24" HT.
VAC	VGACCINIUM CORYMBOSUM	HIGHBUSH BLUEBERRY	53	18"-24" HT.
<u>SEED MIX</u>				
LOW UPLAND SEED MIX			26,340 SQ. FT.	1 LB/ 2,500 SQ. FT
WETLAND/ BASIN MIX (SEASONALLY FLOODED)			440 SQ. FT.	1 LB/ 2,500 SQ. FT



FOR PROFILE SEE SHEET NO. 49

**FENCING DETAILS**

TIMBER FENCE (THREE-RAIL) STA. 107+15 RT TO STA. 108+38 RT  
 TIMBER FENCE (TWO-RAIL) STA. 104+80 LT TO STA. 106+70 LT  
 TIMBER FENCE (THREE-RAIL) STA. 108+88 RT TO STA. 109+30 RT  
 TIMBER FENCE (THREE-RAIL) STA. 109+00 LT TO STA. 109+85 LT  
 STOCKADE FENCE STA. 113+52 LT TO STA. 115+55 LT  
 TIMBER FENCE (TWO-RAIL) STA. 115+55 LT TO STA. 115+85 LT  
 TIMBER FENCE (TWO-RAIL) STA. 116+50 LT TO STA. 116+80 LT  
 STOCKADE FENCE STA. 116+80 LT TO STA. 117+50 LT  
 STOCKADE FENCE STA. 113+93 RT TO STA. 115+37 RT  
 TIMBER FENCE (TWO-RAIL) STA. 115+37 RT TO STA. 115+67 RT

**HIGHWAY GUARD DETAILS**

NONE  
 TRAFFIC SIGNAL CONDUIT  
 NONE

**WATER SUPPLY ALTERATIONS**

NONE  
 DRAINAGE DETAILS  
 NONE

**PLANT LIST**

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
PS	PINUS STROBUS	WHITE PINE	4	5-6" HT.
<b>SHRUBS</b>				
IG	ILEX GLABRA	INKBERRY	11	2-3" HT.
ILX	ILEX VERTICILLATA	WINTERBERRY	42	18"-24" HT.
VAC	VGACCINIUM CORYMBOSUM	HIGHBUSH BLUEBERRY	18	18"-24" HT.

**LEGEND:**

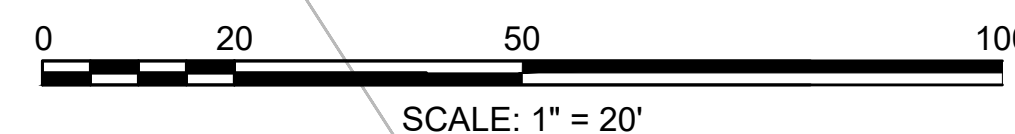
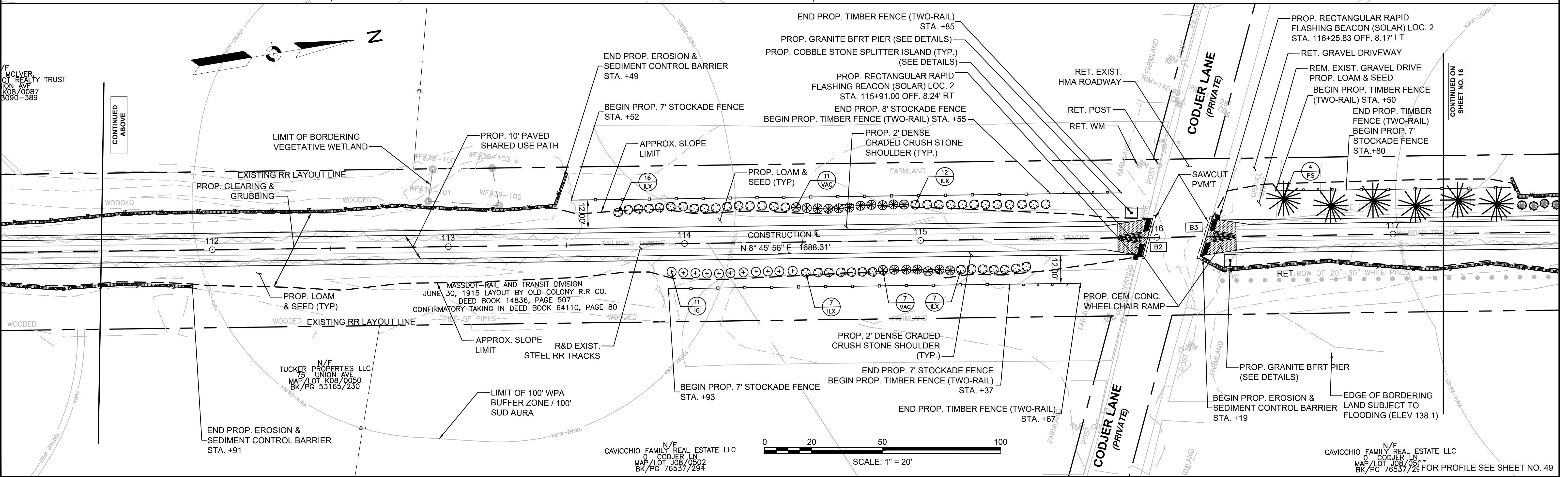
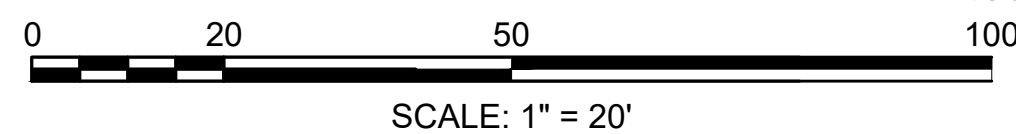
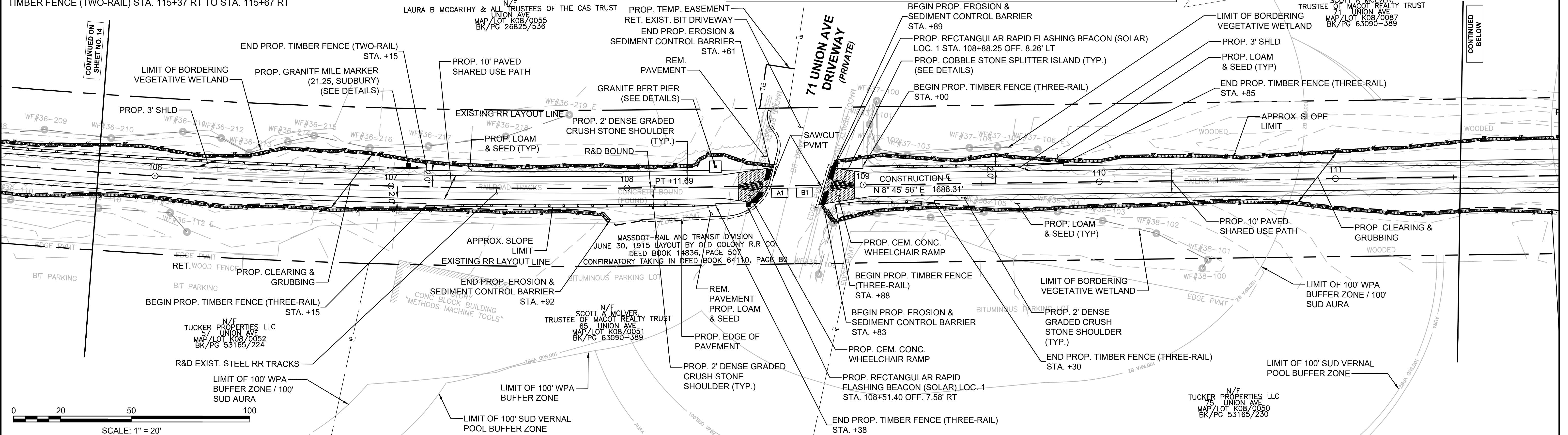
PROPOSED WHEELCHAIR RAMP DETAIL # X#  
 PLANT QUANTITY AND SPECIES X XX

**SUDBURY BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	15	318

PROJECT FILE NO. 608164

**CONSTRUCTION PLANS**



**FENCING DETAILS**

STOCKADE FENCE STA. 116+80 LT TO STA. 117+50 LT  
 STOCKADE FENCE STA. 116+61 RT TO STA. 117+50 RT  
 TIMBER FENCE (THREE-RAIL) STA. 119+75 LT TO STA. 132+25 LT  
 TIMBER FENCE (THREE-RAIL) STA. 119+75 RT TO STA. 131+75 RT

**HIGHWAY GUARD DETAILS**

NONE

**WATER SUPPLY ALTERATIONS**

NONE

**TRAFFIC SIGNAL CONDUIT**

NONE

**DRAINAGE DETAILS**

NONE

**PLANT LIST**

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
AS	ACER SACCHARUM	LEGACY SUGAR MAPLE	8	2-2.5" CAL.
QA	QUERCUS ALBA	WHITE OAK	1	2-2.5" CAL.
TA	TILIA AMERICANA	AMERICAN LINDEN	10	2"-2.5" CAL.
PS	PINUS STROBUS	WHITE PINE	10	5-6' HT.
PG	PICEA GLAUCA	WHITE SPRUCE	17	4-5' HT.
<b>SHRUBS</b>				
IG	ILEX GLABRA	INKBERRY	9	2-3' HT.

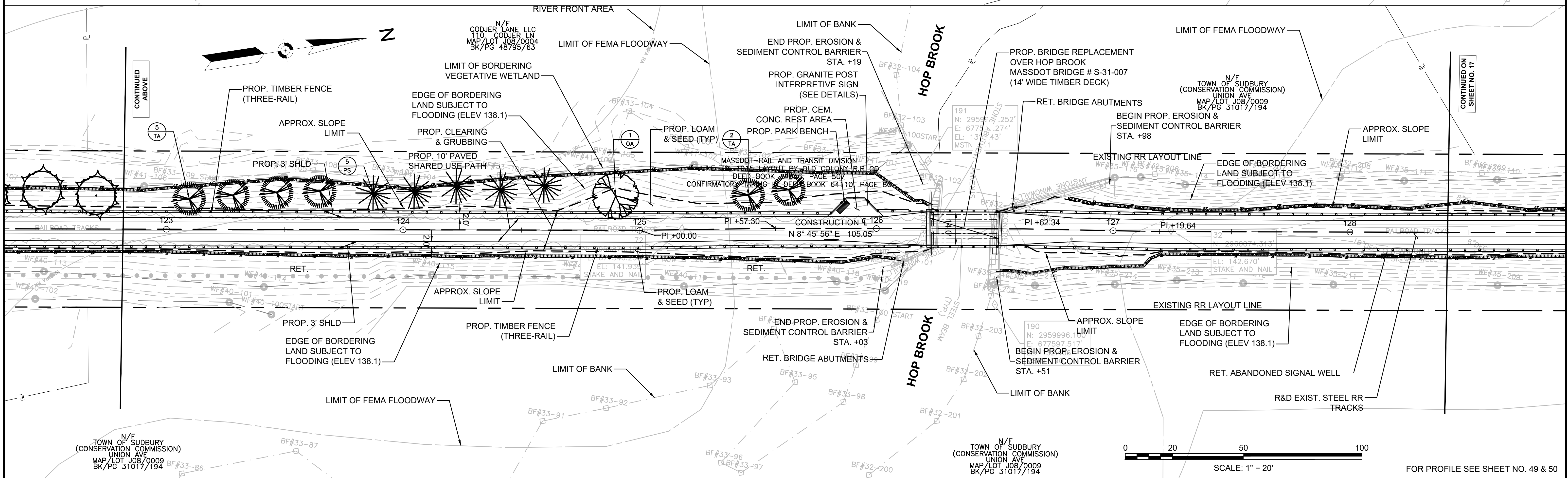
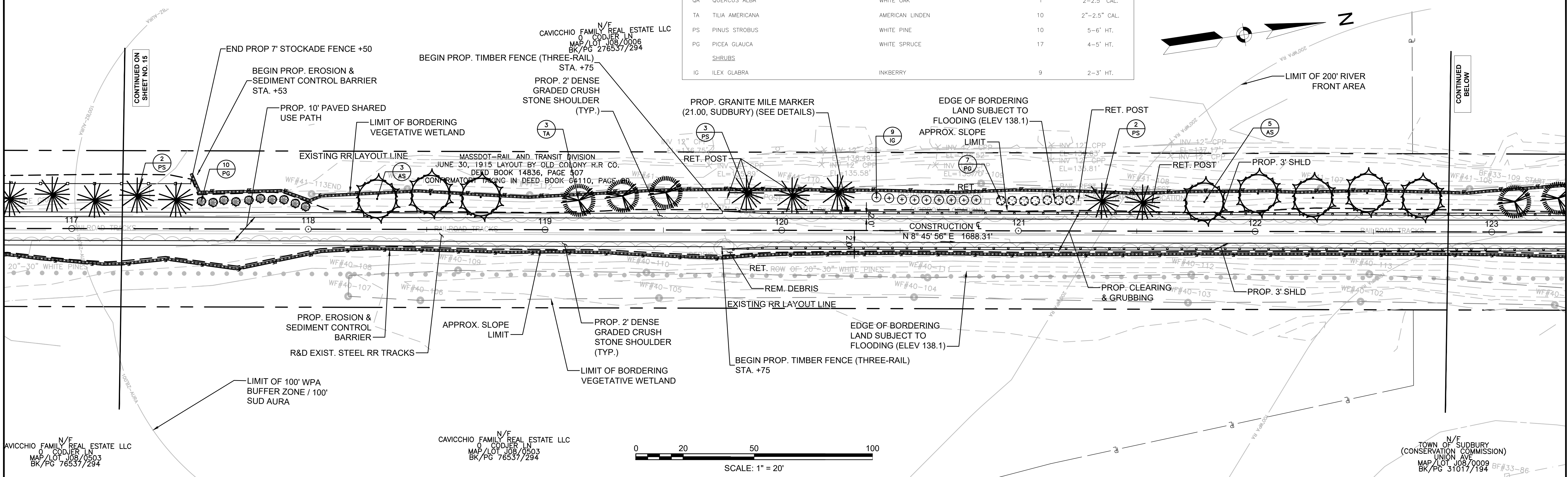
**LEGEND:**

PROPOSED WHEELCHAIR RAMP DETAIL # X#  
 PLANT QUANTITY AND SPECIES X  
 XX

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	16	318
PROJECT FILE NO.		608164	

**CONSTRUCTION PLANS**





FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 119+75 LT TO STA. 132+25 LT  
 TIMBER FENCE (THREE-RAIL) STA. 119+75 RT TO STA. 131+75 RT

HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

DRAINAGE DETAILS

NONE

PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
AR	ACER RUBRUM 'OCTOBER GLORY'	OCTOBER GLORY MAPLE	7	2-2.5" CAL.
QA	QUERCUS ALBA	WHITE OAK	6	2-2.5" CAL.
TA	TILIA AMERICANA	AMERICAN LINDEN	18	2"-2.5" CAL.
PS	PINUS STROBUS	WHITE PINE	33	5-6" HT.
<b>SHRUBS</b>				
ILX	ILEX VERTICILLATA	WINTERBERRY	4	18"-24" HT.
VAC	VGACCINIUM CORYMBOSUM	HIGHBUSH BLUEBERRY	7	18"-24" HT.

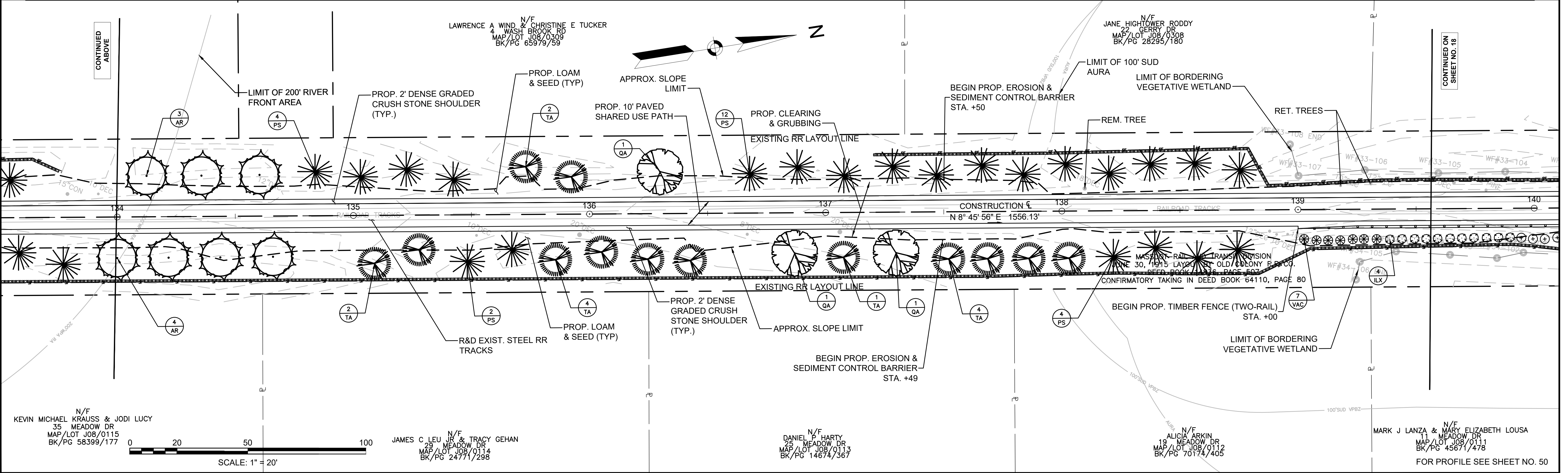
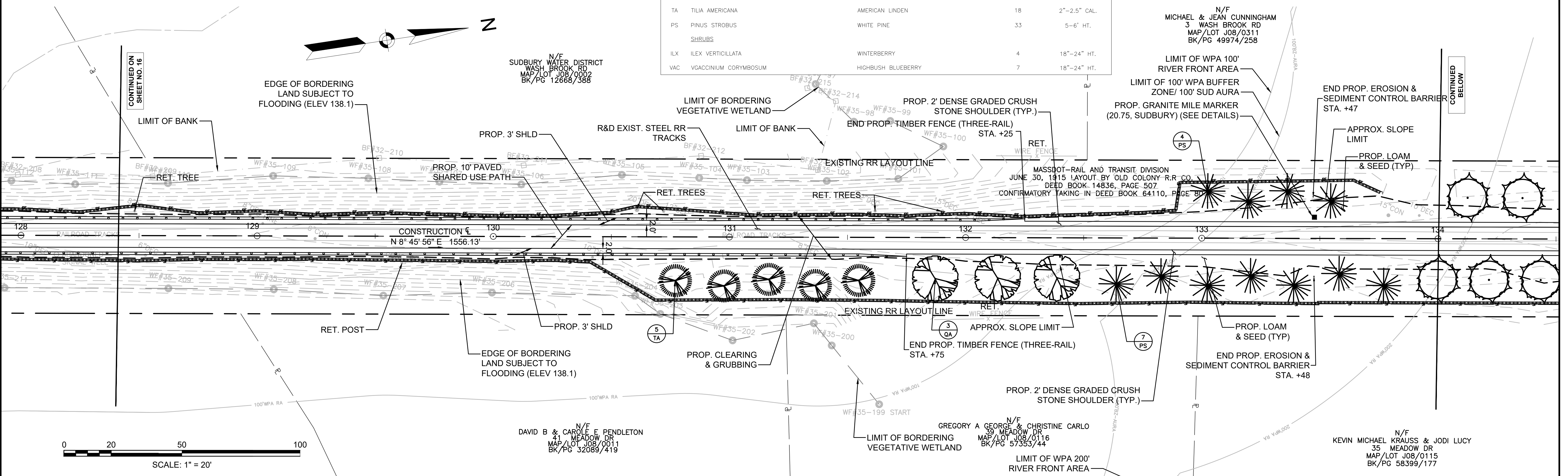
LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#  
 PLANT QUANTITY AND SPECIES X  
 XX

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	17	318

PROJECT FILE NO. 608164  
**CONSTRUCTION PLANS**



**FENCING DETAILS**

TIMBER FENCE (TWO-RAIL) STA. 139+00 RT TO STA. 142+50 RT  
 STOCKADE FENCE STA. 143+30 LT TO STA. 144+60 LT

**HIGHWAY GUARD DETAILS**

NONE

**TRAFFIC SIGNAL CONDUIT**

NONE

**WATER SUPPLY ALTERATIONS**

NONE

**DRAINAGE DETAILS**

NONE

**PLANT LIST**

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
AS	ACER SACCHARUM	LEGACY SUGAR MAPLE	5	2"-2.5" CAL.
TA	TILIA AMERICANA	AMERICAN LINDEN	5	2"-2.5" CAL.
PS	PINUS STROBUS	WHITE PINE	11	5-6" HT.
<b>SHRUBS</b>				
IG	ILEX GLABRA	INKBERRY	7	2-3" HT.
CLA	CLETHRA ALNIFOLIA	SUMMERSWEET	26	18"-24" HT.
ILX	ILEX VERTICILLATA	WINTERBERRY	18	18"-24" HT.

**LEGEND:**

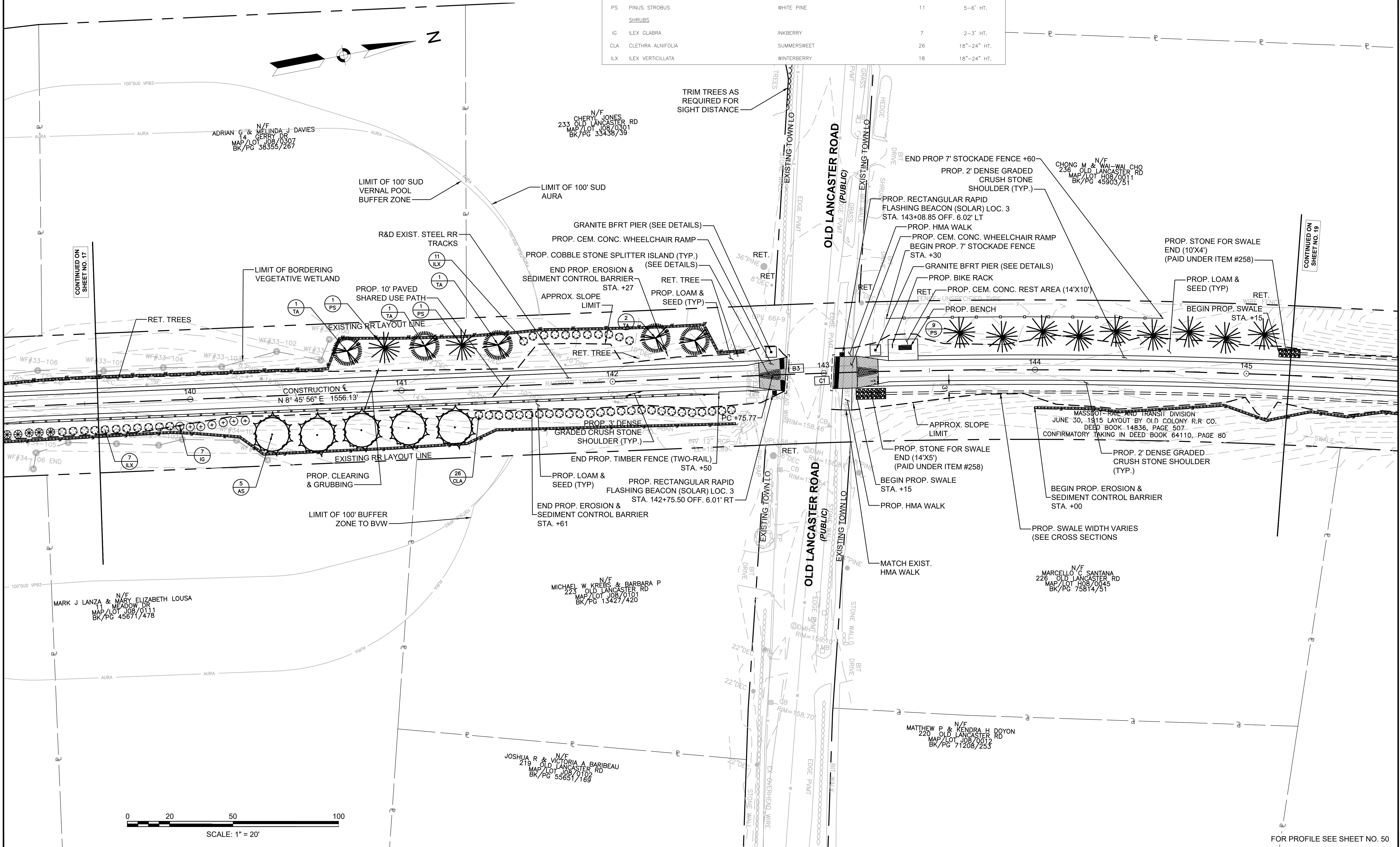
PROPOSED WHEELCHAIR RAMP DETAIL # X#  
 PLANT QUANTITY AND SPECIES X  
XX

**SUDBURY  
 BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	18	318

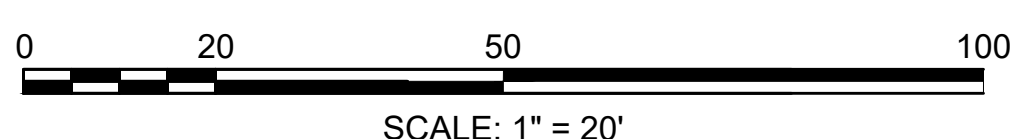
PROJECT FILE NO. 608164

**CONSTRUCTION PLANS**



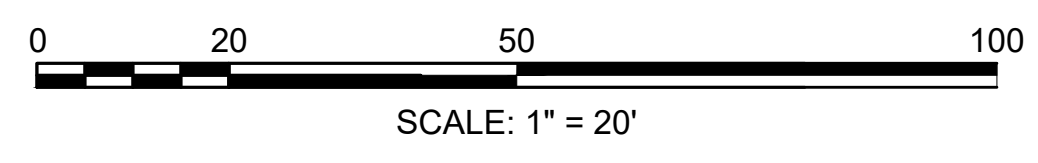
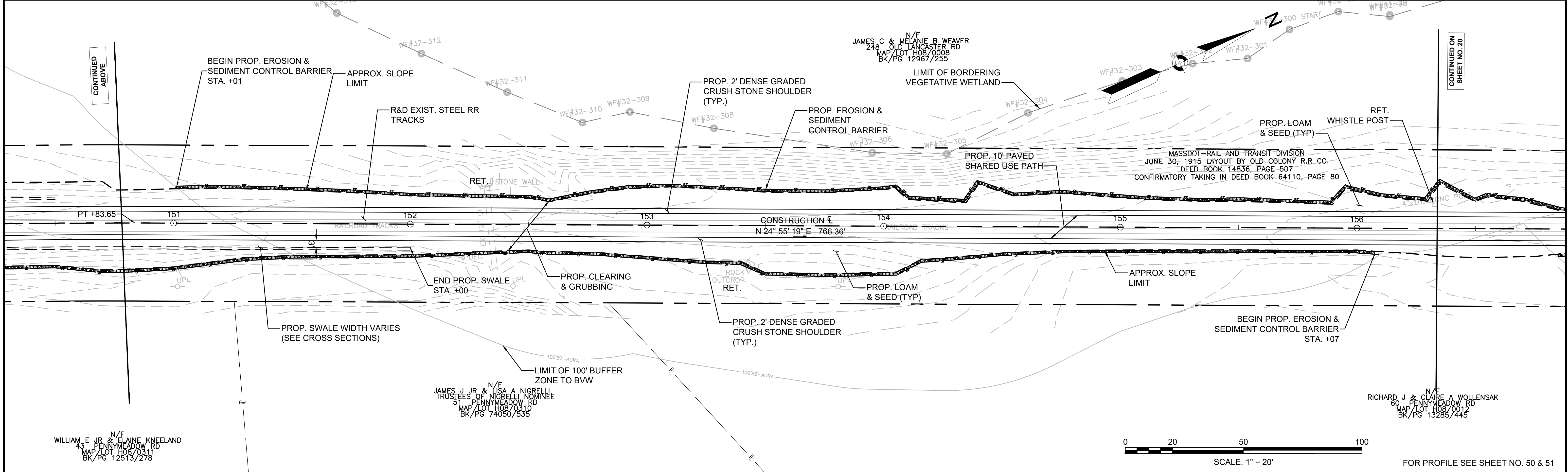
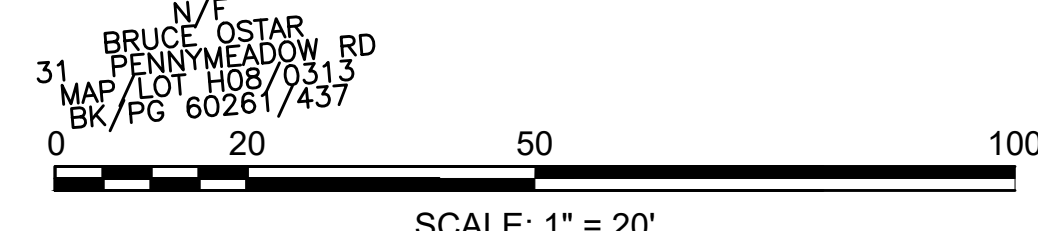
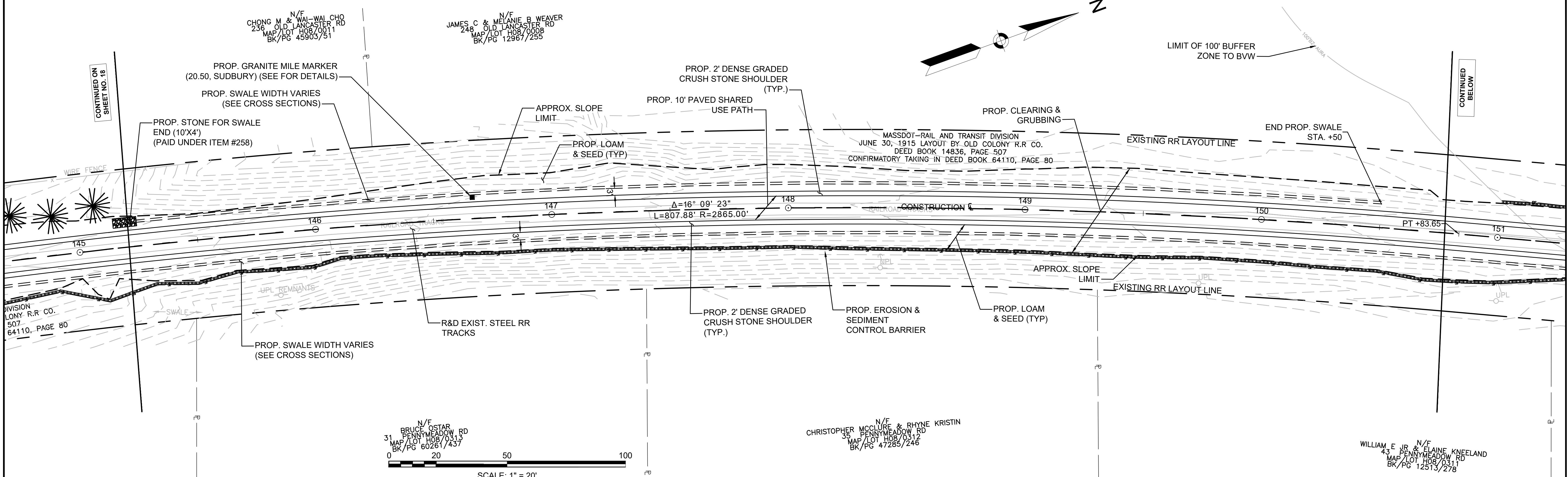
CONTINUED ON SHEET NO. 17

CONTINUED ON SHEET NO. 19



FOR PROFILE SEE SHEET NO. 50

HIGHWAY GUARD DETAILS NONE  
 TRAFFIC SIGNAL CONDUIT NONE  
 WATER SUPPLY ALTERATIONS NONE  
 DRAINAGE DETAILS NONE



FOR PROFILE SEE SHEET NO. 50 & 51

FENCING DETAILS

HIGHWAY GUARD DETAILS

TRAFFIC SIGNAL CONDUIT

WATER SUPPLY ALTERATIONS

DRAINAGE DETAILS

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

SUDBURY BRUCE FREEMAN TRAIL

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	20	318
PROJECT FILE NO.		608164	

CONSTRUCTION PLANS

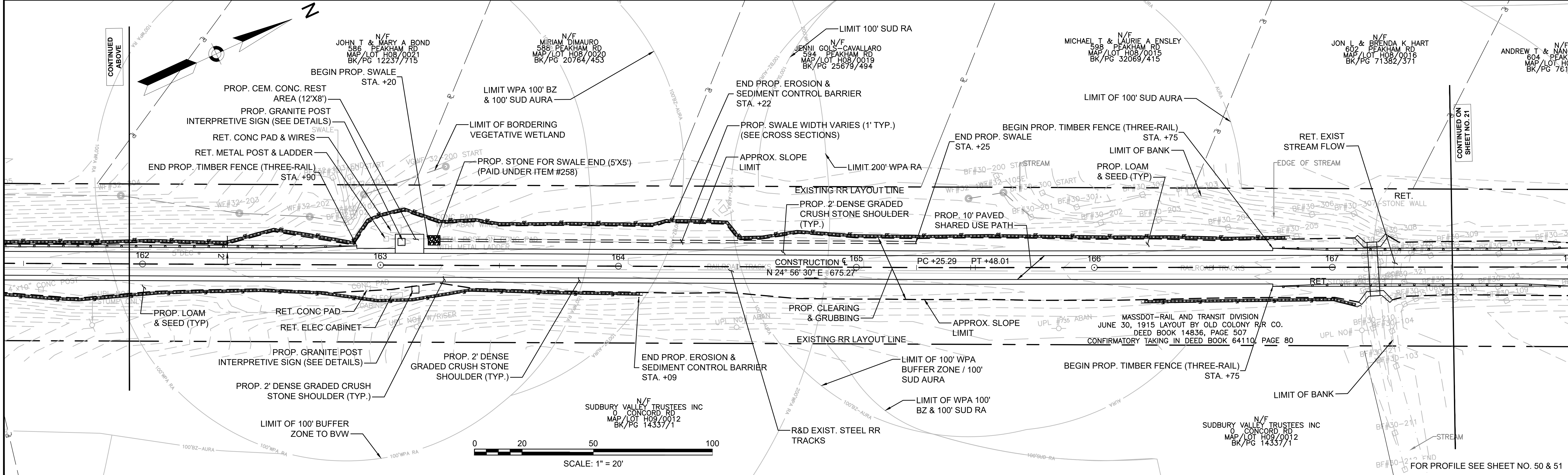
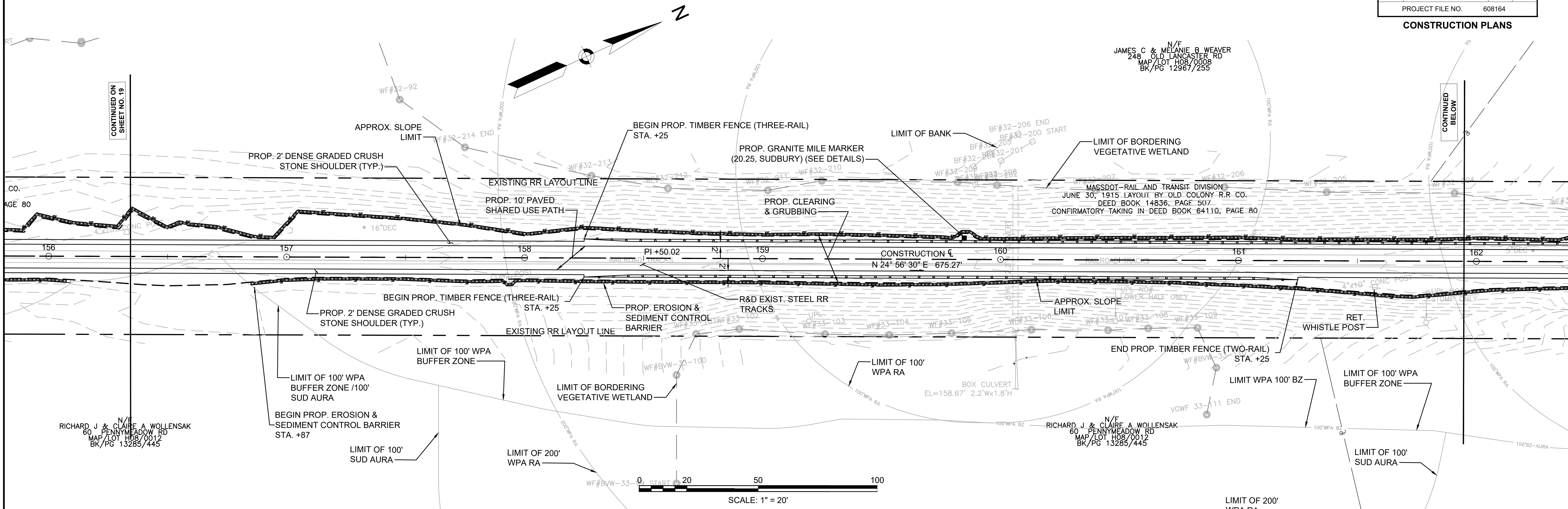
TIMBER FENCE (THREE-RAIL) STA. 158+25 LT TO STA. 163+10 LT  
TIMBER FENCE (THREE-RAIL) STA. 158+25 RT TO STA. 161+25 RT

NONE

NONE

SHEET 178

SHEET 178



FOR PROFILE SEE SHEET NO. 50 & 51

608164\_HD.CONSTRUCTION PLANS.DWG Plotted on 12-May-2021 8:18 AM

FENCING DETAILS

HIGHWAY GUARD DETAILS

TRAFFIC SIGNAL CONDUIT

WATER SUPPLY ALTERATIONS

DRAINAGE DETAILS

TIMBER FENCE (THREE-RAIL) STA. 166+75 RT TO STA. 168+00 RT  
TIMBER FENCE (THREE-RAIL) STA. 166+75 LT TO STA. 168+00 LT

NONE

NONE

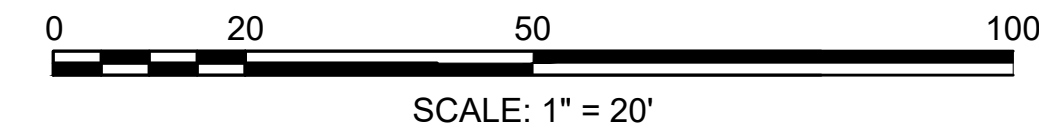
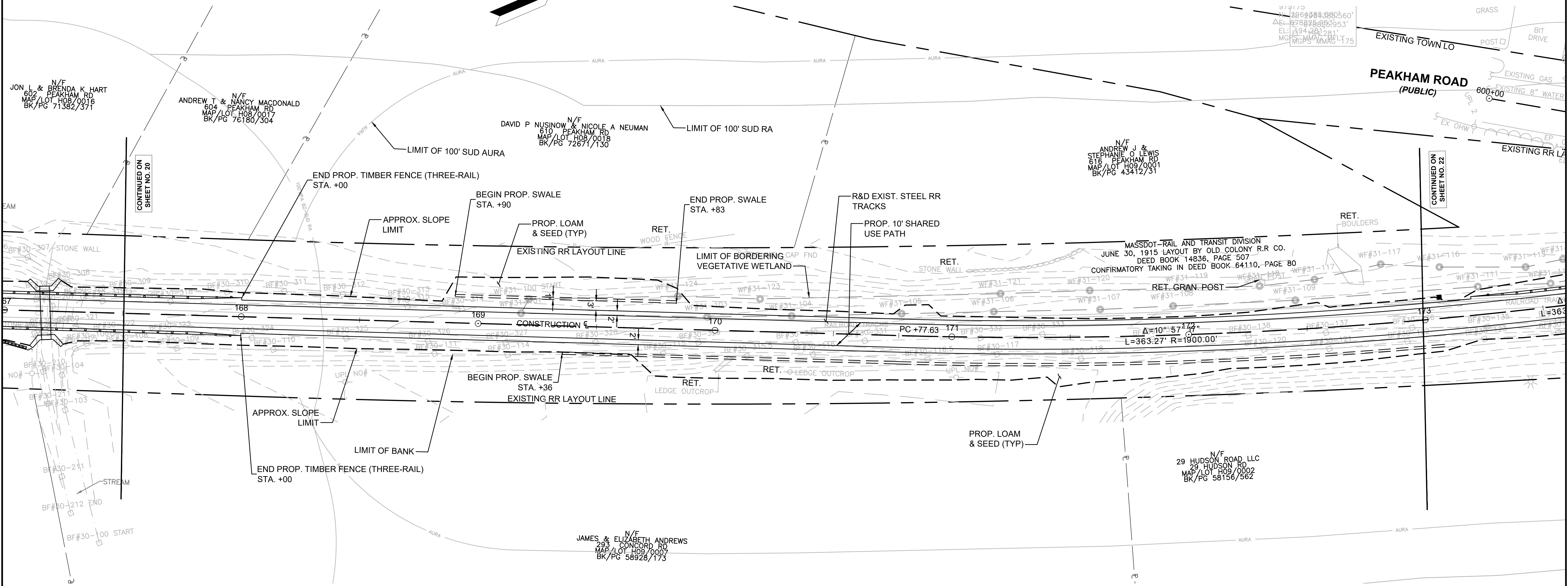
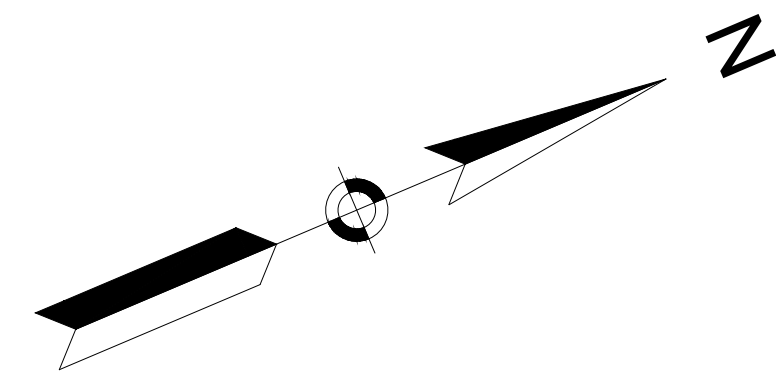
NONE

NONE

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	21	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			



CONTINUED ON SHEET NO. 20

CONTINUED ON SHEET NO. 22

FENCING DETAILS

HIGHWAY GUARD DETAILS

TRAFFIC SIGNAL CONDUIT

WATER SUPPLY ALTERATIONS

DRAINAGE DETAILS

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	22	318

PROJECT FILE NO. 608164

**CONSTRUCTION PLANS**

WF#	DESCRIPTION
WF#20336 A1	
WF#20336 B200 START	
WF#20336 B201	
WF#20336 B202	
WF#20336 B203	
WF#20336 B204	
WF#20336 B205	
WF#20336 B206	
WF#20336 B207 END	

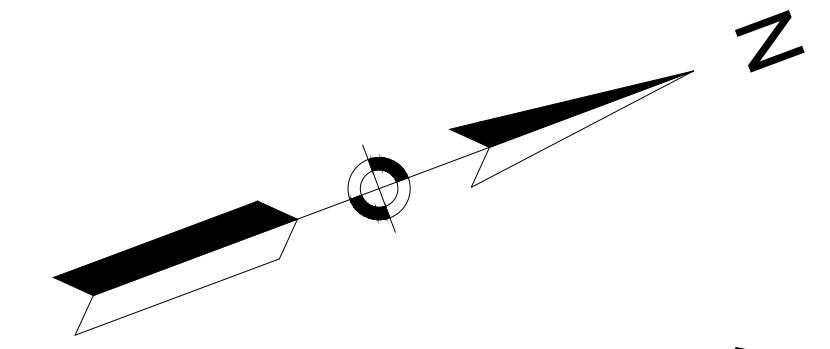
TIMBER FENCE (TWO-RAIL) STA. 173+20 LT TO STA. 173+90 LT

NONE

SEE SHEET 160

SEE SHEET 180

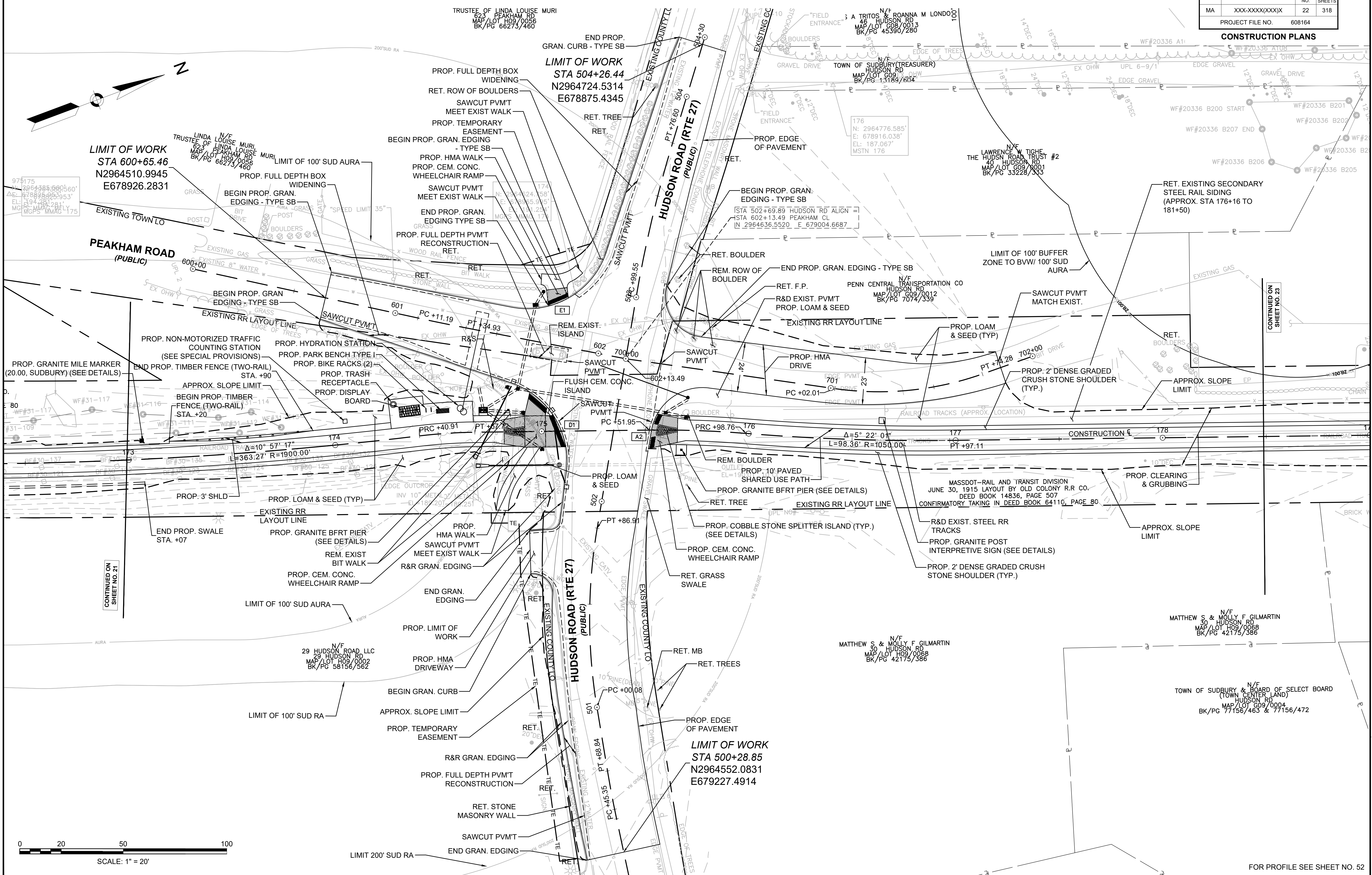
SEE SHEET 180



**LIMIT OF WORK**  
STA 600+65.46  
N2964510.9945  
E678926.2831

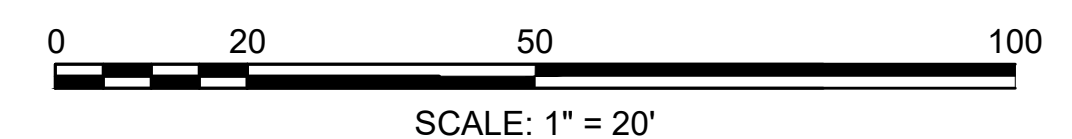
**LIMIT OF WORK**  
STA 504+26.44  
N2964724.5314  
E678875.4345

**LIMIT OF WORK**  
STA 500+28.85  
N2964552.0831  
E679227.4914



CONTINUED ON SHEET NO. 21

CONTINUED ON SHEET NO. 23



FOR PROFILE SEE SHEET NO. 52



FENCING DETAILS

HIGHWAY GUARD DETAILS

TRAFFIC SIGNAL CONDUIT

WATER SUPPLY ALTERATIONS

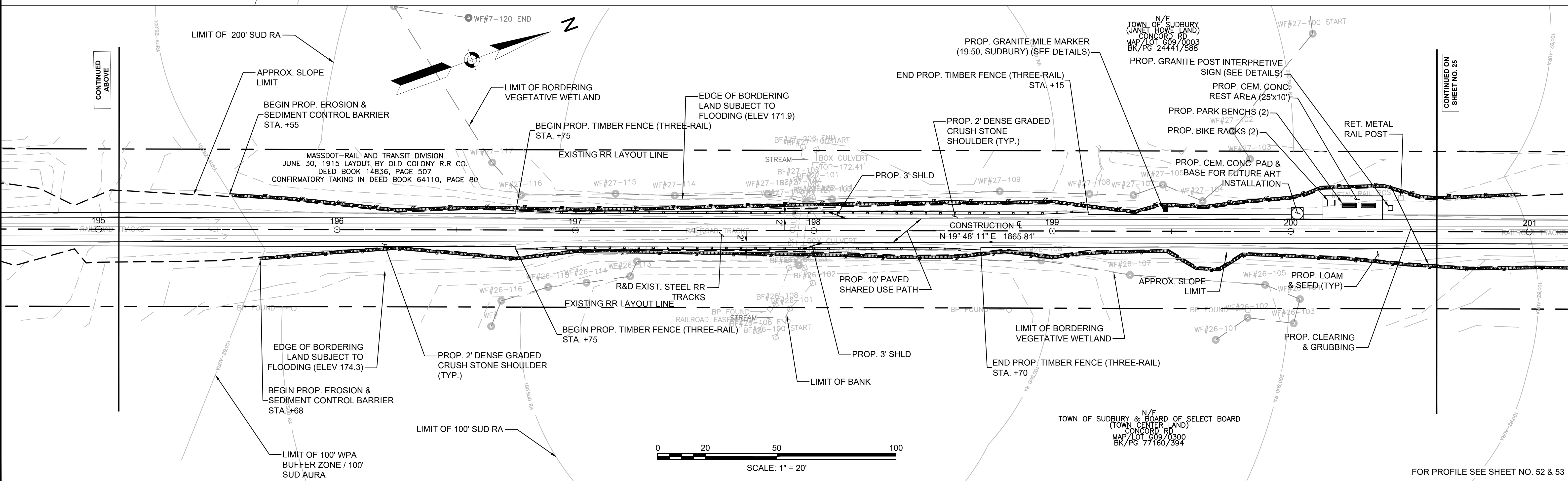
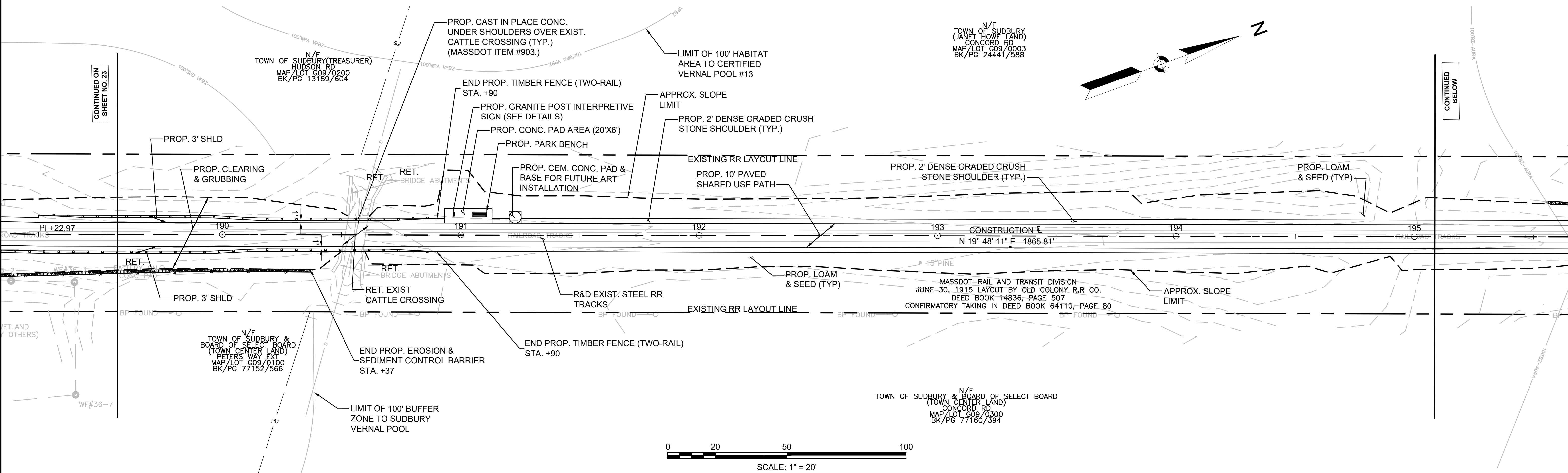
DRAINAGE DETAILS

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	24	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			

TIMBER FENCE (TWO-RAIL) STA. 184+75 RT TO STA. 190+90 RT  
 TIMBER FENCE (TWO-RAIL) STA. 189+23 LT TO STA. 190+90 LT  
 TIMBER FENCE (THREE-RAIL) STA. 196+75 RT TO STA. 198+70 RT  
 TIMBER FENCE (THREE-RAIL) STA. 196+75 LT TO STA. 199+15 LT



CONTINUED ON  
SHEET NO. 23

CONTINUED  
BELOW

CONTINUED  
ABOVE

CONTINUED ON  
SHEET NO. 25

FOR PROFILE SEE SHEET NO. 52 & 53



HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

DRAINAGE DETAILS

NONE

LEGEND:

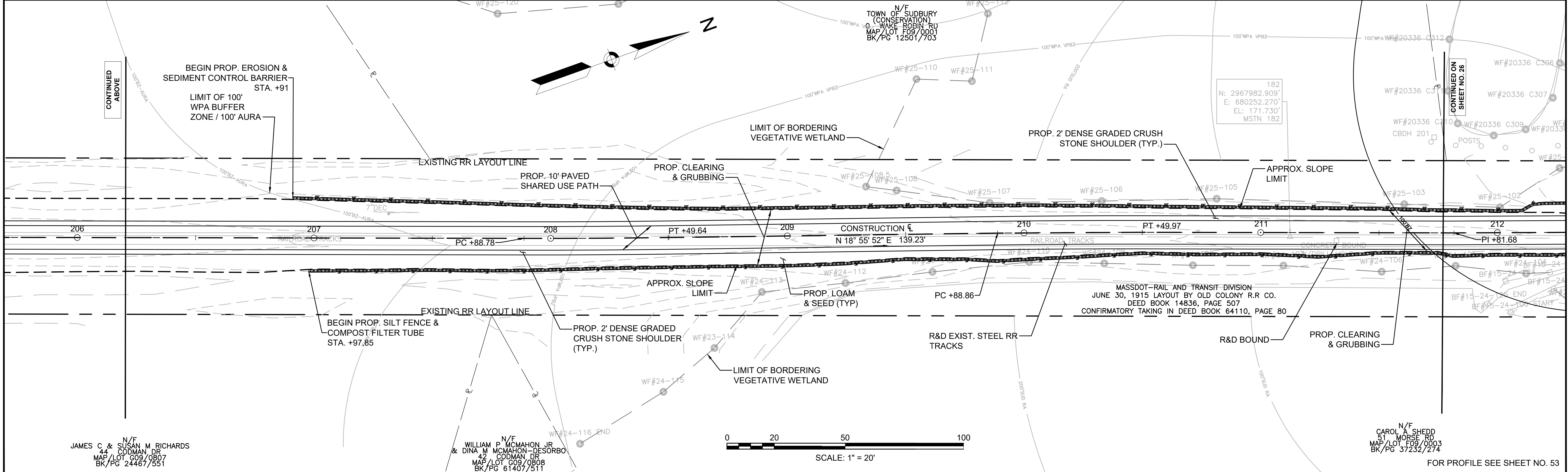
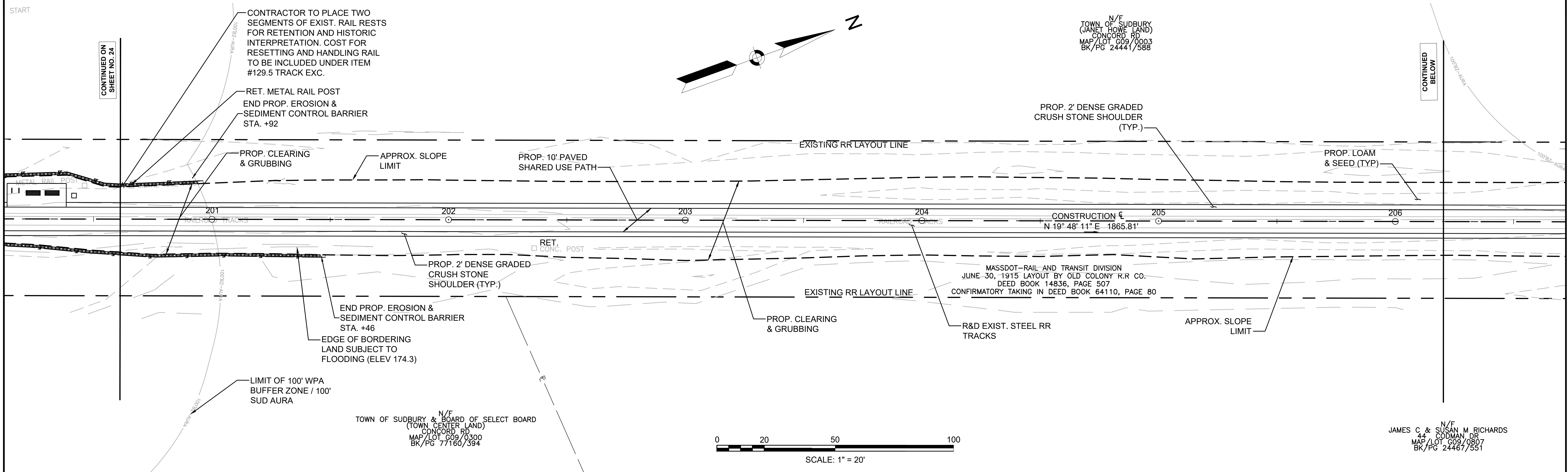
PROPOSED WHEELCHAIR RAMP DETAIL # X#

**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	25	318

PROJECT FILE NO. 608164

**CONSTRUCTION PLANS**



FOR PROFILE SEE SHEET NO. 53

NOTES:  
 1. PROP. MILLING MULCH TO BE PLACED UNDER GUARDRAIL (SEE TYPICAL SECTIONS & MASSDOT STD. SPEC. ITEM 469.)

**FENCING DETAILS**  
 TIMBER FENCE (THREE-RAIL)  
 STA. 216+25 RT TO STA. 216+53 RT  
 TIMBER FENCE (THREE-RAIL)  
 STA. 216+89 RT TO STA. 217+00 RT

**HIGHWAY GUARD DETAILS**  
 TIMBER GUARDRAIL  
 STA. 216+09 RT TO STA. 216+53 RT  
 TIMBER GUARDRAIL  
 STA. 216+68 RT TO STA. 217+64 RT  
 TIMBER GUARDRAIL  
 STA. 216+01 RT TO STA. 216+52 RT  
 TIMBER GUARDRAIL  
 STA. 216+68 RT TO STA. 217+64 RT

**TRAFFIC SIGNAL CONDUIT**  
 NONE

**WATER SUPPLY ALTERATIONS**  
 SHEET 184

**DRAINAGE DETAILS**  
 SHEET 184

**PLANT LIST**

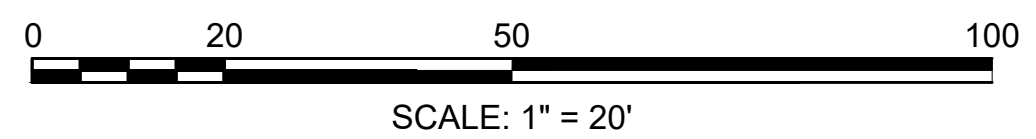
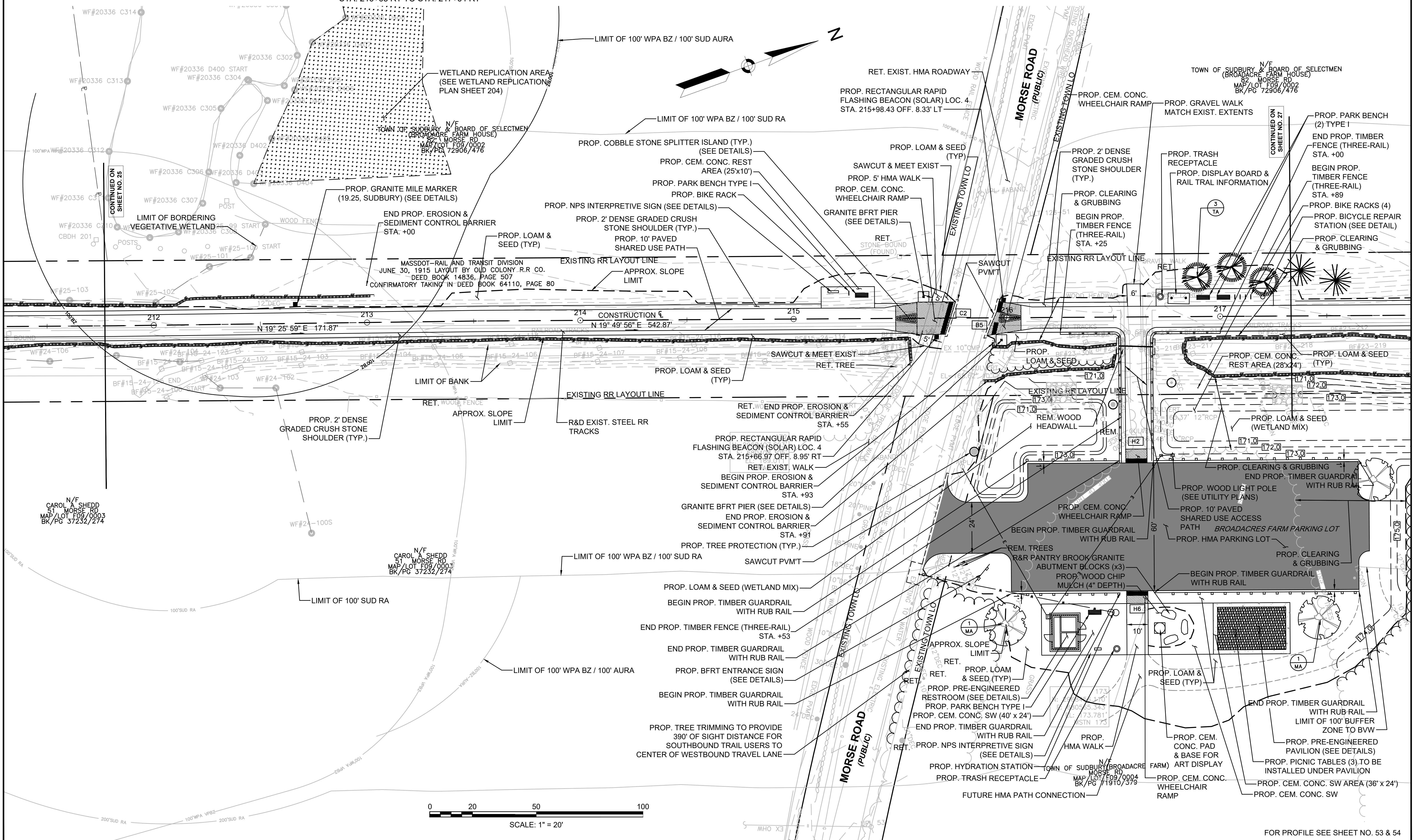
KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
TA	TILIA AMERICANA	AMERICAN LINDEN	4	2"-2.5" CAL.
MA	MALUS 'IVORY SPEAR'	IVORY SPEAR CRABAPPLE	2	2"-2.5" CAL.

**LEGEND:**  
 PROPOSED WHEELCHAIR RAMP DETAIL # [Symbol]  
 PLANT QUANTITY AND SPECIES [Symbol]

**SUDBURY**  
**BRUCE FREEMAN TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	26	318

PROJECT FILE NO. 608164  
**CONSTRUCTION PLANS**



FOR PROFILE SEE SHEET NO. 53 & 54

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 222+25 LT TO STA. 231+73 LT  
 TIMBER FENCE (THREE-RAIL) STA. 224+23 RT TO STA. 228+73 RT

HIGHWAY GUARD DETAILS

NONE

WATER SUPPLY ALTERATIONS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

DRAINAGE DETAILS

NONE

PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
TA	TILIA AMERICANA	AMERICAN LINDEN	1	2"-2.5" CAL.
PS	PINUS STROBUS	WHITE PINE	2	5-6' HT.
MA	MALUS 'IVORY SPEAR'	IVORY SPEAR CRABAPPLE	1	2"-2.5" CAL.
<b>SHRUBS</b>				
ARA	ARONIA ARBUTIFOLIA	RED CHOKEBERRY	7	3-4' HT. B&B
ILX	ILEX VERTICILLATA	WINTERBERRY	42	18"-24" HT.

LEGEND:

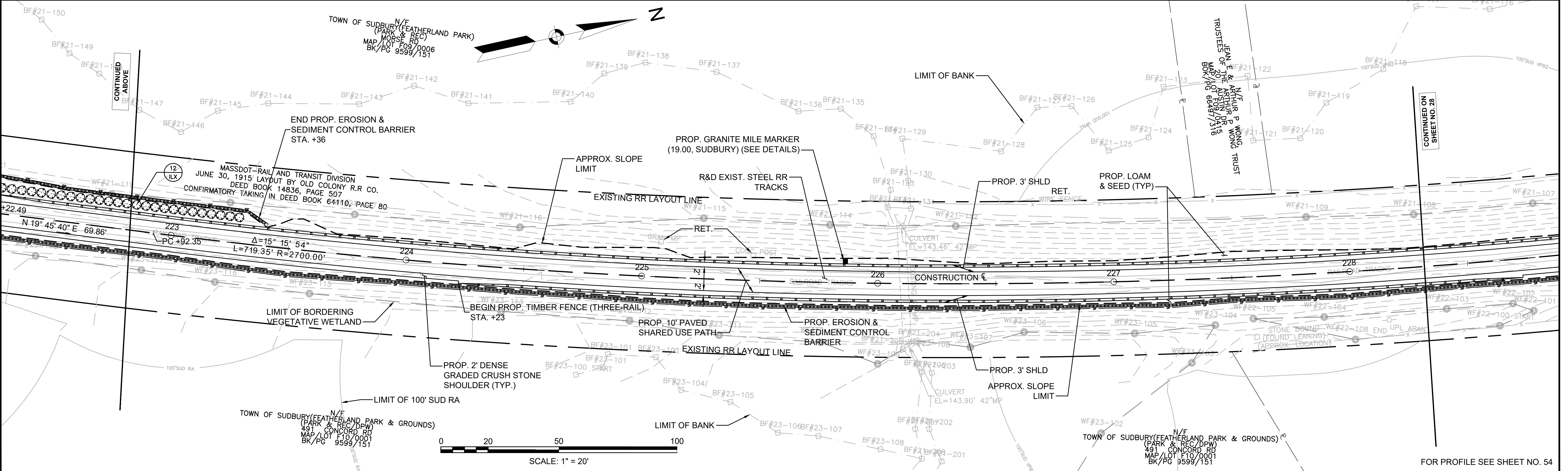
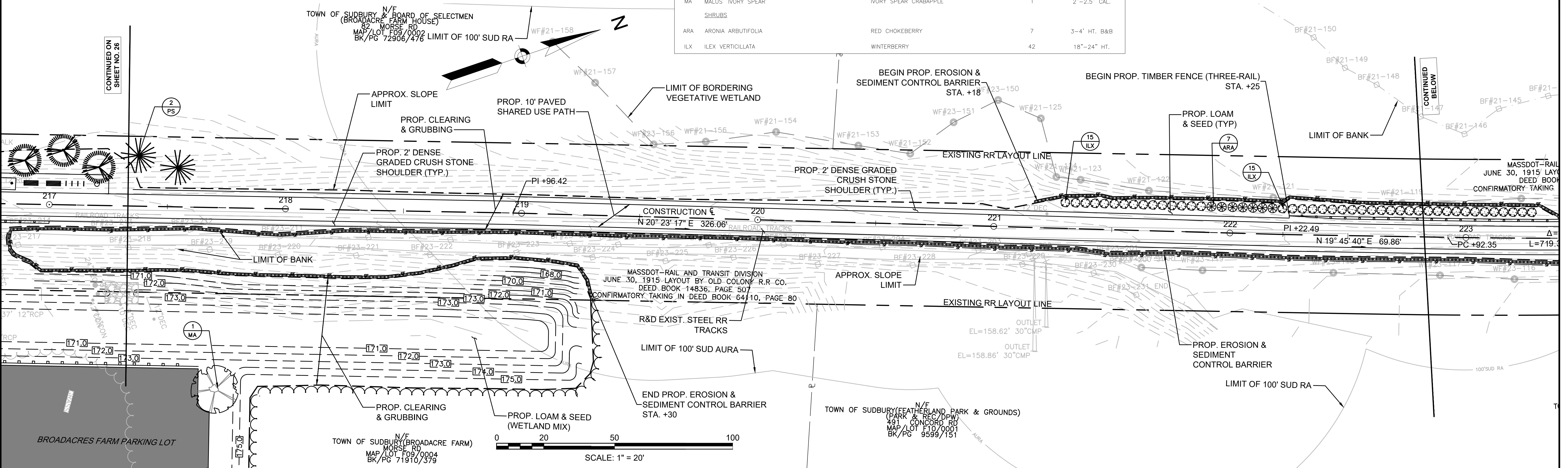
PROPOSED WHEELCHAIR RAMP DETAIL # X#  
 PLANT QUANTITY AND SPECIES X  
 XX

BRUCE FREEMAN RAIL TRAIL

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	27	318

PROJECT FILE NO. 608164

CONSTRUCTION PLANS



CONTINUED ON SHEET NO. 26

CONTINUED BELOW

CONTINUED ABOVE

CONTINUED ON SHEET NO. 28

FOR PROFILE SEE SHEET NO. 54

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 222+25 LT TO STA. 231+73 LT  
TIMBER FENCE (THREE-RAIL) STA. 224+23 RT TO STA. 228+73 RT  
TIMBER FENCE (TWO-RAIL) STA. 236+90 LT TO STA. 241+73 LT  
TIMBER FENCE (TWO-RAIL) STA. 236+90 RT TO STA. 241+73 RT

HIGHWAY GUARD DETAILS

NONE

WATER SUPPLY ALTERATIONS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

DRAINAGE DETAILS

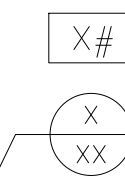
NONE

PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
TREES				
PG	PICEA GLAUCA	WHITE SPRUCE	22	4-5' HT.

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL #  
PLANT QUANTITY AND SPECIES

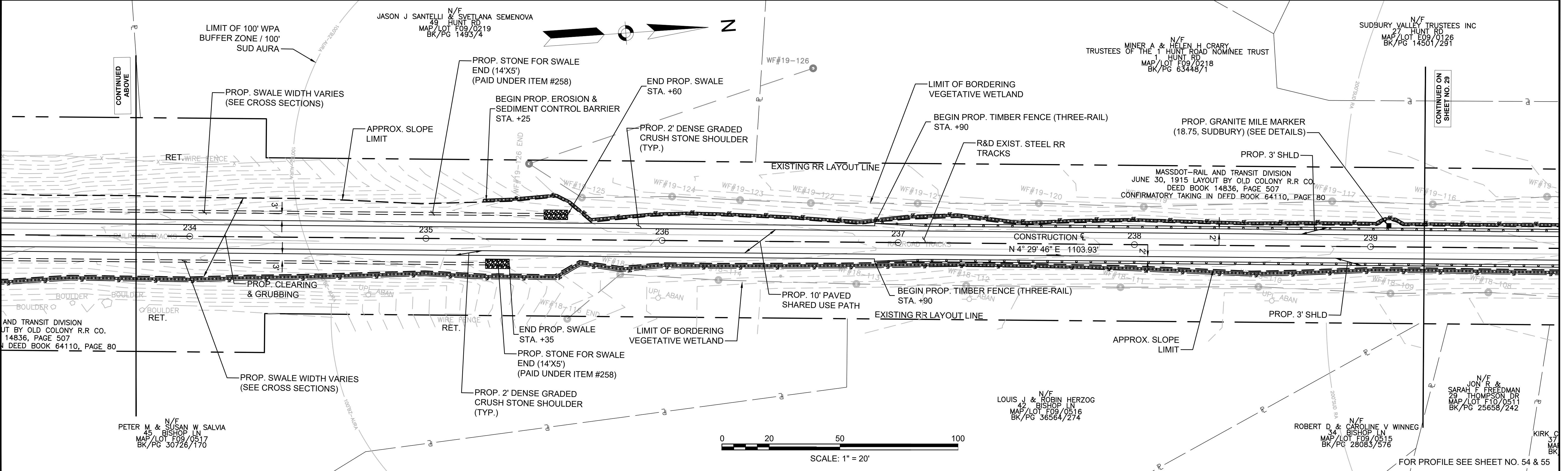
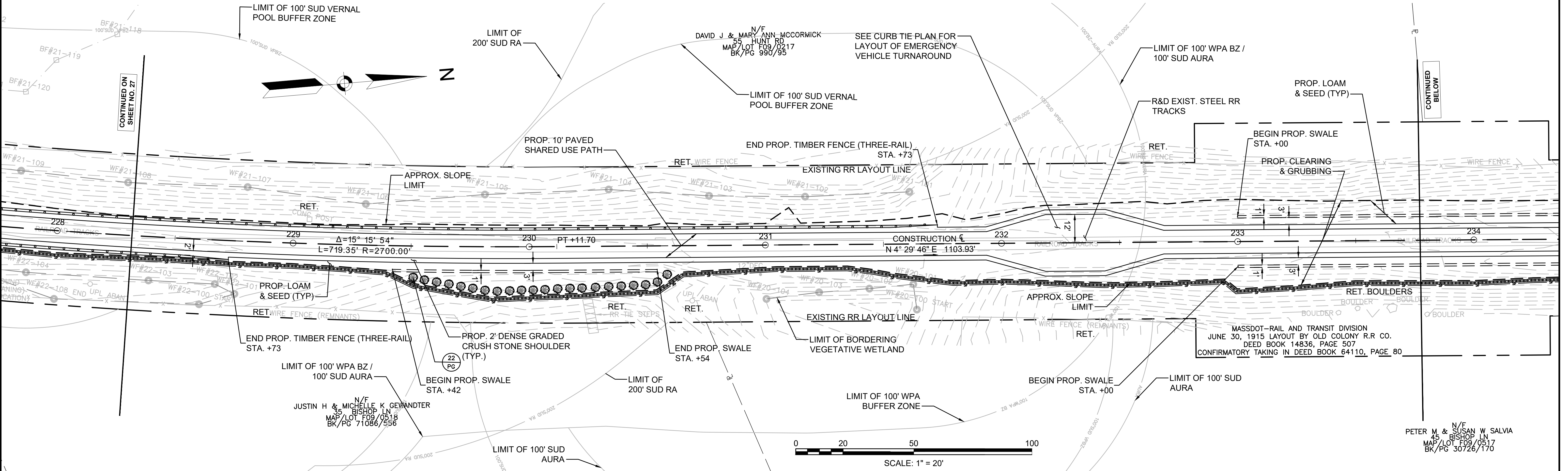


SUDBURY BRUCE FREEMAN RAIL TRAIL

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	28	318

PROJECT FILE NO. 608164

CONSTRUCTION PLANS



**FENCING DETAILS**

TIMBER FENCE (THREE-RAIL) STA. 236+90 LT TO STA. 243+45 LT  
 TIMBER FENCE (THREE-RAIL) STA. 236+90 RT TO STA. 241+73 RT  
 TIMBER FENCE (TWO-RAIL) STA. 245+75 LT TO STA. 248+30 LT  
 TIMBER FENCE (TWO-RAIL) STA. 245+75 RT TO STA. 248+30 RT  
 STOCKADE FENCE STA. 247+75 LT TO STA. 251+16 LT

**HIGHWAY GUARD DETAILS**

NONE

**TRAFFIC SIGNAL CONDUIT**

NONE

**WATER SUPPLY ALTERATIONS**

NONE

**DRAINAGE DETAILS**

NONE

**PLANT LIST**

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
TA	TILIA AMERICANA	AMERICAN LINDEN	15	2"-2.5" CAL.
PS	PINUS STROBUS	WHITE PINE	10	5-6' HT.
<b>SHRUBS</b>				
IG	ILEX GLABRA	INKBERRY	5	2-3' HT.
ILX	ILEX VERTICILLATA	WINTERBERRY	11	18"-24" HT.

**LEGEND:**

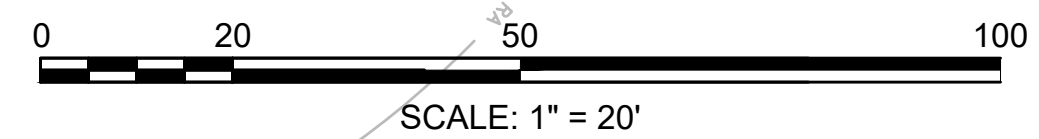
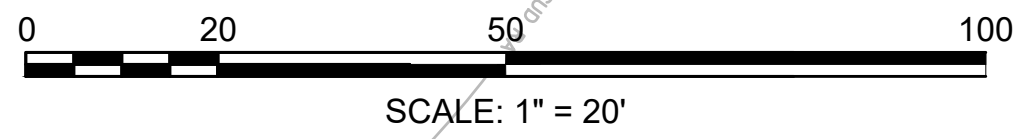
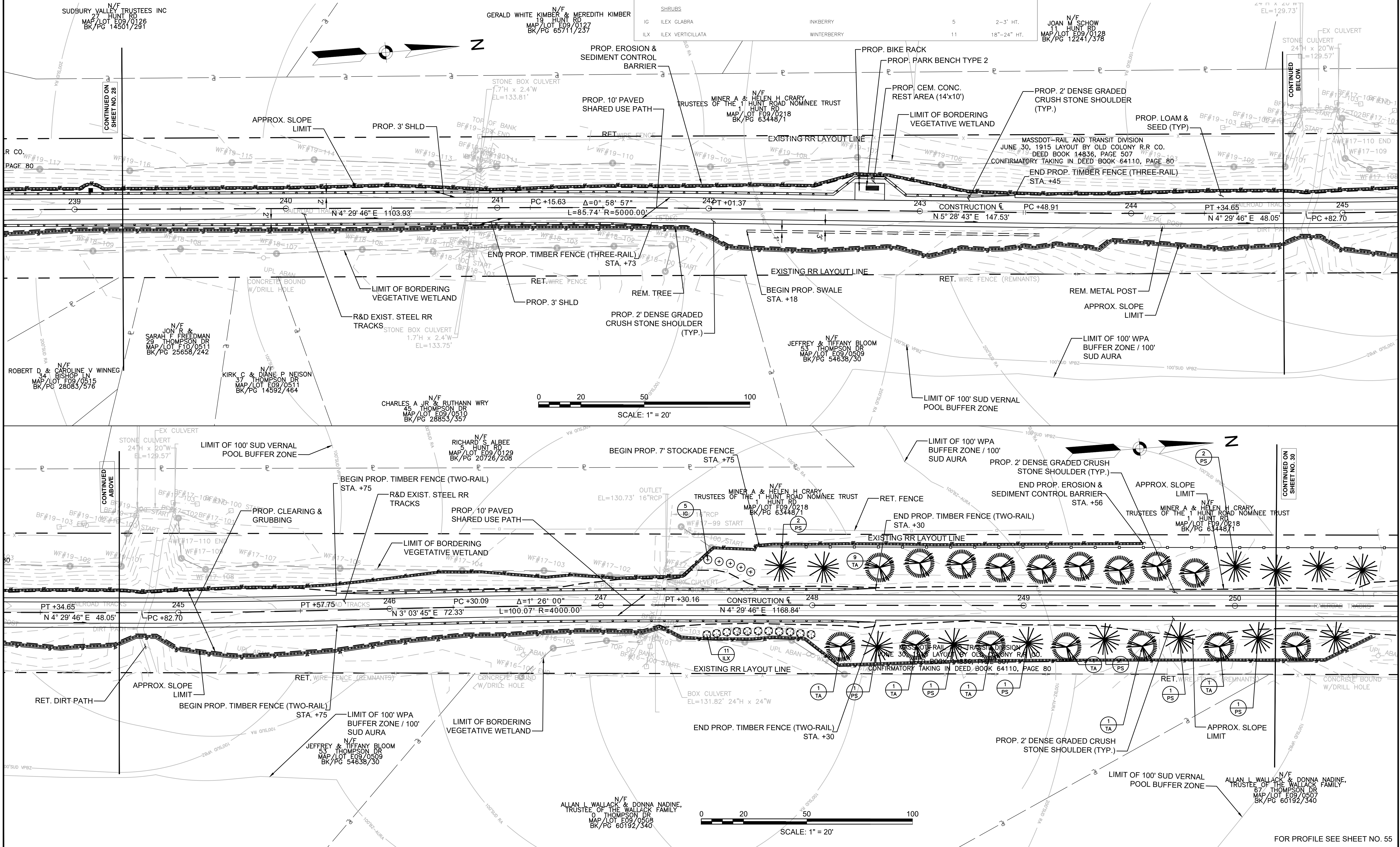
PROPOSED WHEELCHAIR RAMP DETAIL # X#  
 PLANT QUANTITY AND SPECIES X XX

**SUBBURY BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	29	318

PROJECT FILE NO. 608164

**CONSTRUCTION PLANS**



FOR PROFILE SEE SHEET NO. 55

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 254+48 LT TO STA. 258+73 LT  
 TIMBER FENCE (THREE-RAIL) STA. 258+23 RT TO STA. 258+73 RT

HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

DRAINAGE DETAILS

SHEET 188

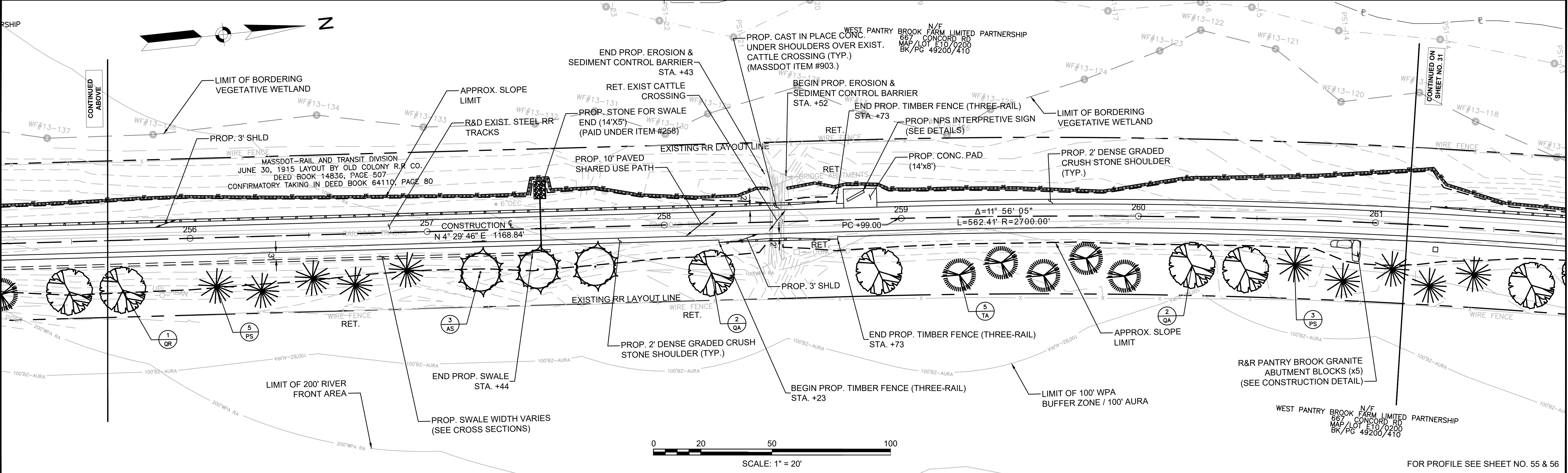
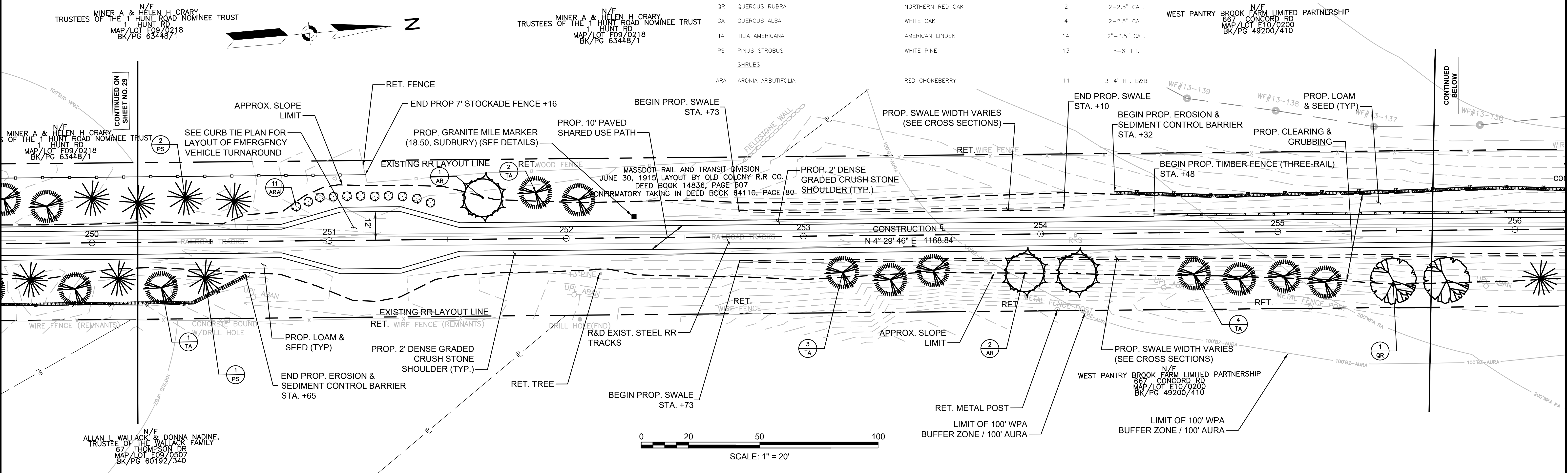
PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
AR	ACER RUBRUM 'OCTOBER GLORY'	OCTOBER GLORY MAPLE	3	2-2.5" CAL.
AS	ACER SACCHARUM	LEGACY SUGAR MAPLE	3	2-2.5" CAL.
QR	QUERCUS RUBRA	NORTHERN RED OAK	2	2-2.5" CAL.
QA	QUERCUS ALBA	WHITE OAK	4	2-2.5" CAL.
TA	TILIA AMERICANA	AMERICAN LINDEN	14	2"-2.5" CAL.
PS	PINUS STROBUS	WHITE PINE	13	5-6" HT.
<b>SHRUBS</b>				
ARA	ARONIA ARBUTIFOLIA	RED CHOKEBERRY	11	3-4' HT. B&B

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#  
 PLANT QUANTITY AND SPECIES X  
 XX

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	30	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			



FOR PROFILE SEE SHEET NO. 55 & 56

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 261+23 LT TO STA. 266+73 LT  
 TIMBER FENCE (THREE-RAIL) STA. 262+73 RT TO STA. 270+23 RT

NOTES:  
 1. ANY GRANITE ABUTMENT BLOCKS NOT BEING MODIFIED OR LEFT IN PLACE AS PART OF THE BRIDGE REPLACEMENT (SEE BRIDGE PLANS) SHALL BE REMOVED AND STACKED AT THE SUDBURY DPW YARD. THE CONTRACTOR SHALL ALSO TAKE SEVEN OF THE GRANITE BLOCKS AND REMOVED AND RESET AS FOLLOWS: TWO BLOCKS TO BE RESET AS BENCHES AT APPROX. STA. 101+75 LT AND FIVE BLOCKS TO BE RESET AS A HISTORIC INTERPRETATION AT APPROX. STA. 261+25 RT. PAYMENT FOR R&R GRANITE ABUTMENT BLOCKS SHALL BE MADE UNDER ITEM 998.2.

HIGHWAY GUARD DETAILS

NONE

WATER SUPPLY ALTERATIONS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

DRAINAGE DETAILS

NONE

PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
AR	ACER RUBRUM 'OCTOBER GLORY'	OCTOBER GLORY MAPLE	4	2-2.5" CAL.
AS	ACER SACCHARUM	LEGACY SUGAR MAPLE	11	2-2.5" CAL.
QR	QUERCUS RUBRA	NORTHERN RED OAK	5	2-2.5" CAL.
TA	TILIA AMERICANA	AMERICAN LINDEN	8	2"-2.5" CAL.
PS	PINUS STROBUS	WHITE PINE	7	5-6' HT.

LEGEND:

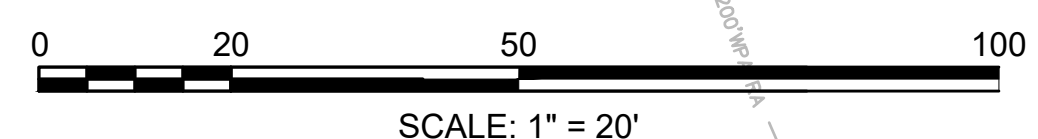
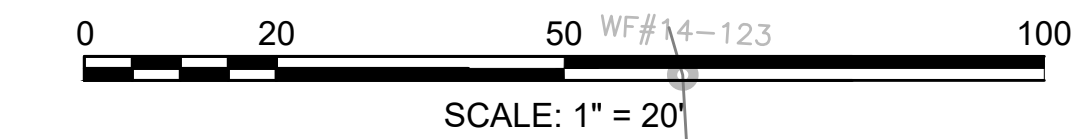
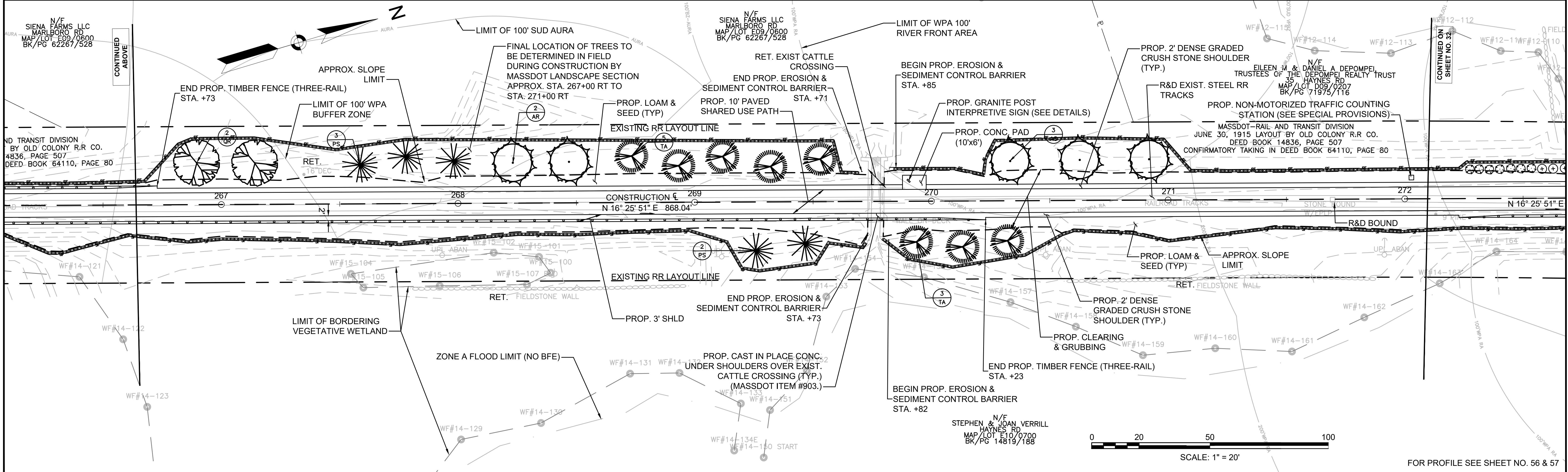
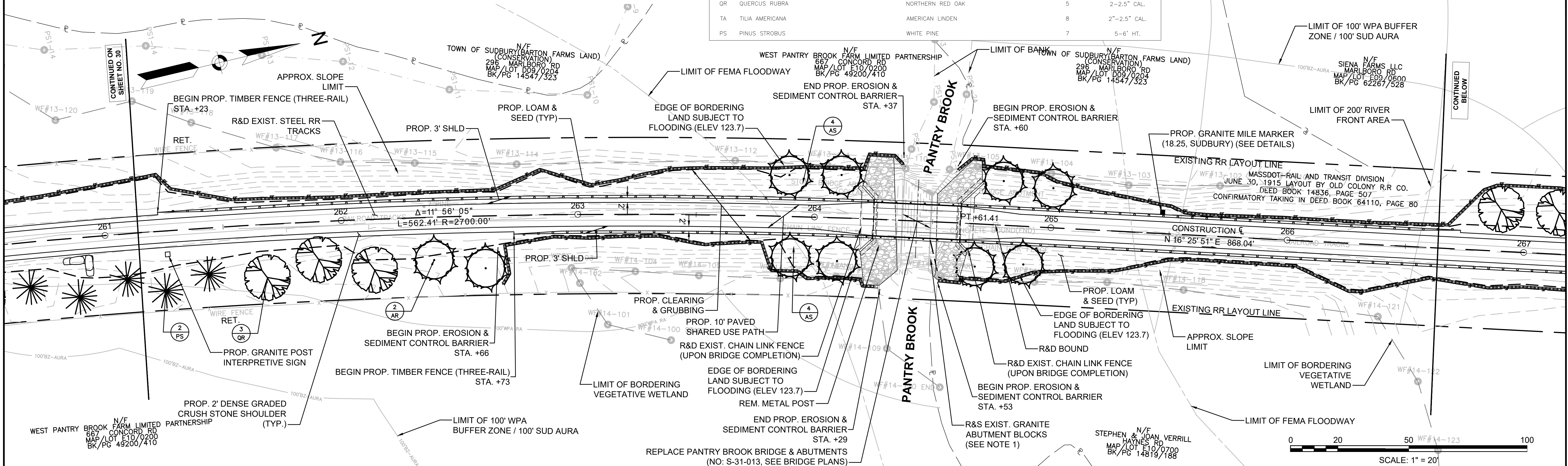
PROPOSED WHEELCHAIR RAMP DETAIL # X#  
 PLANT QUANTITY AND SPECIES X XX

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	31	318

PROJECT FILE NO. 608164

**CONSTRUCTION PLANS**



FOR PROFILE SEE SHEET NO. 56 & 57

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 272+23 RT TO STA. 273+73 RT  
 STOCKADE FENCE STA. 275+55 LT TO STA. 280+25 LT  
 STOCKADE FENCE STA. 275+80 RT TO STA. 279+80 RT

HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

DRAINAGE DETAILS

NONE

PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
SHRUBS				
IG	ILEX GLABRA	INKBERRY	8	2'-3" HT.
CLA	CLETHRA ALNIFOLIA	SUMMERSWEET	3	18"-24" HT.
ILX	ILEX VERTICILLATA	WINTERBERRY	24	18"-24" HT.

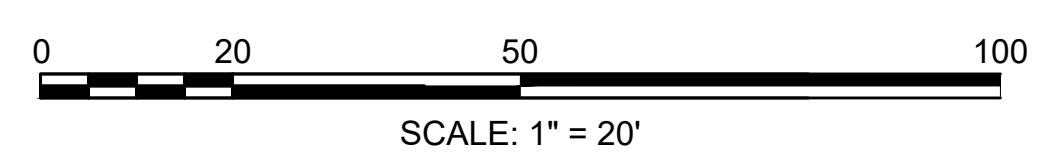
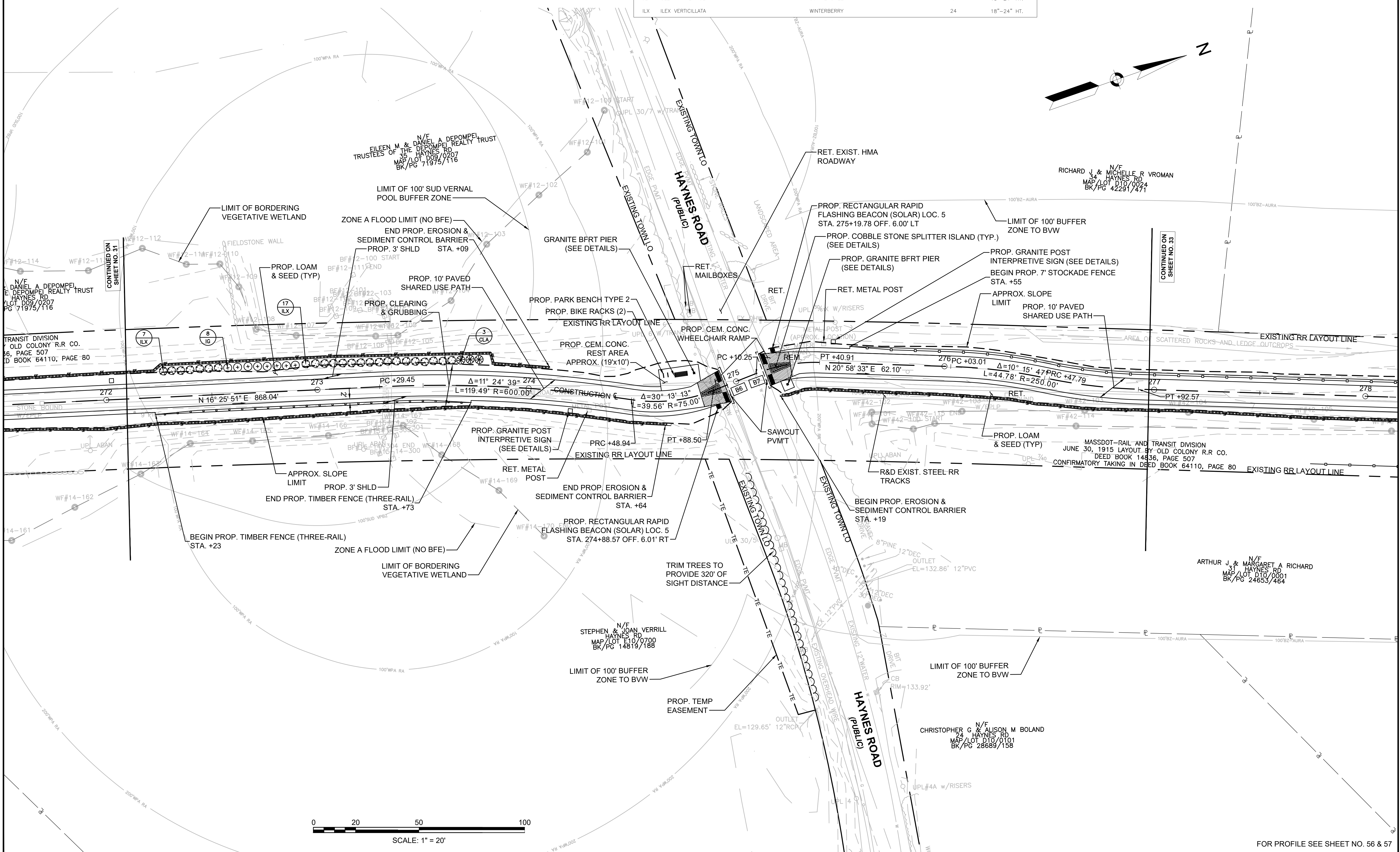
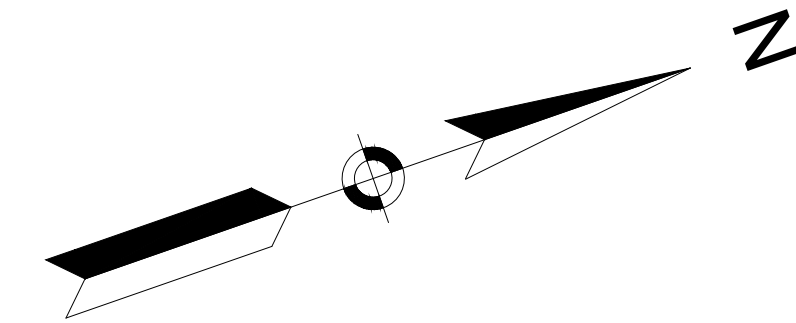
LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # [Symbol]  
 PLANT QUANTITY AND SPECIES [Symbol]

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	32	318

PROJECT FILE NO. 608164  
**CONSTRUCTION PLANS**



FOR PROFILE SEE SHEET NO. 56 & 57



FENCING DETAILS

STOCKADE FENCE STA. 277+75 RT TO STA. 282+00 RT  
 STOCKADE FENCE STA. 275+55 LT TO STA. 280+25 LT

HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

DRAINAGE DETAILS

NONE

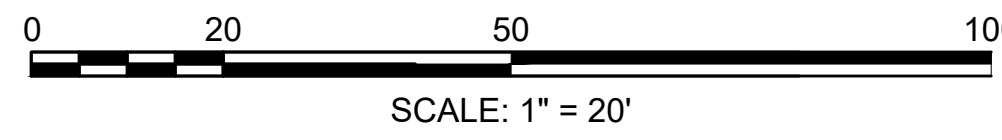
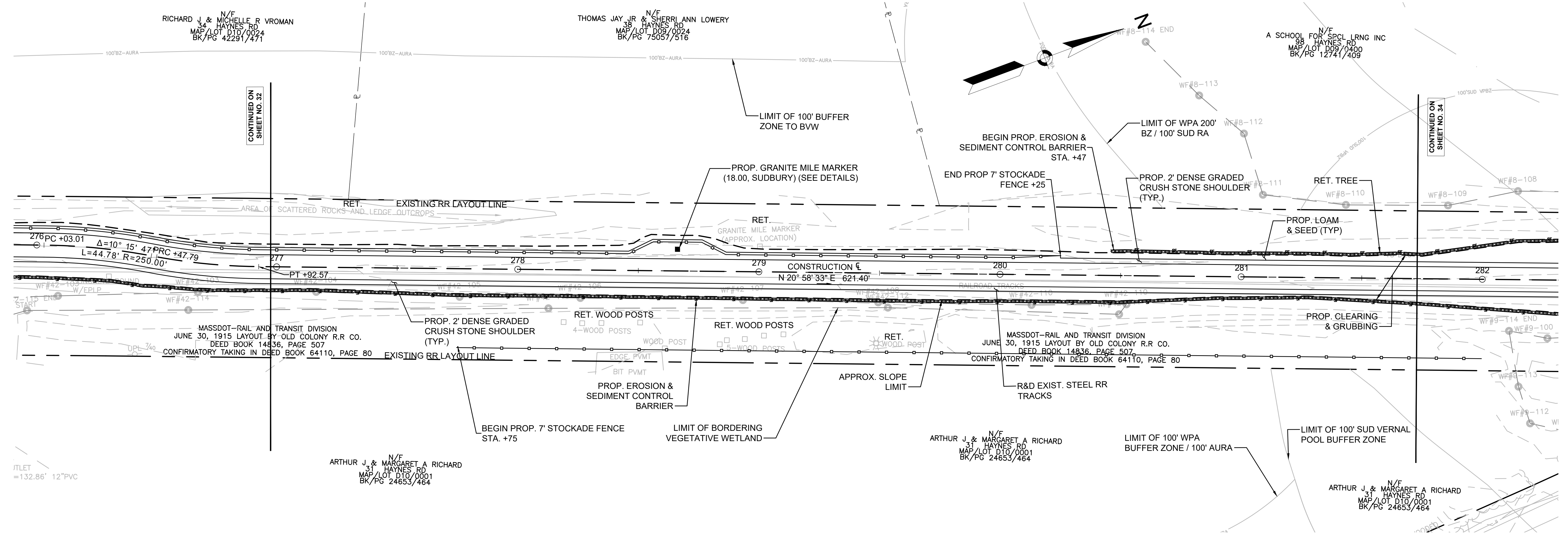
LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

SUDBURY  
 BRUCE FREEMAN RAIL TRAIL

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	33	318
PROJECT FILE NO.		608164	

CONSTRUCTION PLANS



CONTINUED ON SHEET NO. 32

CONTINUED ON SHEET NO. 34

ITILET = 132.86' 12" PVC

FOR PROFILE SEE SHEET NO. ## AND ##

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 283+73 LT TO STA. 284+28 LT  
TIMBER FENCE (THREE-RAIL) STA. 284+68 RT TO STA. 285+50 RT  
TIMBER FENCE (TWO-RAIL) STA. 285+50 RT TO STA. 293+25 RT

HIGHWAY GUARD DETAILS

STEEL W-BEAM GUARDRAIL (TL-2) W/ WOOD POST STA. 284+28 LT TO STA. 284+46 LT  
STEEL W-BEAM GUARDRAIL (TL-2) W/ WOOD POST STA. 284+75 LT TO STA. 285+18 LT  
STEEL W-BEAM GUARDRAIL (TL-2) W/ WOOD POST STA. 284+51 RT TO STA. 284+63 RT

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

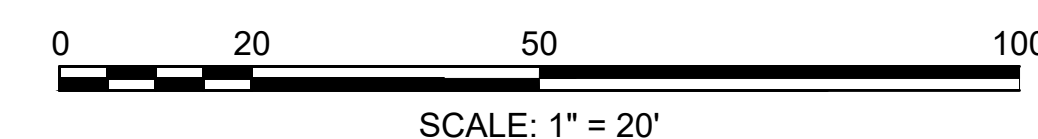
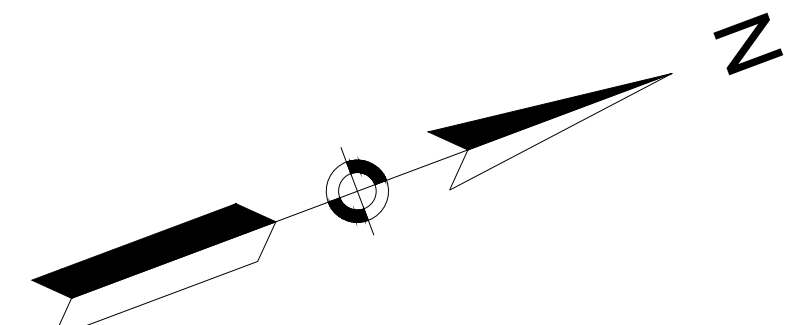
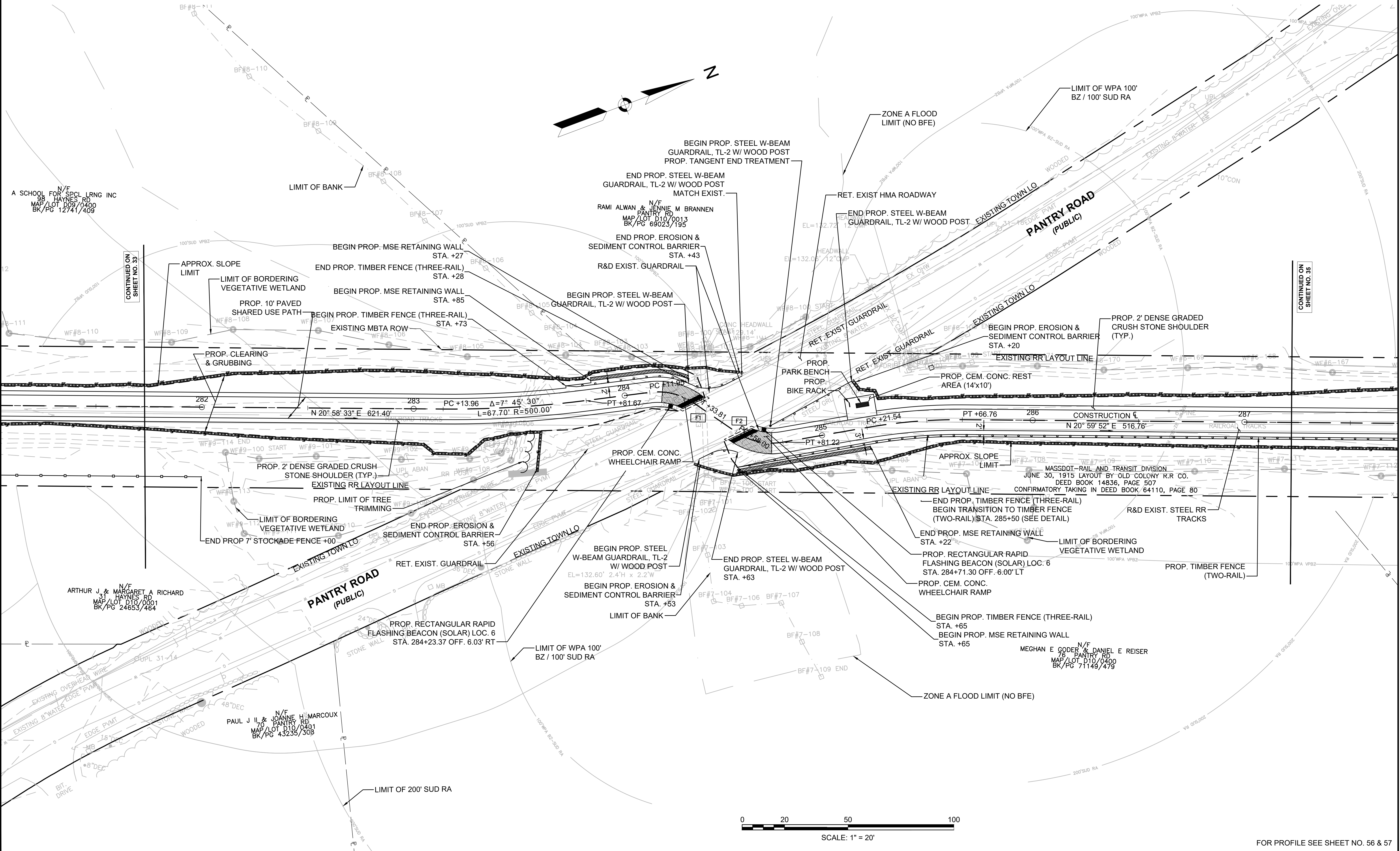
DRAINAGE DETAILS

NONE

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

SUBURBY			
BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	34	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			



CONTINUED ON SHEET NO. 33

CONTINUED ON SHEET NO. 35

FOR PROFILE SEE SHEET NO. 56 & 57

FENCING DETAILS

TIMBER FENCE (TWO-RAIL) STA. 284+68 RT TO STA. 288+00 RT  
 TIMBER FENCE (THREE-RAIL) STA. 288+00 RT TO STA. 291+25 RT

HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

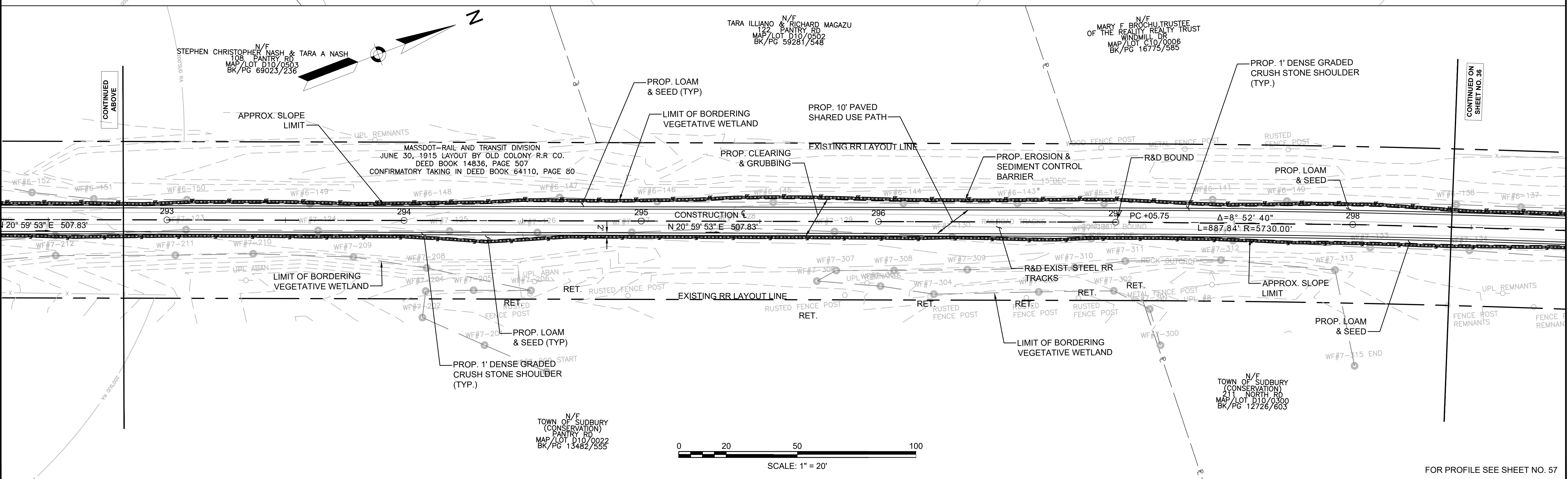
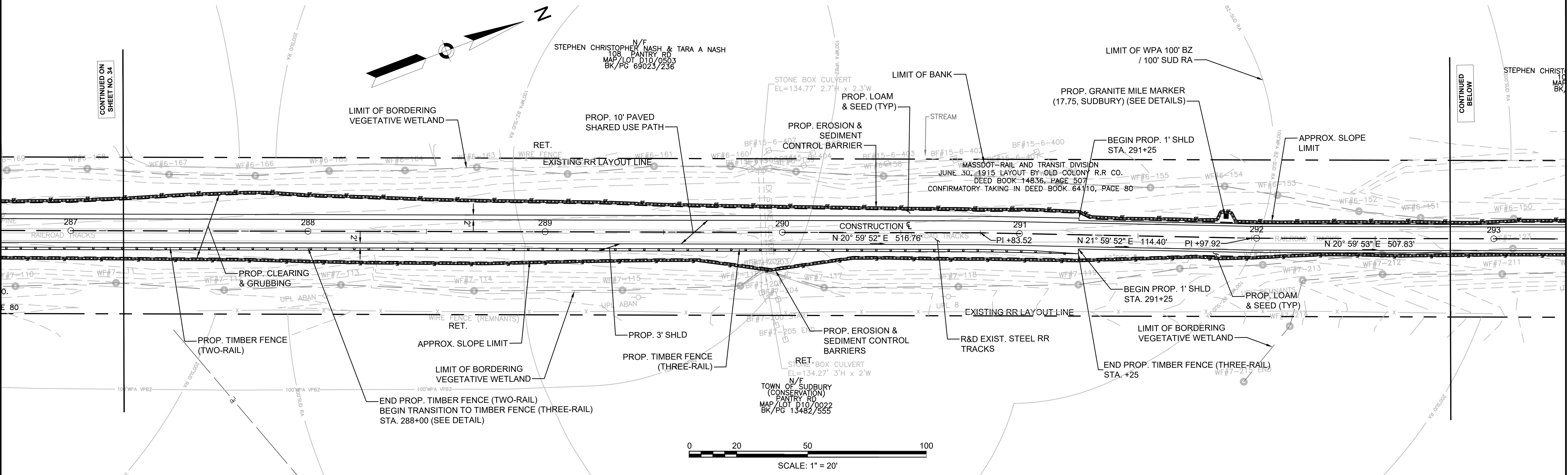
DRAINAGE DETAILS

NONE

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	35	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			



FOR PROFILE SEE SHEET NO. 57

FENCING DETAILS

NONE

HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

DRAINAGE DETAILS

SEE SHEET 194

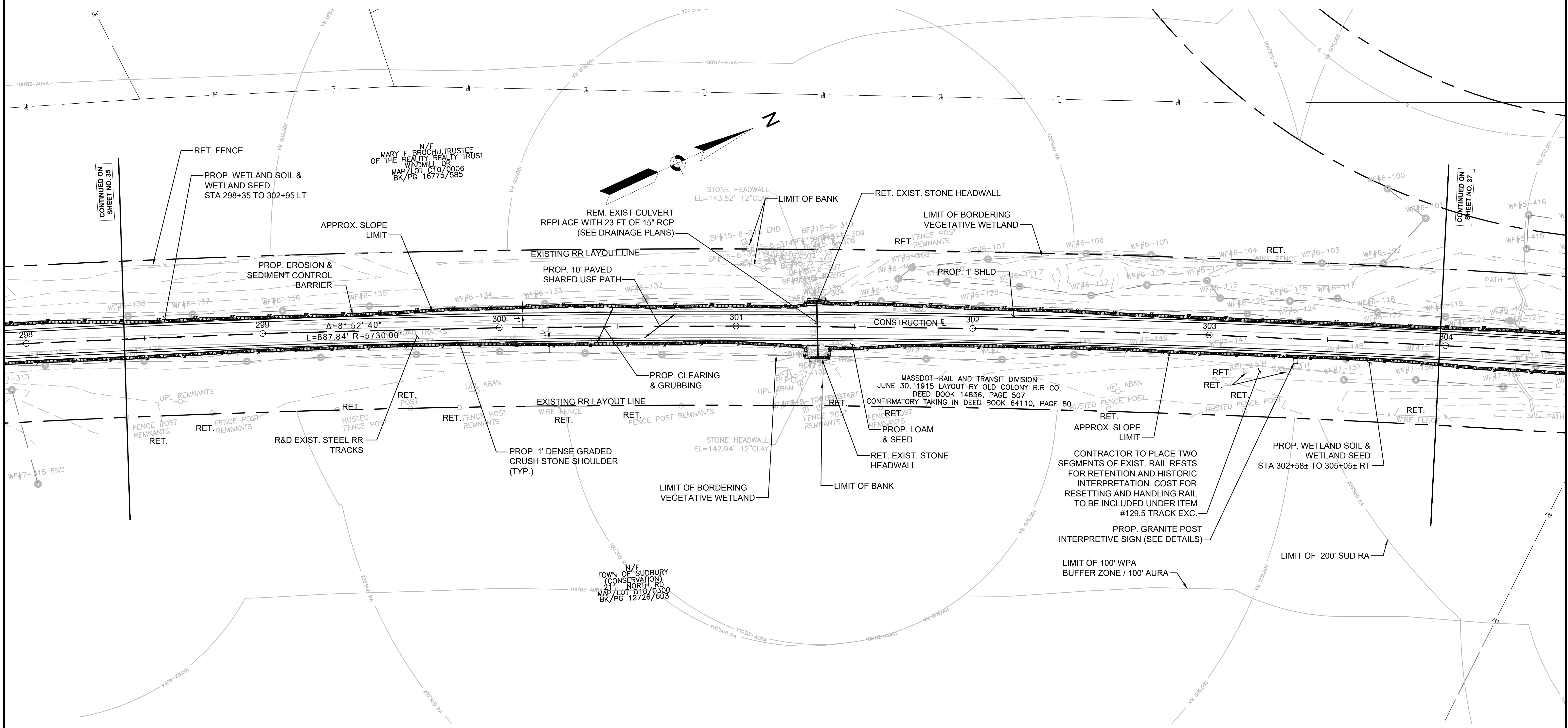
LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

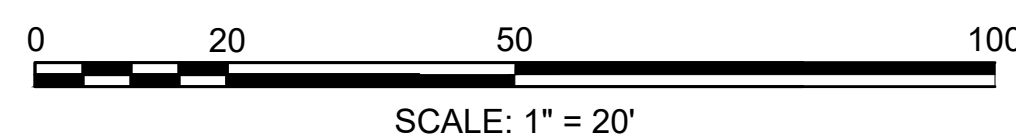
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	36	318
PROJECT FILE NO.		608164	

**CONSTRUCTION PLANS**



CONTINUED ON SHEET NO. 35

CONTINUED ON SHEET NO. 37



FOR PROFILE SEE SHEET NO. 57 & 58

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 305+25 LT TO STA. 306+84 LT  
TIMBER FENCE (THREE-RAIL) STA. 308+79 LT TO STA. 309+78 LT  
TIMBER FENCE (THREE-RAIL) STA. 305+25 RT TO STA. 307+10 RT  
TIMBER FENCE (THREE-RAIL) STA. 308+16 RT TO STA. 315+78 RT

HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

SEE SHEET 162

WATER SUPPLY ALTERATIONS

NONE

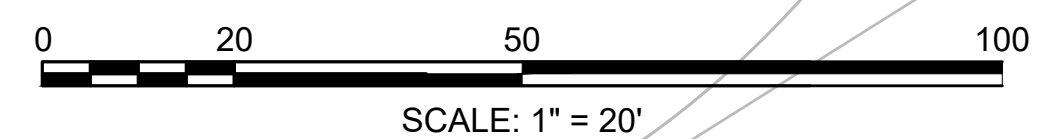
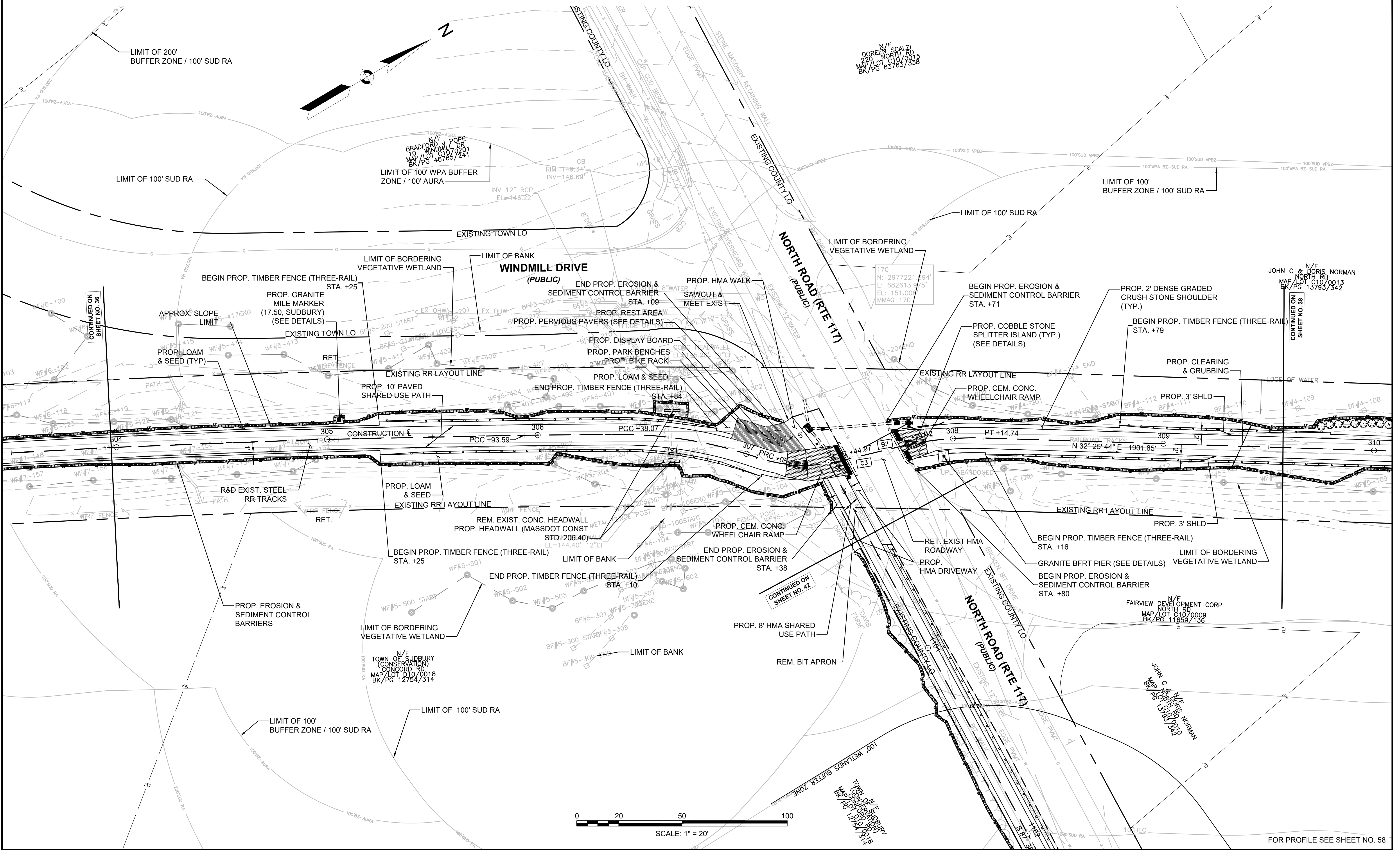
DRAINAGE DETAILS

SEE SHEET 195

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	37	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			



FOR PROFILE SEE SHEET NO. 58

**FENCING DETAILS**

TIMBER FENCE (TWO-RAIL) STA. 308+78 LT TO STA. 309+78 LT  
 TIMBER FENCE (TWO-RAIL) STA. 313+28 LT TO STA. 316+28 LT  
 TIMBER FENCE (TWO-RAIL) STA. 308+15 RT TO STA. 315+78 RT  
 STOCKADE FENCE STA. 316+56 LT TO STA. 319+96 LT  
 STOCKADE FENCE STA. 316+00 RT TO STA. 319+75 RT

**HIGHWAY GUARD DETAILS**

NONE

**TRAFFIC SIGNAL CONDUIT**

NONE

**WATER SUPPLY ALTERATIONS**

NONE

**DRAINAGE DETAILS**

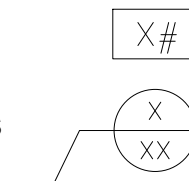
NONE

**PLANT LIST**

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
IG	ILEX GLABRA	INKBERRY	10	2-3' HT.
VAC	VGACCINIUM CORYMBOSUM	HIGHBUSH BLUEBERRY	15	18"-24" HT.

**LEGEND:**

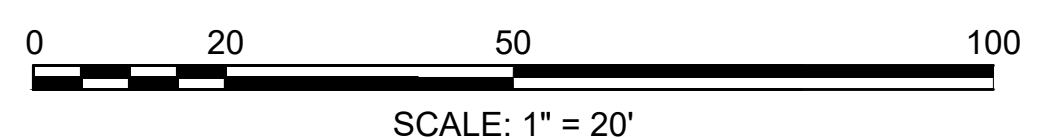
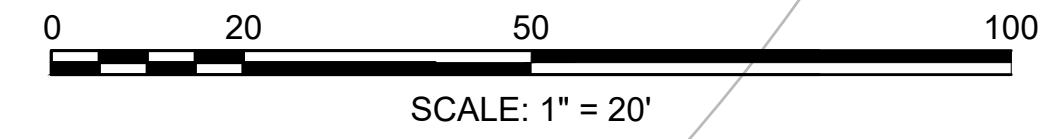
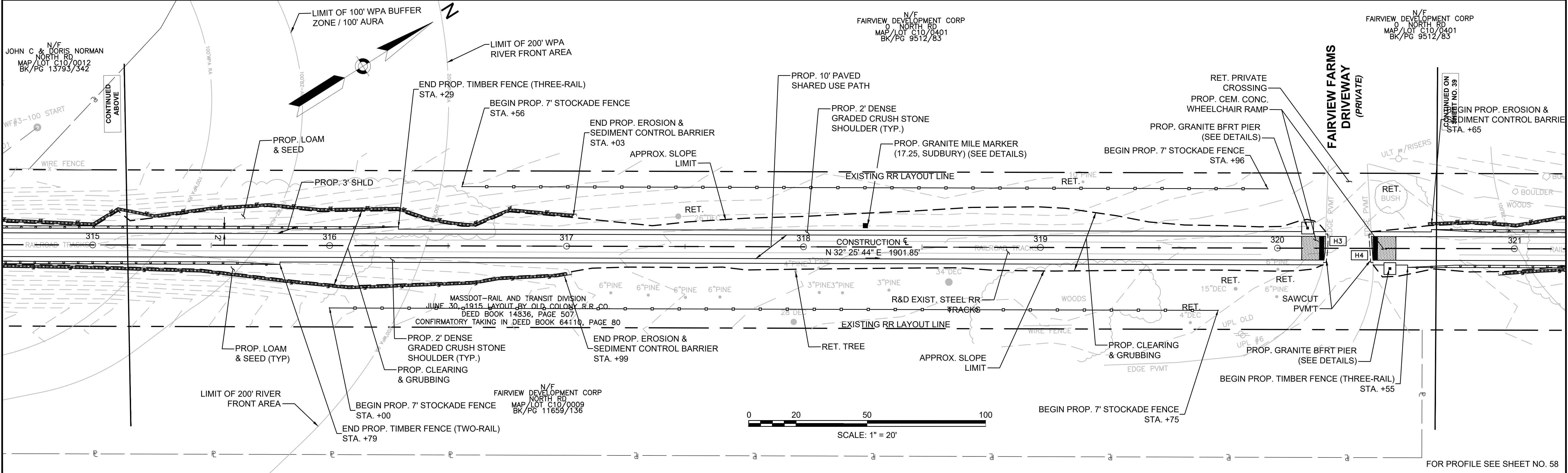
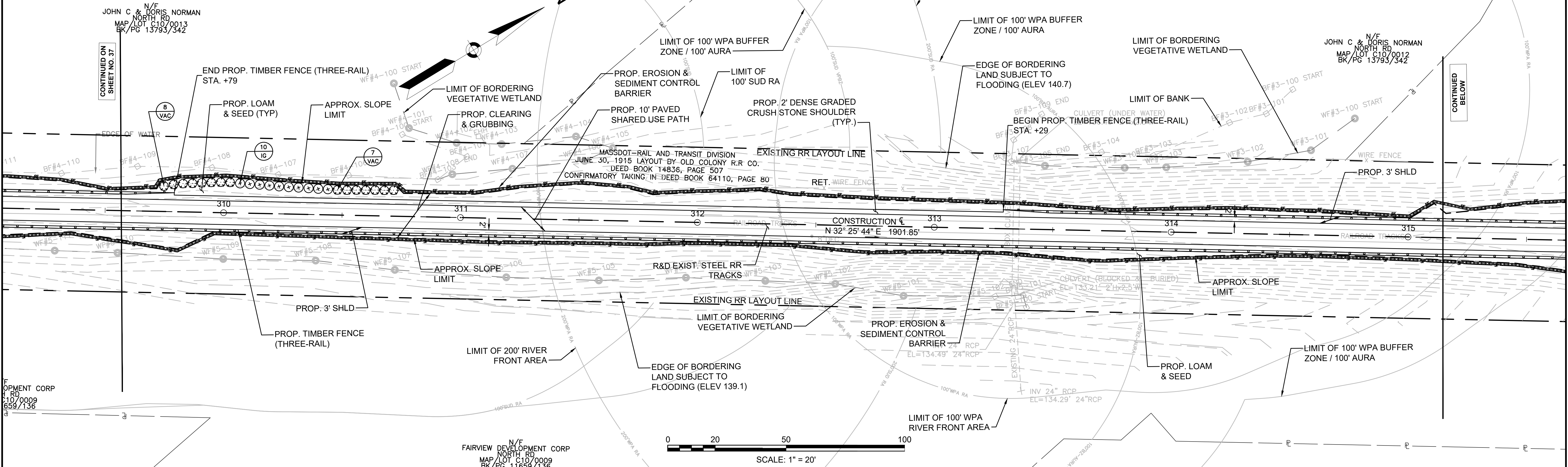
PROPOSED WHEELCHAIR RAMP DETAIL #  
 PLANT QUANTITY AND SPECIES



**SUDBURY BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	38	318
PROJECT FILE NO.		608164	

**CONSTRUCTION PLANS**



FOR PROFILE SEE SHEET NO. 58

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 320+65 LT TO STA. 324+79 LT  
 TIMBER FENCE (THREE-RAIL) STA. 320+55 RT TO STA. 321+79 RT  
 TIMBER FENCE (THREE-RAIL) STA. 322+79 RT TO STA. 329+79 RT

HIGHWAY GUARD DETAILS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

NONE

DRAINAGE DETAILS

NONE

PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
TA	TILIA AMERICANA	AMERICAN LINDEN	26	5-6' HT.
<b>SHRUBS</b>				
IG	ILEX GLABRA	INKBERRY	8	2-3' HT.
ARA	ARONIA ARBUTIFOLIA	RED CHOKEBERRY	13	3-4' HT. B&B
CLA	CLETHRA ALNIFOLIA	SUMMERSWEET	28	18"-24" HT.
ILX	ILEX VERTICILLATA	WINTERBERRY	57	18"-24" HT.
VAC	VGACCINIUM CORYMBOSUM	HIGHBUSH BLUEBERRY	7	18"-24" HT.

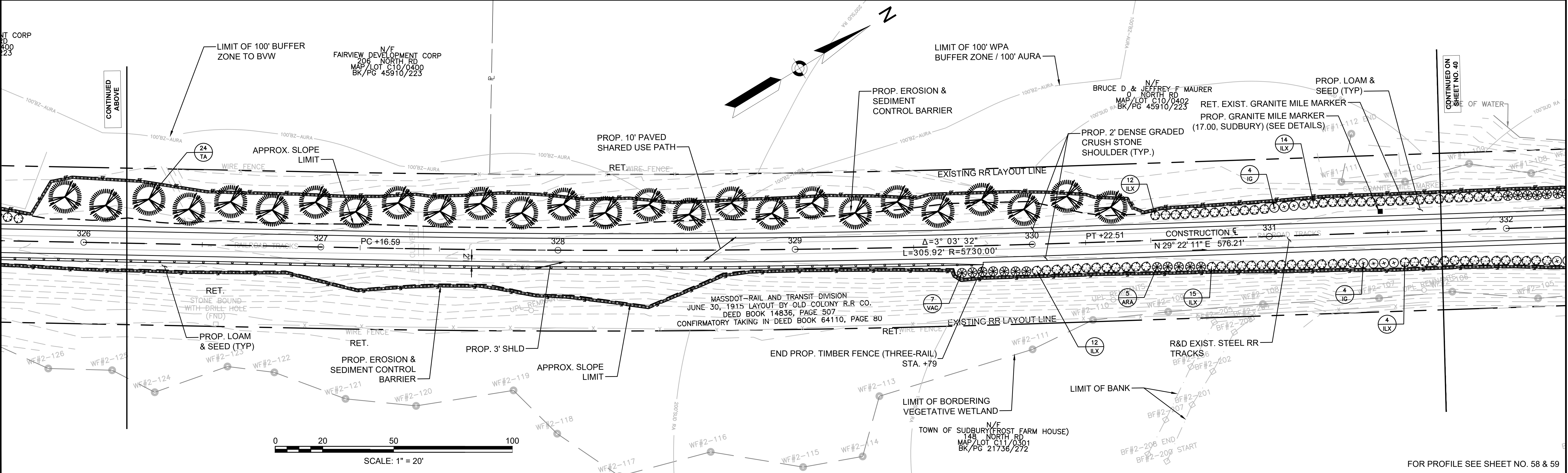
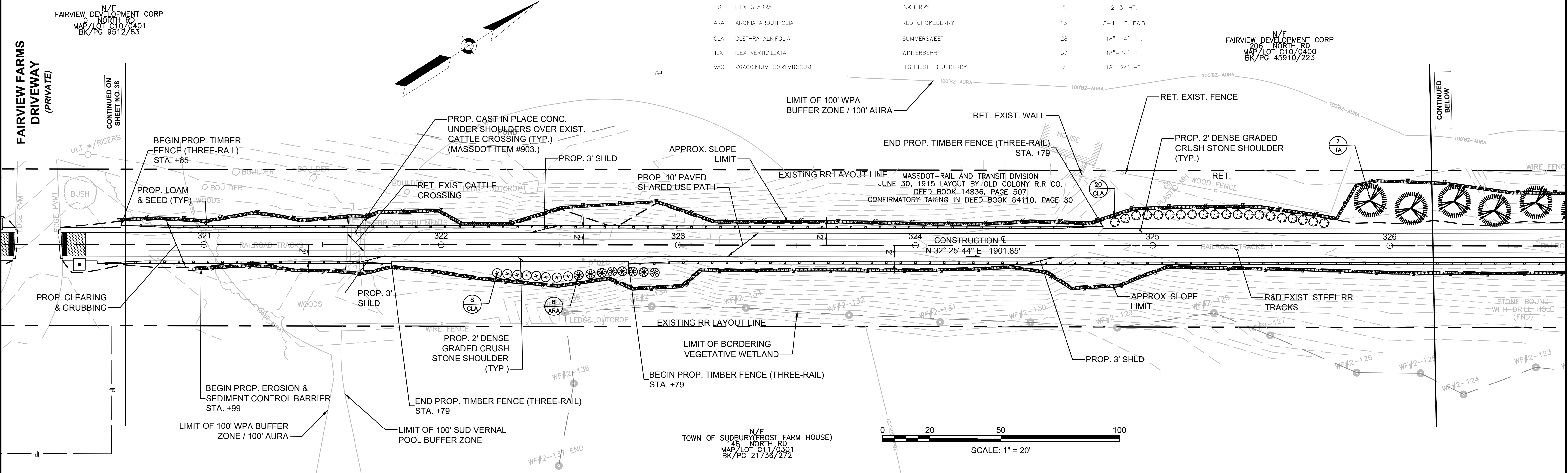
LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # [Symbol]  
 PLANT QUANTITY AND SPECIES [Symbol]

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	39	318

PROJECT FILE NO. 608164  
**CONSTRUCTION PLANS**



FOR PROFILE SEE SHEET NO. 58 & 59

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 332+16 LT TO STA. 333+11 LT

HIGHWAY GUARD DETAILS

NONE

WATER SUPPLY ALTERATIONS

NONE

TRAFFIC SIGNAL CONDUIT

NONE

DRAINAGE DETAILS

NONE

PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
SHRUBS				
IG	ILEX GLABRA	INKBERRY	14	2-3' HT.
ARA	ARONIA ARBUTIFOLIA	RED CHOKEBERRY	5	3-4' HT. B&B
CLA	CLETHRA ALNIFOLIA	SUMMERSWEET	6	18"-24" HT.
ILX	ILEX VERTICILLATA	WINTERBERRY	81	18"-24" HT.
VAC	VGACCINIUM CORYMBOSUM	HIGHBUSH BLUEBERRY	6	18"-24" HT.

LEGEND:

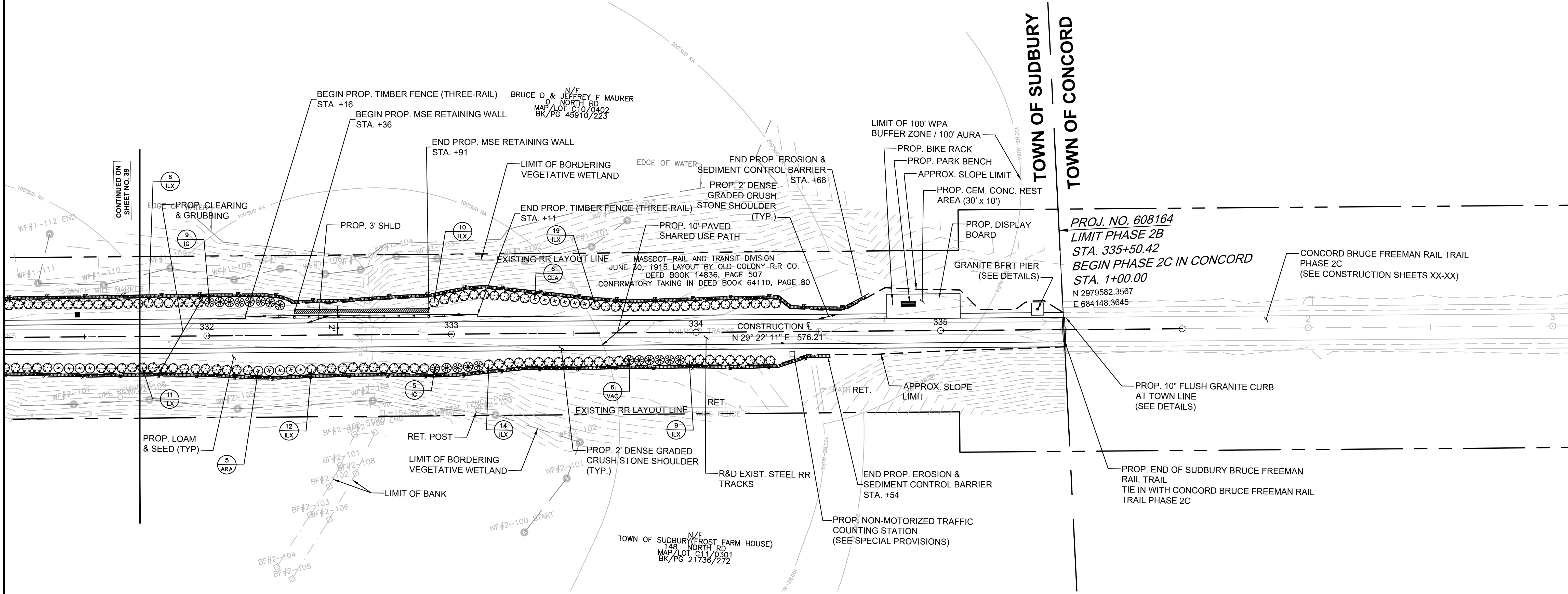
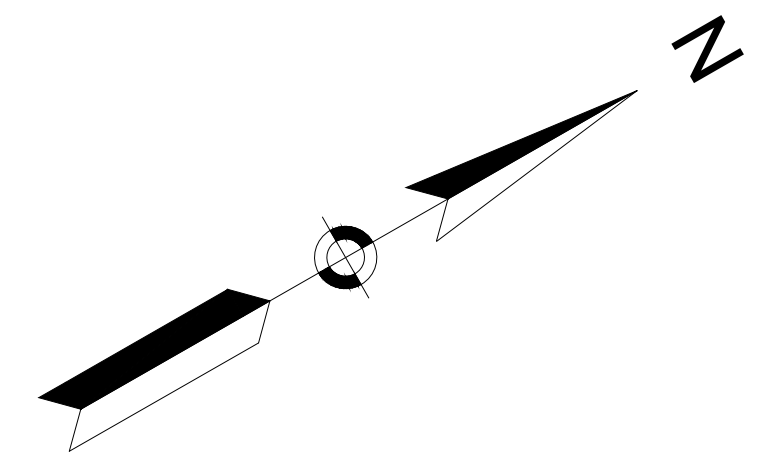
PROPOSED WHEELCHAIR RAMP DETAIL #

X#

PLANT QUANTITY AND SPECIES

X  
XX

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	40	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			



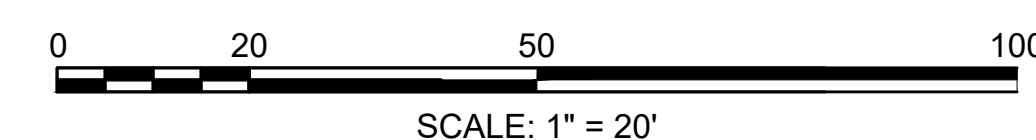
TOWN OF SUDBURY  
TOWN OF CONCORD

PROJ. NO. 608164  
LIMIT PHASE 2B  
STA. 335+50.42  
BEGIN PHASE 2C IN CONCORD  
STA. 1+00.00  
N 2979582.3567  
E 684148.3645

CONCORD BRUCE FREEMAN RAIL TRAIL  
PHASE 2C  
(SEE CONSTRUCTION SHEETS XX-XX)

PROP. 10" FLUSH GRANITE CURB  
AT TOWN LINE  
(SEE DETAILS)

PROP. END OF SUDBURY BRUCE FREEMAN  
RAIL TRAIL  
TIE IN WITH CONCORD BRUCE FREEMAN RAIL  
TRAIL PHASE 2C



CONTINUED ON  
SHEET NO. 39

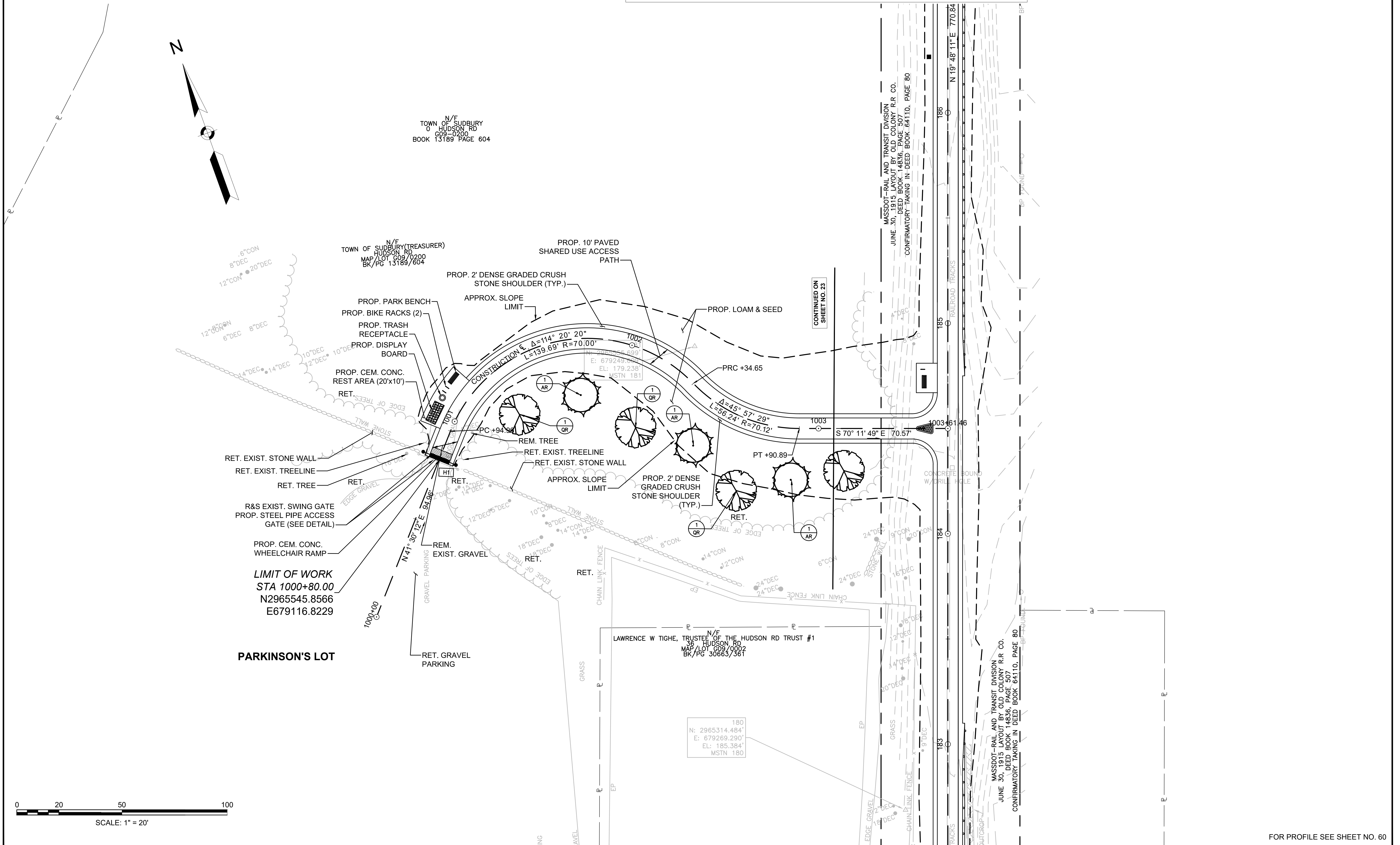


FENCING DETAILS NONE  
 HIGHWAY GUARD DETAILS NONE  
 TRAFFIC SIGNAL CONDUIT NONE  
 WATER SUPPLY ALTERATIONS NONE  
 DRAINAGE DETAILS NONE

PLANT LIST				
KEY	BOTANICAL NAME	COMMON NAME	QTY.	SIZE
<b>TREES</b>				
AR	ACER RUBRUM 'OCTOBER GLORY'	OCTOBER GLORY MAPLE	3	2-2.5" CAL.
QR	QUERCUS RUBRA	NORTHERN RED OAK	3	2-2.5" CAL.

LEGEND:  
 PROPOSED WHEELCHAIR RAMP DETAIL # X#  
 PLANT QUANTITY AND SPECIES X  
 XX

SUBURBY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	41	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			



FOR PROFILE SEE SHEET NO. 60

FENCING DETAILS

TIMBER FENCE (THREE-RAIL) STA. 1102+10 RT TO STA. 1103+20 RT

HIGHWAY GUARD DETAILS

STEEL W-BEAM (TL-3) W/WOOD POST & RUB RAIL  
STA. 1100+51 LT TO STA. 1103+20 RT  
TIMBER GUARDRAIL W/ RUB RAIL  
STA. 1104+34 TO STA. 1107+27

TRAFFIC SIGNAL CONDUIT

NONE

WATER SUPPLY ALTERATIONS

SEE SHEET 200

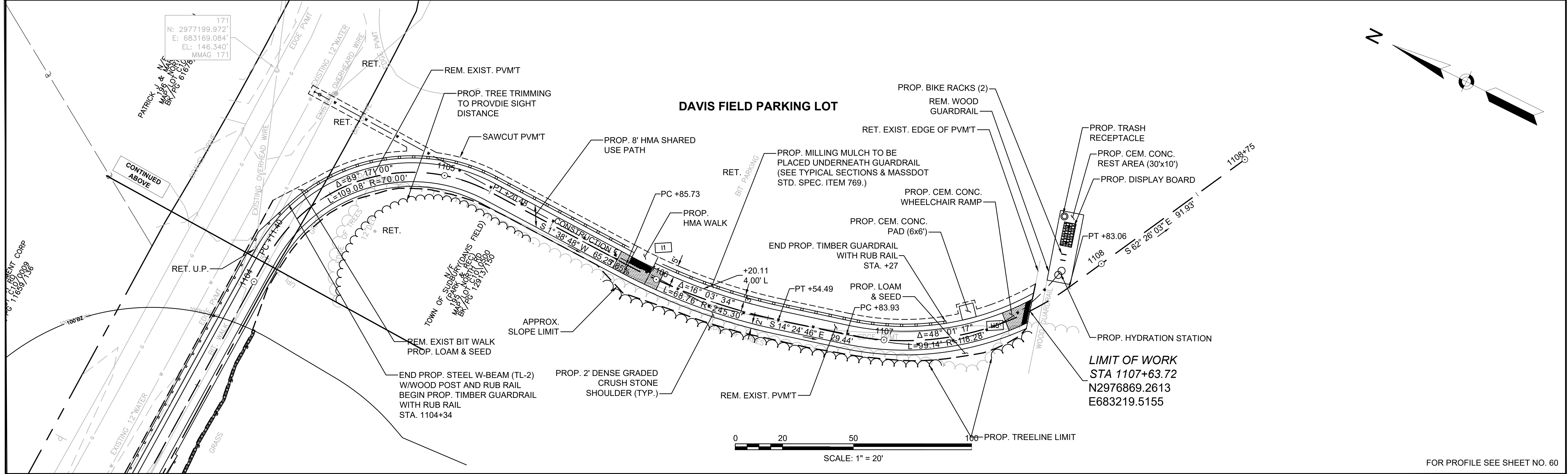
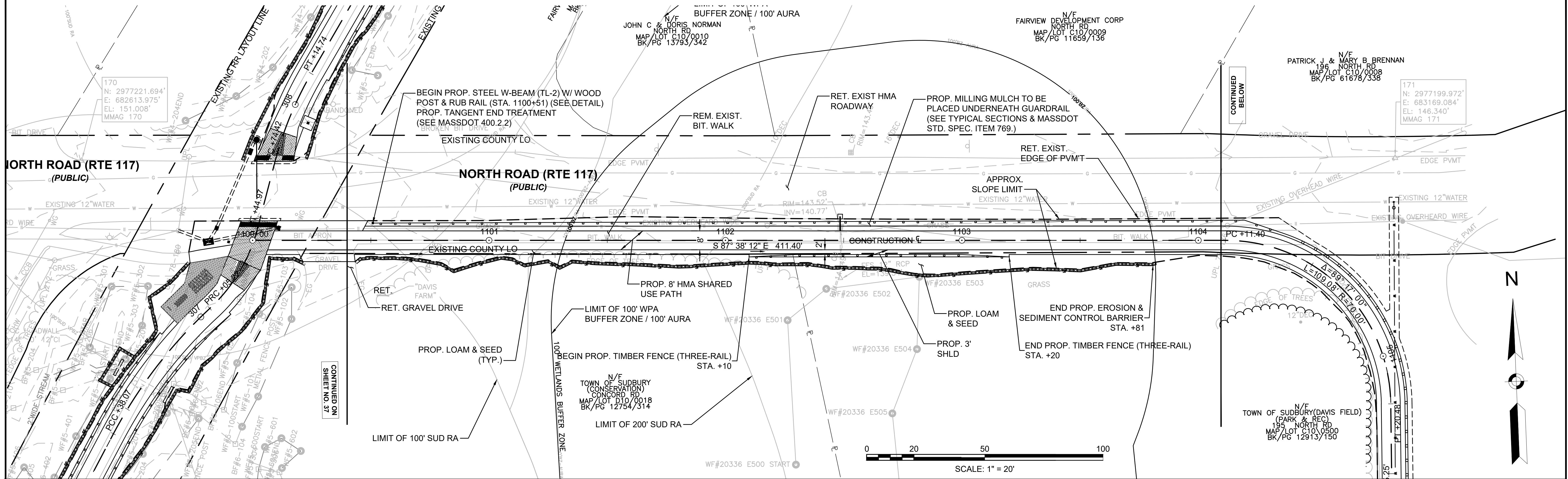
DRAINAGE DETAILS

SEE SHEET 200

LEGEND:

PROPOSED WHEELCHAIR RAMP DETAIL # X#

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	42	318
PROJECT FILE NO.		608164	
CONSTRUCTION PLANS			



**TIMBER RAIL FENCE**  
 (SEE SHEET 21 FOR DETAILS)  
 STA. 1+00 LT TO 1+98 LT  
 STA. 2+03 LT TO 5+98 LT  
 STA. 6+03 LT TO 12+93 LT

**HIGHWAY GUARD DETAILS**  
 NONE

**TRAFFIC SIGNAL CONDUIT**  
 NONE

**WATER SUPPLY ALTERATIONS**  
 NONE

**DRAINAGE DETAILS**  
 SEE SHEET 18

CONCORD  
 BRUCE FREEMAN RAIL TRAIL PHASE 2D

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	-	2	32

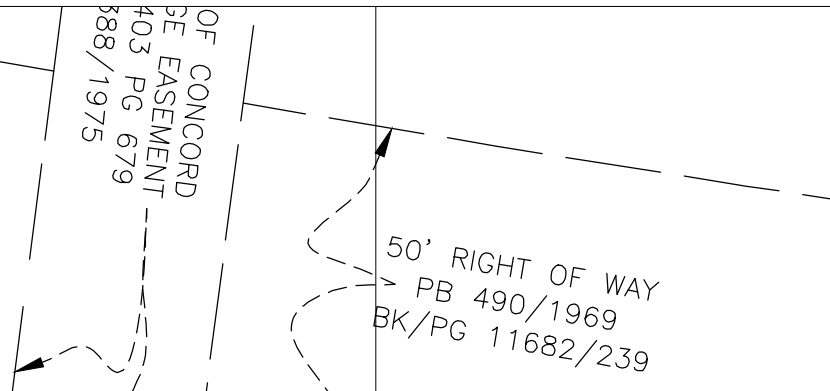
PROJECT FILE NO. 608164  
 CONSTRUCTION PLANS

DENSE FOLIAGE

LIMIT OF 100' BUFFER ZONE TO BVW

TOWN OF CONCORD  
 50' FLOWAGE EASEMENT  
 PB 388/1975

N/F HOME!  
 ELIZABETH Z. HOMES  
 BK/PG 20733/306  
 #211  
 STONE ROOT LN.



CONTINUED BELOW

BEGIN PROJECT  
 STA. 1+00.00  
 MEET BFRT PHASE 2D - SUDBURY  
 N2979582.3567  
 E684148.3645

N/F COMMONWEALTH OF MASSACHUSETTS  
 (EXECUTIVE OFFICE OF  
 TRANSPORTATION & CONSTRUCTION)  
 BK/PG 14609/302

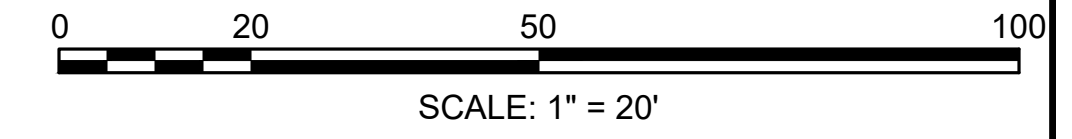
TOWN OF SUDBURY  
 TOWN OF CONCORD

NOTE:  
 1.) WHEN YARD WASTE IS FOUND OUTSIDE OF LIMITS OF WORK,  
 THE CONTRACTOR SHALL VERIFY REMOVAL WITH THE  
 ENGINEER PRIOR TO BEGINNING WORK.

N/F TOWN OF CONCORD  
 BK/PG 1089/192  
 #48B  
 FITCHBURG TPKE

N/F BRIAN M. LEWIS AND  
 CHRISTINA I. LEWIS  
 BK/PG 59423/ 352  
 #201  
 STONE ROOT LN.

N/F ANDREW W. DICK AND  
 DEBORAH A. ELLWOOD  
 BK/PG 56939/ 124  
 #173  
 STONE ROOT LN.



CONTINUED ABOVE

PROP. TIMBER RAIL FENCE  
 (STA. 6+03 TO STA. 12+93 LT)  
 (SEE SHEET 21 FOR DETAILS)

50' RIGHT OF WAY  
 PB 490/1969  
 BK/PG 11682/239

CONTINUED ON SHEET 3

PROP. SHRUBS (TYP.)

PROP. 2' DGCS SHOULDER (TYP.)

PROP. CONST. R & PGL

PROP. COMPOST TOPSOIL & SEED (TYP.)  
 PROP. 10.0' FULL DEPTH HMA SHARED USE PATH (TYP.)

PROP. GRANITE MARKER (16.75, CONCORD) (SEE SHEET 22 FOR DETAILS)

PROP. SLOPE LIMIT (TYP.)

PROP. CLEARING AND GRUBBING (TYP.)

REM. EXIST. RAILS AND TIES (TYP.)

PROP. 2' DGCS SHOULDER (TYP.)

N/F COMMONWEALTH OF MASSACHUSETTS  
 (EXECUTIVE OFFICE OF  
 TRANSPORTATION & CONSTRUCTION)  
 BK/PG 14609/302

PROP. COMPOST TOPSOIL & SEED (TYP.)

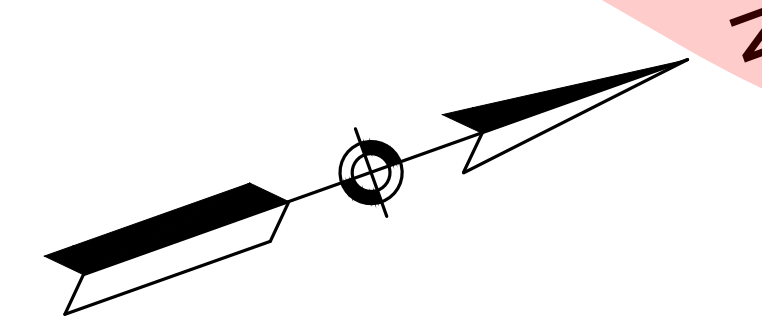
PROP. SLOPE LIMIT (TYP.)

N/F TOWN OF CONCORD  
 BK/PG 1089/192  
 #48B  
 FITCHBURG TPKE

N/F OLD PICKARD FARMS TRUST II  
 BK/PG 12794/501

#17X  
 STONE ROOT LN.

FOR PROFILE: SEE SHEET NO. 5





**TIMBER RAIL FENCE**  
 (SEE SHEET 21 FOR DETAILS)  
 STA. 20+86 LT TO 23+26 LT  
 STA. 23+31 LT TO 27+75 LT

**HIGHWAY GUARD DETAILS**  
 NONE

**TRAFFIC SIGNAL CONDUIT**  
 NONE

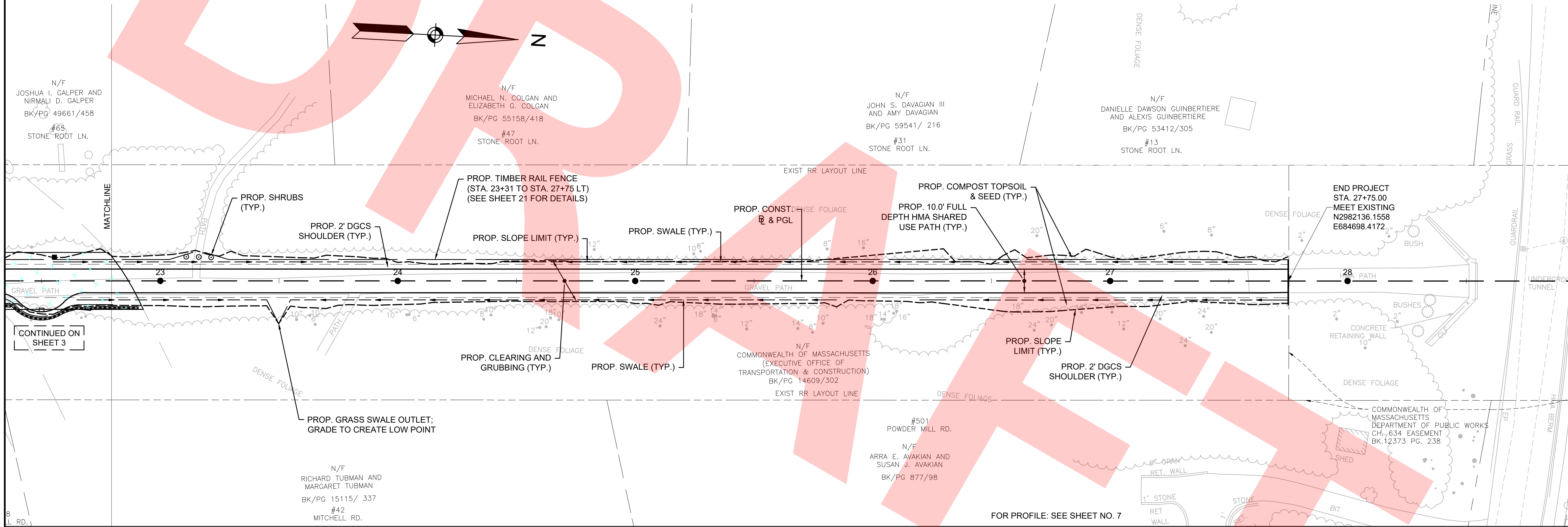
**WATER SUPPLY ALTERATIONS**  
 NONE

**DRAINAGE DETAILS**  
 SEE SHEET 20

CONCORD  
 BRUCE FREEMAN RAIL TRAIL PHASE 2D

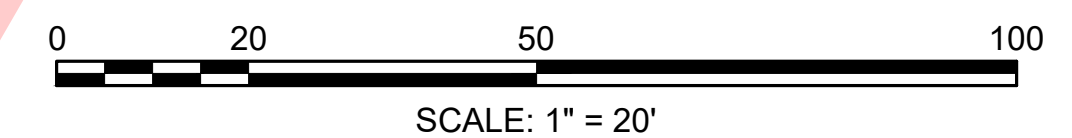
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	-	4	32
PROJECT FILE NO.		608164	

CONSTRUCTION PLANS



CONTINUED ON SHEET 3

FOR PROFILE: SEE SHEET NO. 7



SHEET LEFT BLANK FOR  
TOWN OF CONCORD

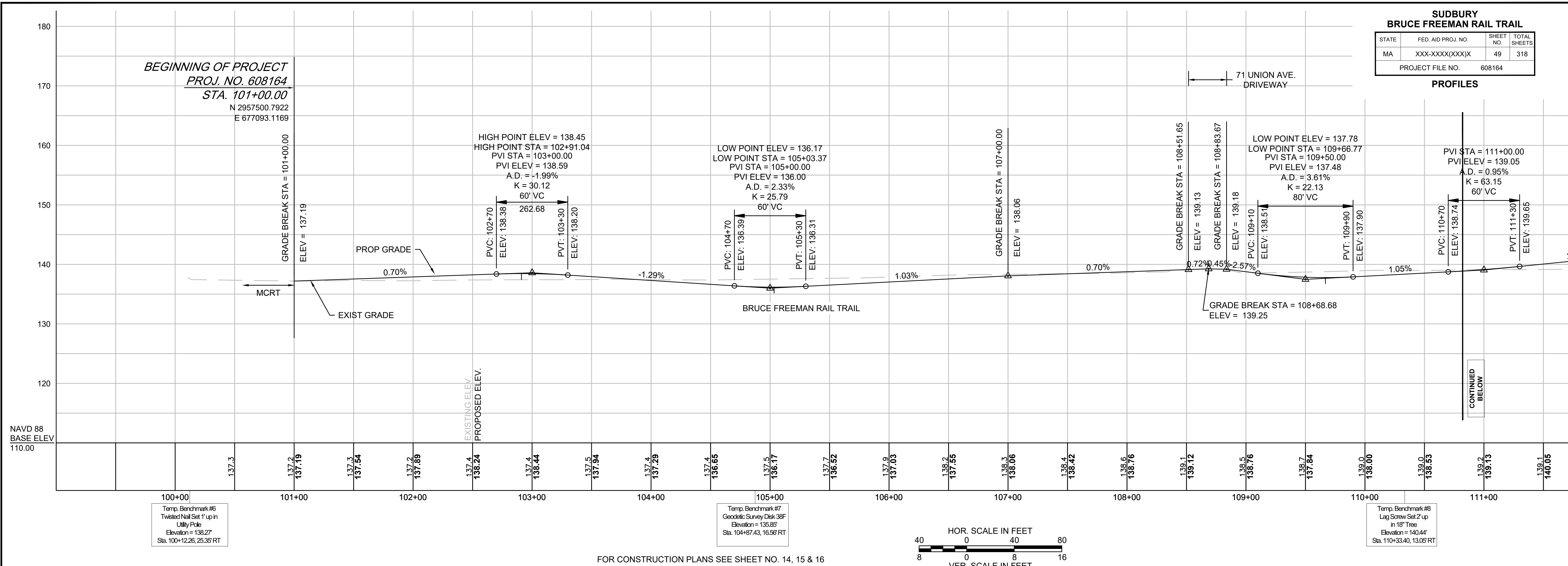
SHEET LEFT BLANK FOR  
TOWN OF CONCORD

SHEET LEFT BLANK FOR  
TOWN OF CONCORD



SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	49	318
PROJECT FILE NO.		608164	

**PROFILES**

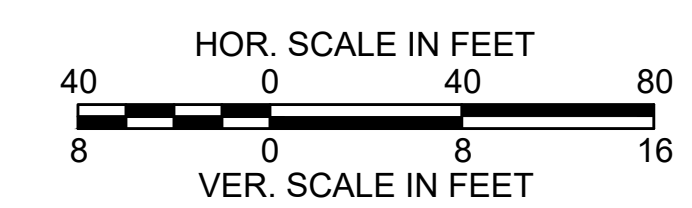


**BEGINNING OF PROJECT**  
**PROJ. NO. 608164**  
**STA. 101+00.00**  
 N 2957500.7922  
 E 677093.1169

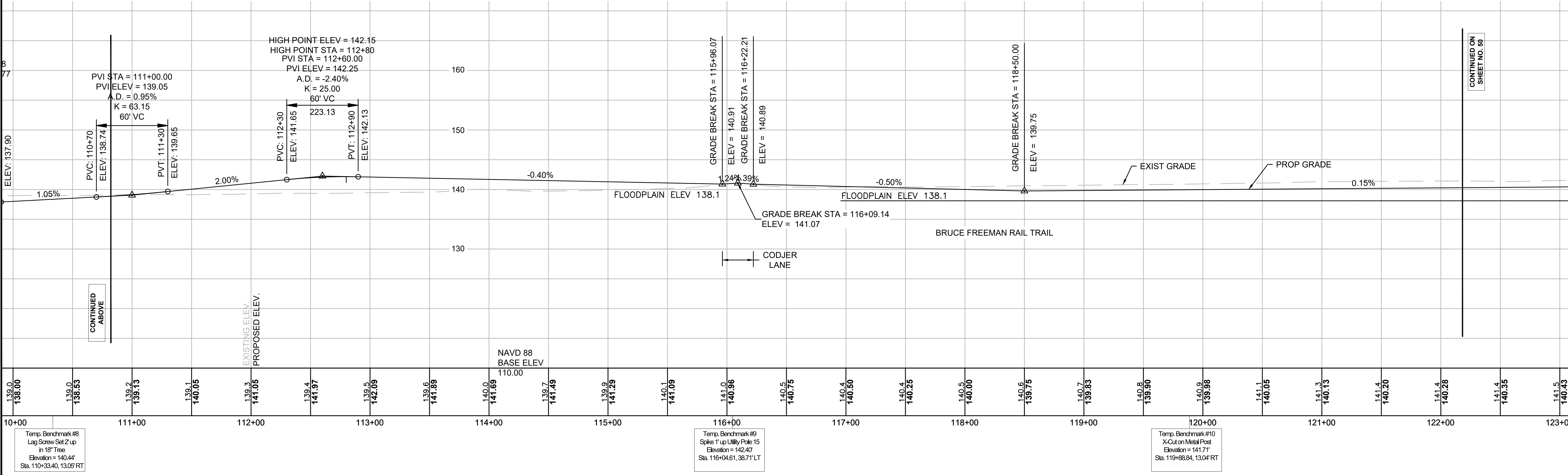
Temp. Benchmark #6  
 Twisted Nail Set 1" up in  
 Utility Pole  
 Elevation = 138.27  
 Sta. 100+12.26, 25.35 RT

Temp. Benchmark #7  
 Geodetic Survey Disk 38F  
 Elevation = 136.85  
 Sta. 104+97.43, 16.56 RT

Temp. Benchmark #8  
 Lag Screw Set 2" up in 18" Tree  
 Elevation = 140.44  
 Sta. 110+33.40, 13.05 RT



FOR CONSTRUCTION PLANS SEE SHEET NO. 14, 15 & 16



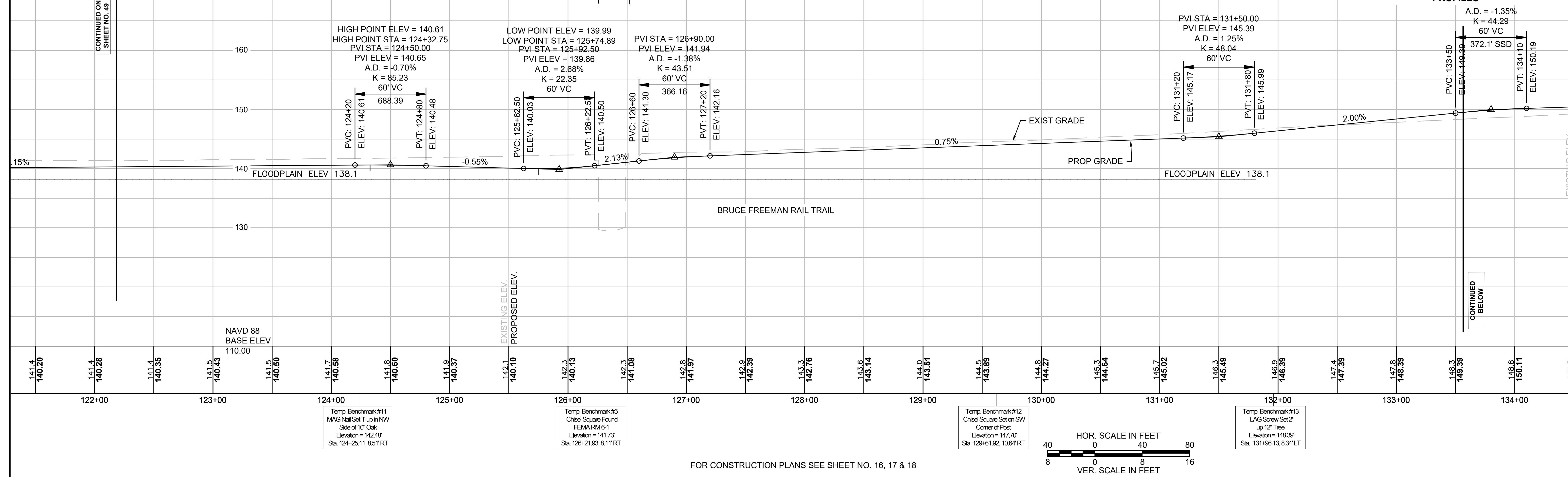
Temp. Benchmark #8  
 Lag Screw Set 2" up in 18" Tree  
 Elevation = 140.44  
 Sta. 110+33.40, 13.05 RT

Temp. Benchmark #9  
 Spike 1" up Utility Pole 15  
 Elevation = 142.40  
 Sta. 116+04.61, 38.71 LT

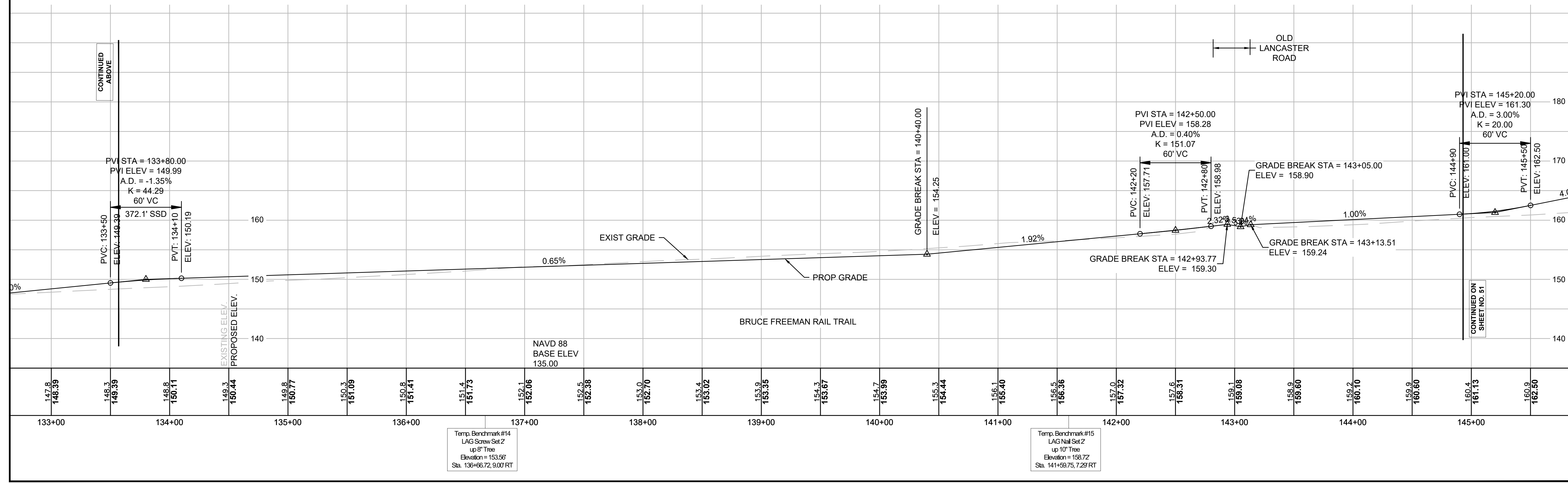
Temp. Benchmark #10  
 X-Cut on Metal Post  
 Elevation = 141.71  
 Sta. 119+88.84, 13.04 RT

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	50	318
PROJECT FILE NO.		608164	

**PROFILES**



FOR CONSTRUCTION PLANS SEE SHEET NO. 16, 17 & 18



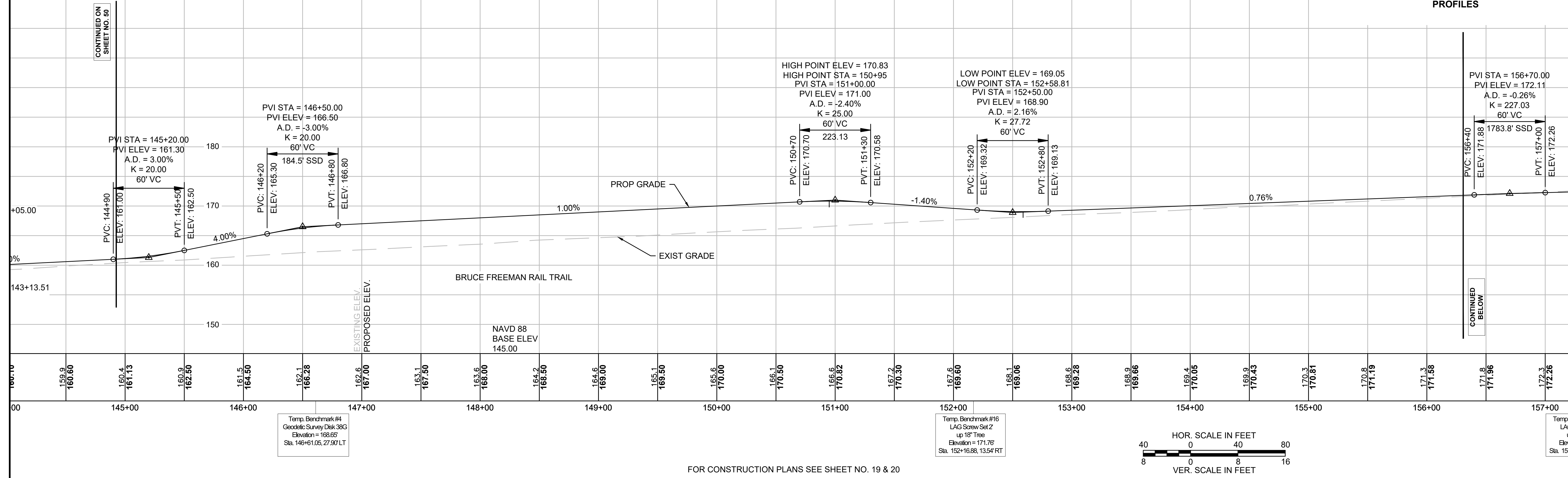
CONTINUED ON  
SHEET NO. 49

CONTINUED  
BELOW

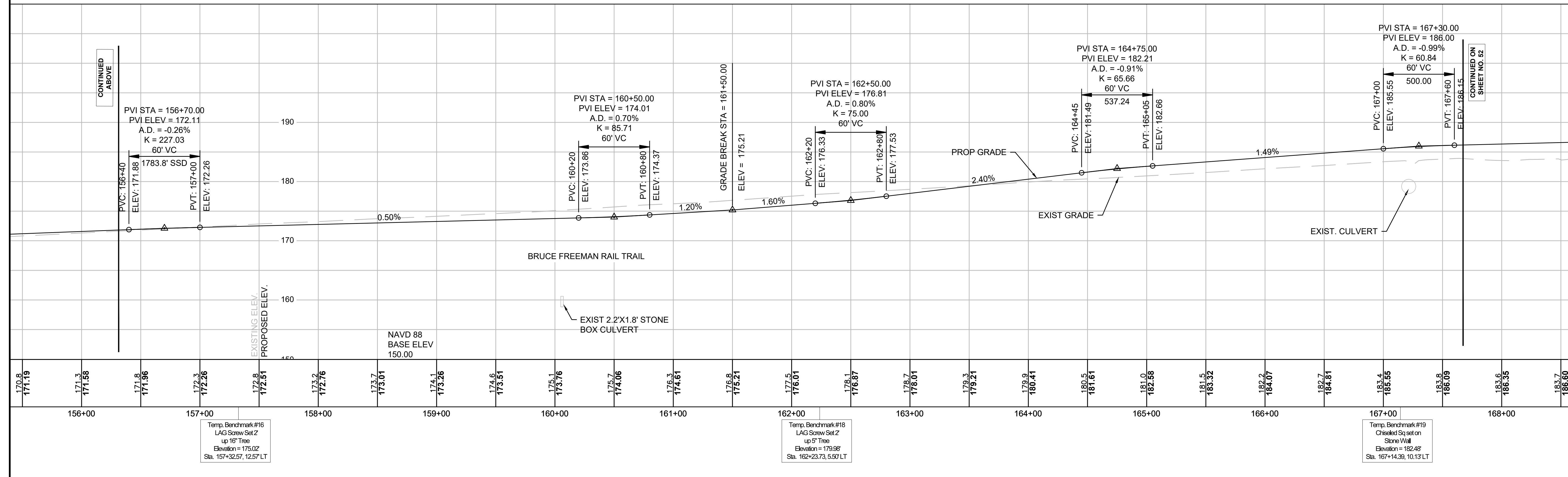
CONTINUED  
ABOVE

CONTINUED ON  
SHEET NO. 51

**PROFILES**

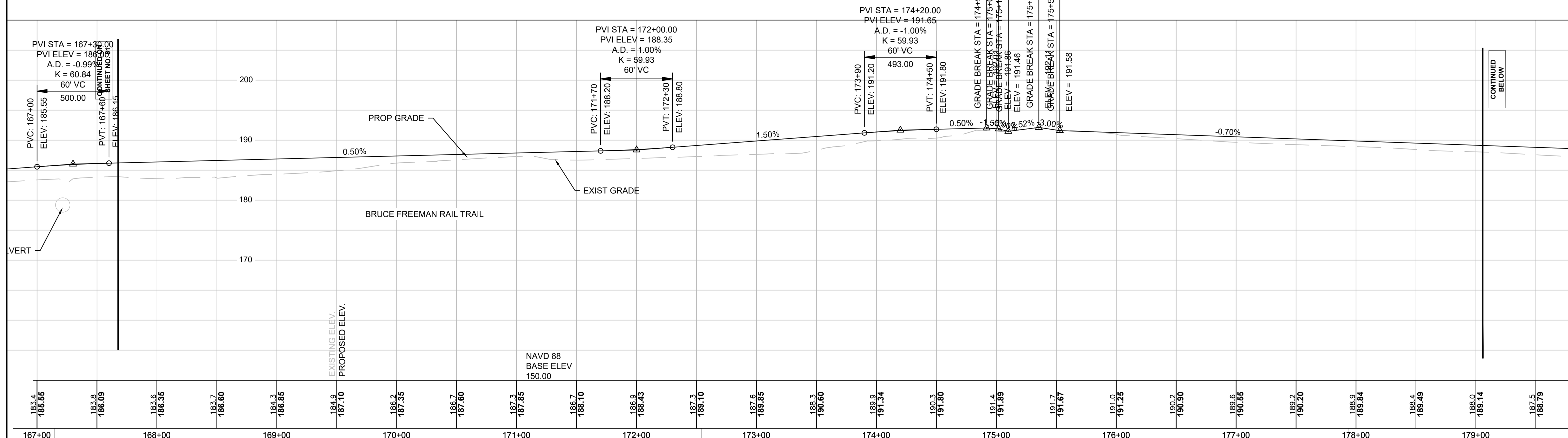


FOR CONSTRUCTION PLANS SEE SHEET NO. 19 & 20



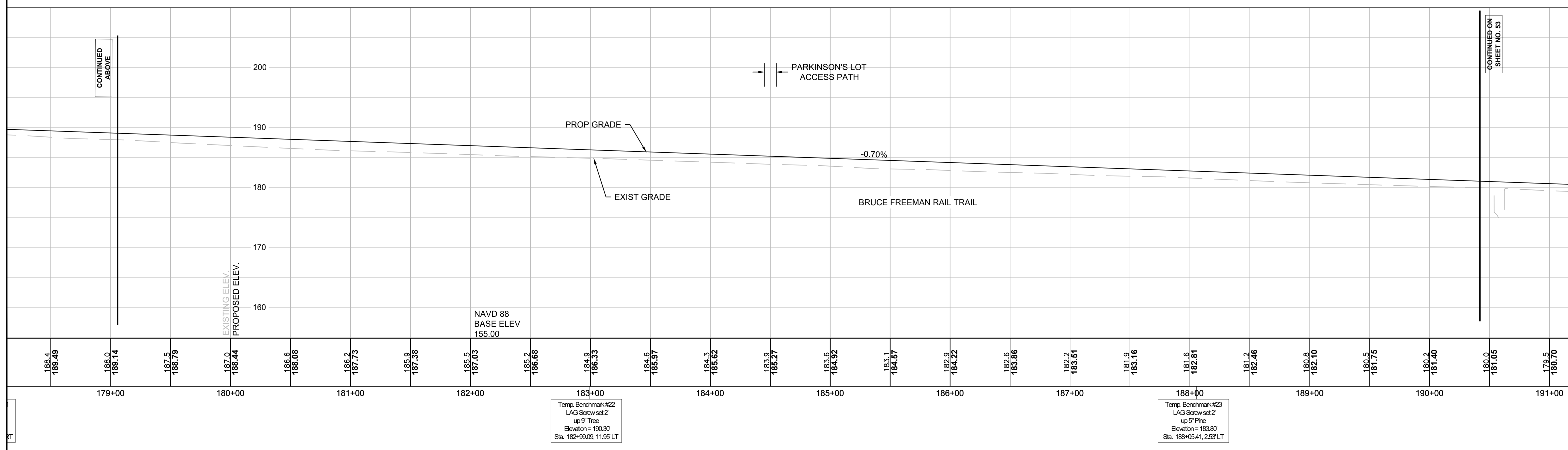
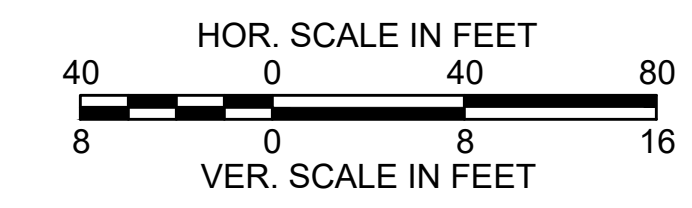
SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	52	318
PROJECT FILE NO.		608164	

**PROFILES**



167+00	168+00	169+00	170+00	171+00	172+00	173+00	174+00	175+00	176+00	177+00	178+00	179+00
183.4 185.55	183.6 186.35	183.7 186.60	184.3 186.85	184.9 187.10	186.2 187.35	186.7 187.60	187.3 187.85	186.7 188.10	186.9 188.43	187.3 189.10	187.6 189.85	188.3 190.60
Temp. Benchmark #19 Checked Sq set on Stone Wall Elevation = 182.48' Sta. 167+14.39, 10.13' LT							Temp. Benchmark #20 Checked Sq set on Boulder Elevation = 190.04' Sta. 172+54.34, 24.71' LT		Temp. Benchmark #22/6 Dill Hole set in Bolt Over Fire Hydrant's Main Outlet Elevation = 191.27' Sta. 175+03.09, 35.32 RT		Temp. Benchmark #21 LAG Screw set 2' up 10" Tree Elevation = 190.30' Sta. 177+93.51, 10.98 RT	

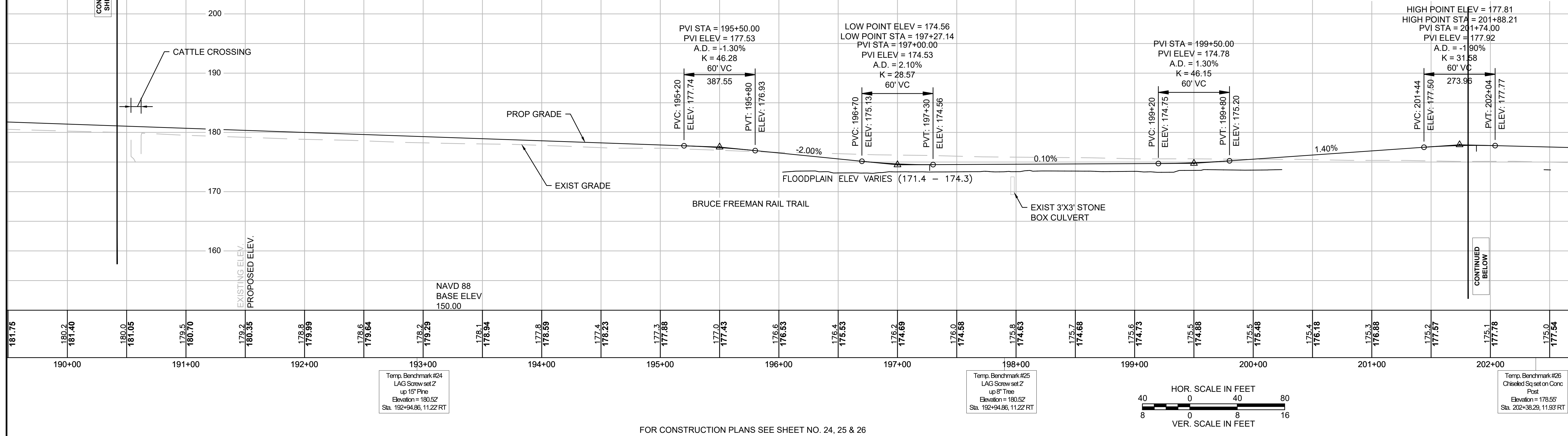
FOR CONSTRUCTION PLANS SEE SHEET NO. 20, 21, 22 & 23



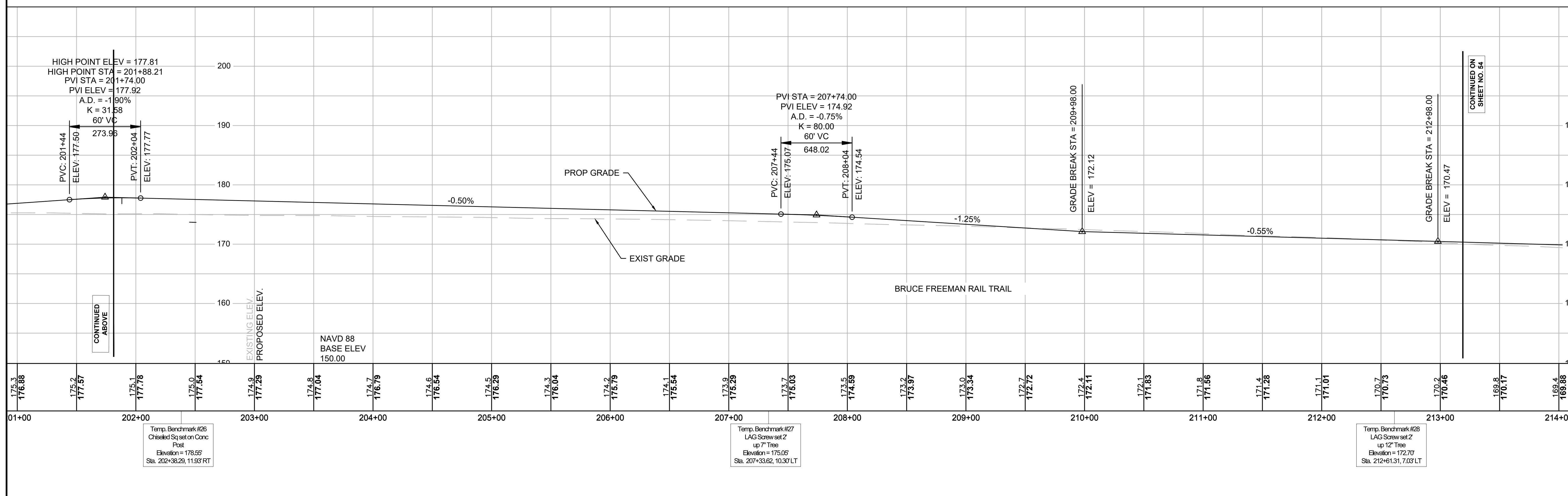
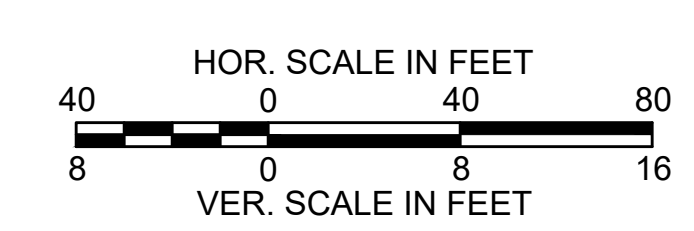
179+00	180+00	181+00	182+00	183+00	184+00	185+00	186+00	187+00	188+00	189+00	190+00	191+00
188.4 189.49	188.0 189.14	187.5 188.79	187.0 188.44	186.6 188.08	186.2 187.73	185.9 187.38	185.5 187.03	185.2 186.68	184.9 186.33	184.6 185.97	184.3 185.62	183.9 185.27
				Temp. Benchmark #22 LAG Screw set 2' up 9" Tree Elevation = 190.30' Sta. 182+99.09, 11.95 LT					Temp. Benchmark #23 LAG Screw set 2' up 5" Pine Elevation = 183.80' Sta. 188+05.41, 2.53' LT			

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	53	318
PROJECT FILE NO.		608164	

**PROFILES**

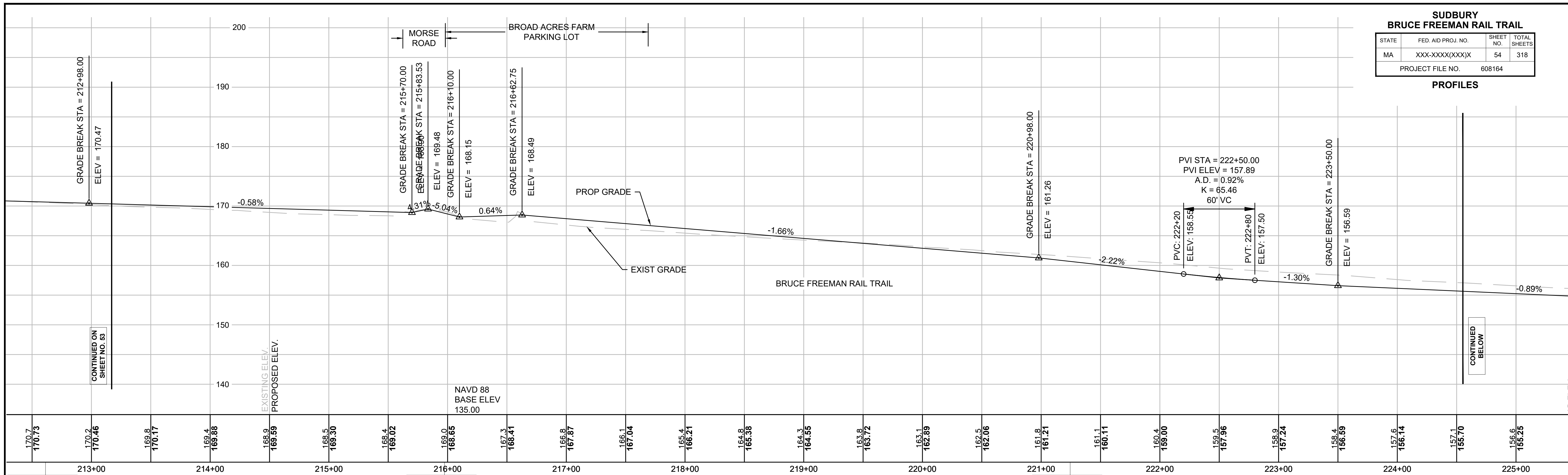


FOR CONSTRUCTION PLANS SEE SHEET NO. 24, 25 & 26



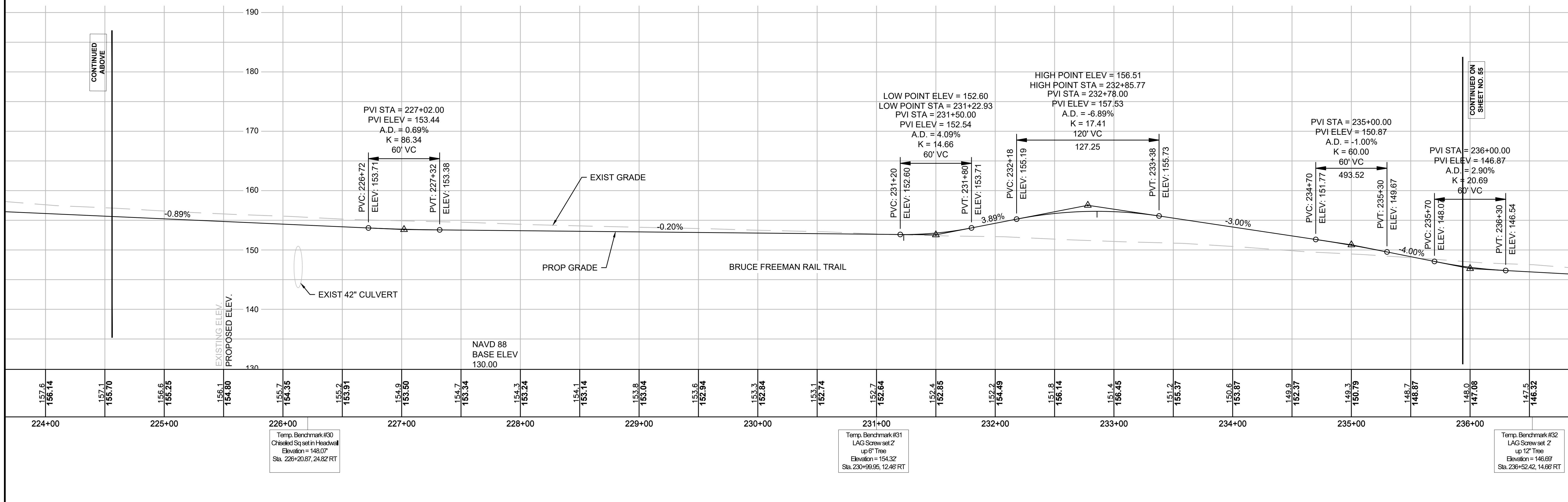
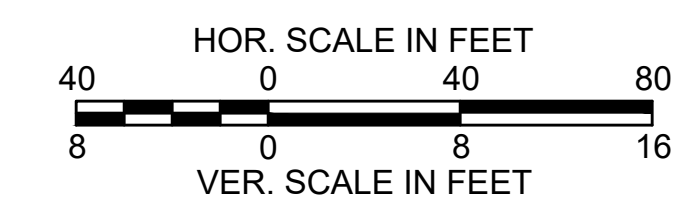
SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	54	318
PROJECT FILE NO.		608164	

**PROFILES**



Station	Existing Elev.	Proposed Elev.
213+00	170.7	170.73
214+00	169.8	170.17
215+00	169.4	169.88
216+00	169.9	169.59
217+00	166.8	167.87
218+00	166.1	167.04
219+00	164.3	164.55
220+00	163.8	163.72
221+00	161.8	161.21
222+00	160.4	159.00
223+00	159.5	157.36
224+00	158.9	157.24
225+00	157.6	156.14

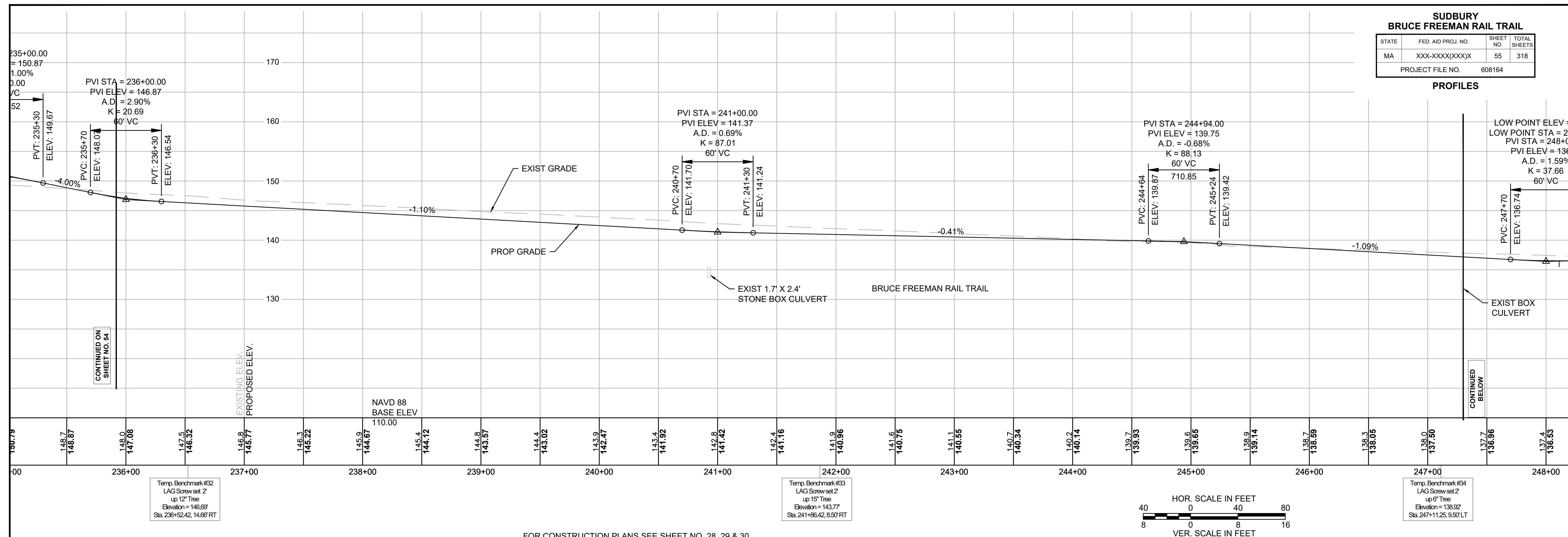
FOR CONSTRUCTION PLANS SEE SHEET NO. 26, 27 & 28



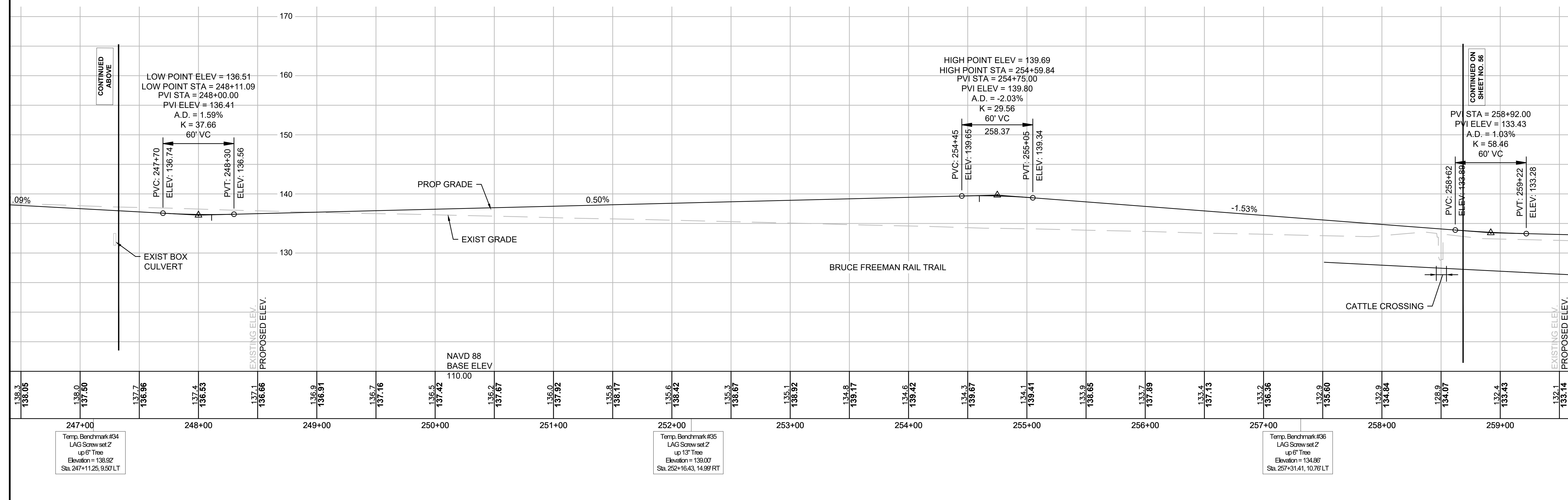
Station	Existing Elev.	Proposed Elev.
224+00	157.6	156.14
225+00	157.1	155.70
226+00	155.2	153.91
227+00	154.9	153.50
228+00	154.7	153.34
229+00	154.1	153.14
230+00	153.3	152.84
231+00	152.7	152.64
232+00	152.4	152.85
233+00	151.8	156.14
234+00	151.2	155.37
235+00	149.3	150.79
236+00	147.5	146.32

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	55	318
PROJECT FILE NO.		608164	

**PROFILES**

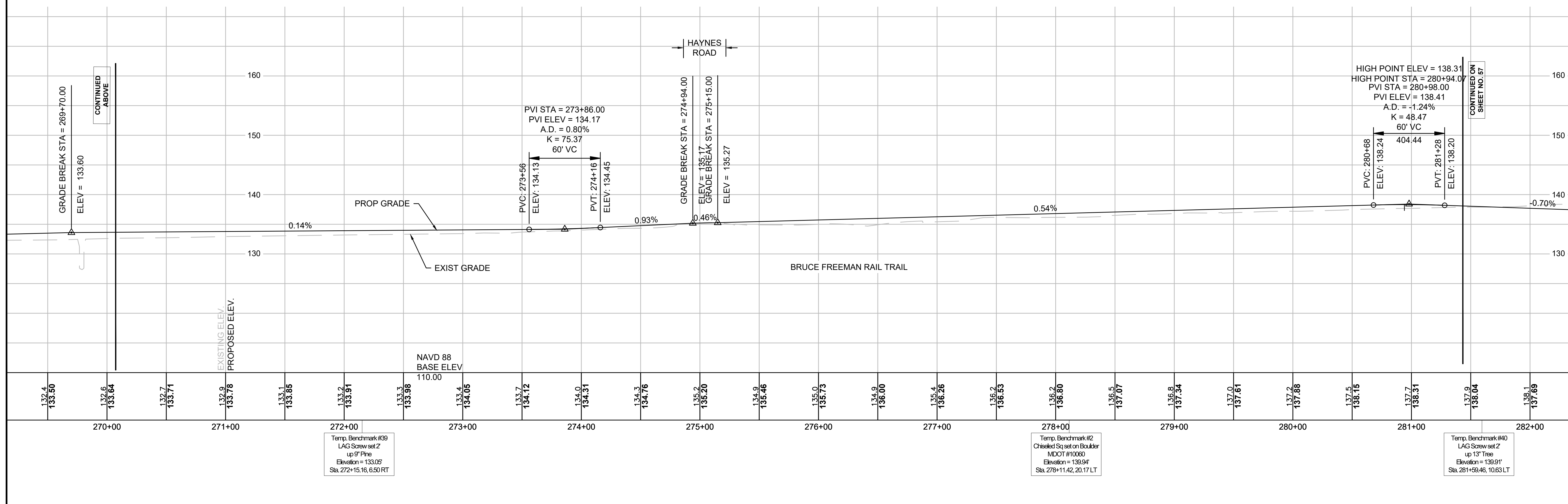
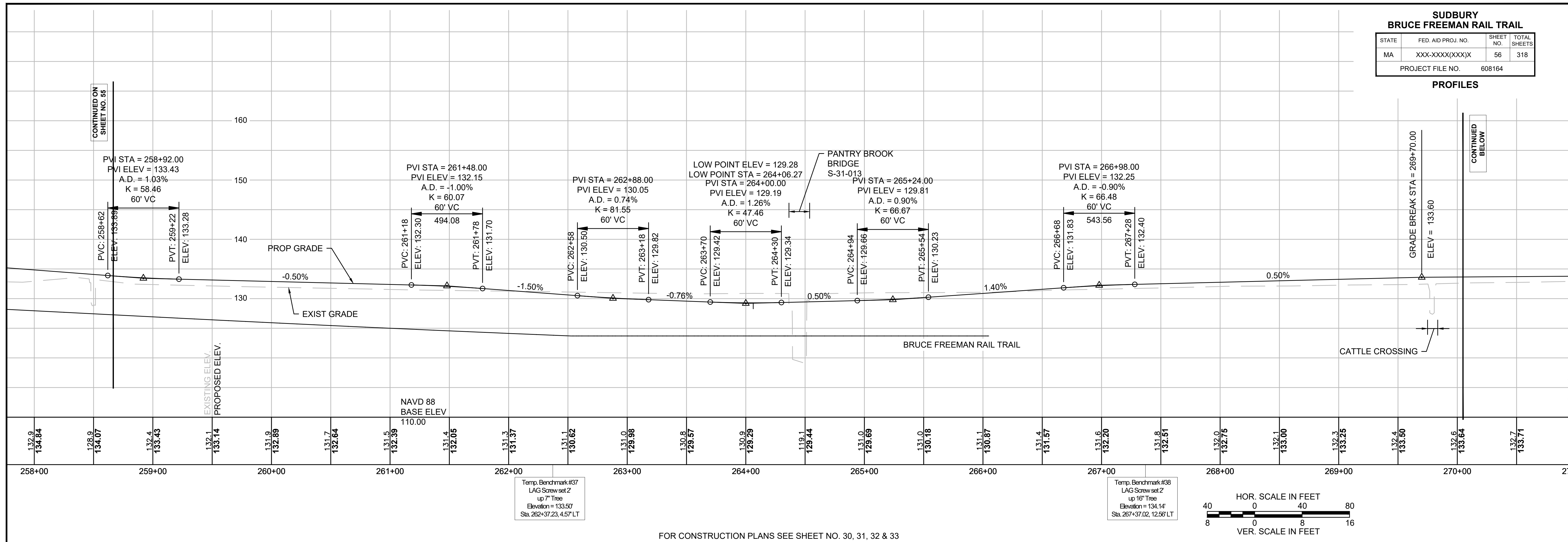


FOR CONSTRUCTION PLANS SEE SHEET NO. 28, 29 & 30



SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	56	318
PROJECT FILE NO.		608164	

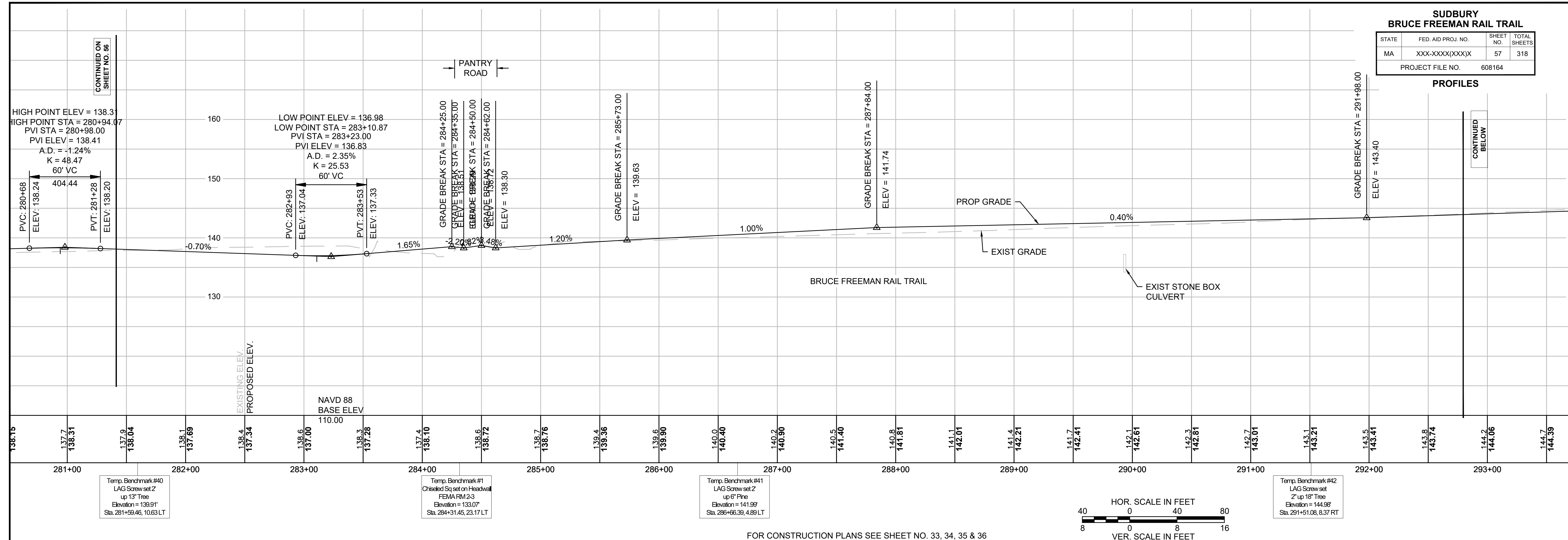
PROFILES



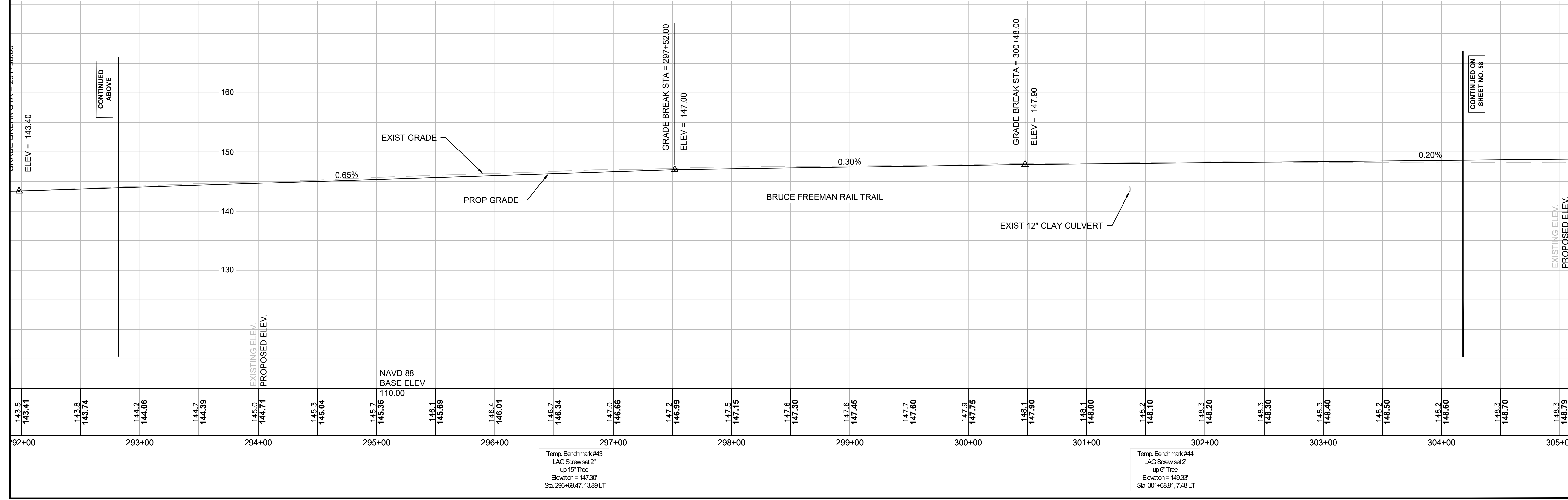
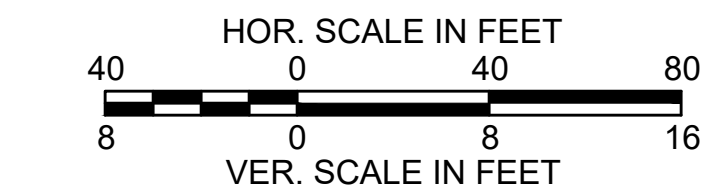


SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	57	318
PROJECT FILE NO.		608164	

**PROFILES**

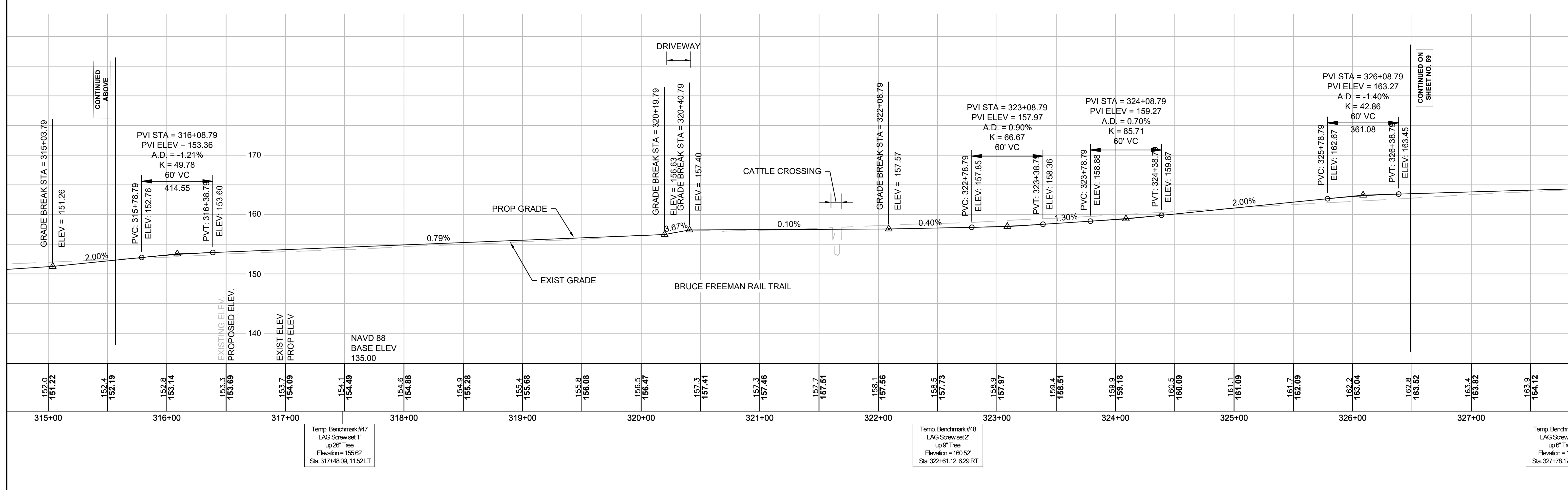
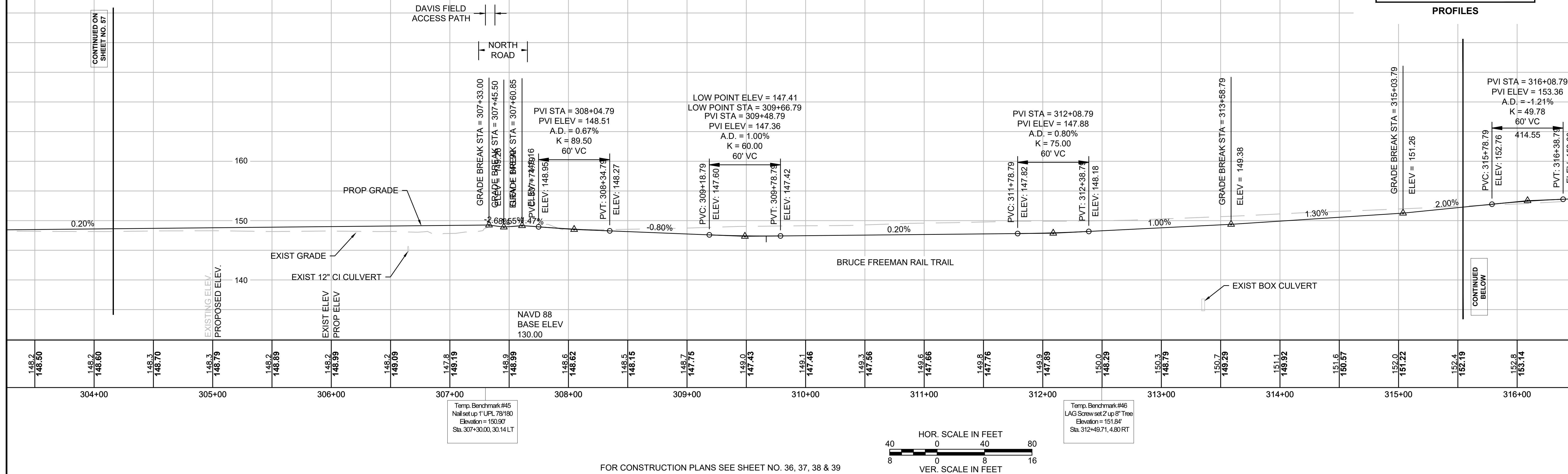


FOR CONSTRUCTION PLANS SEE SHEET NO. 33, 34, 35 & 36



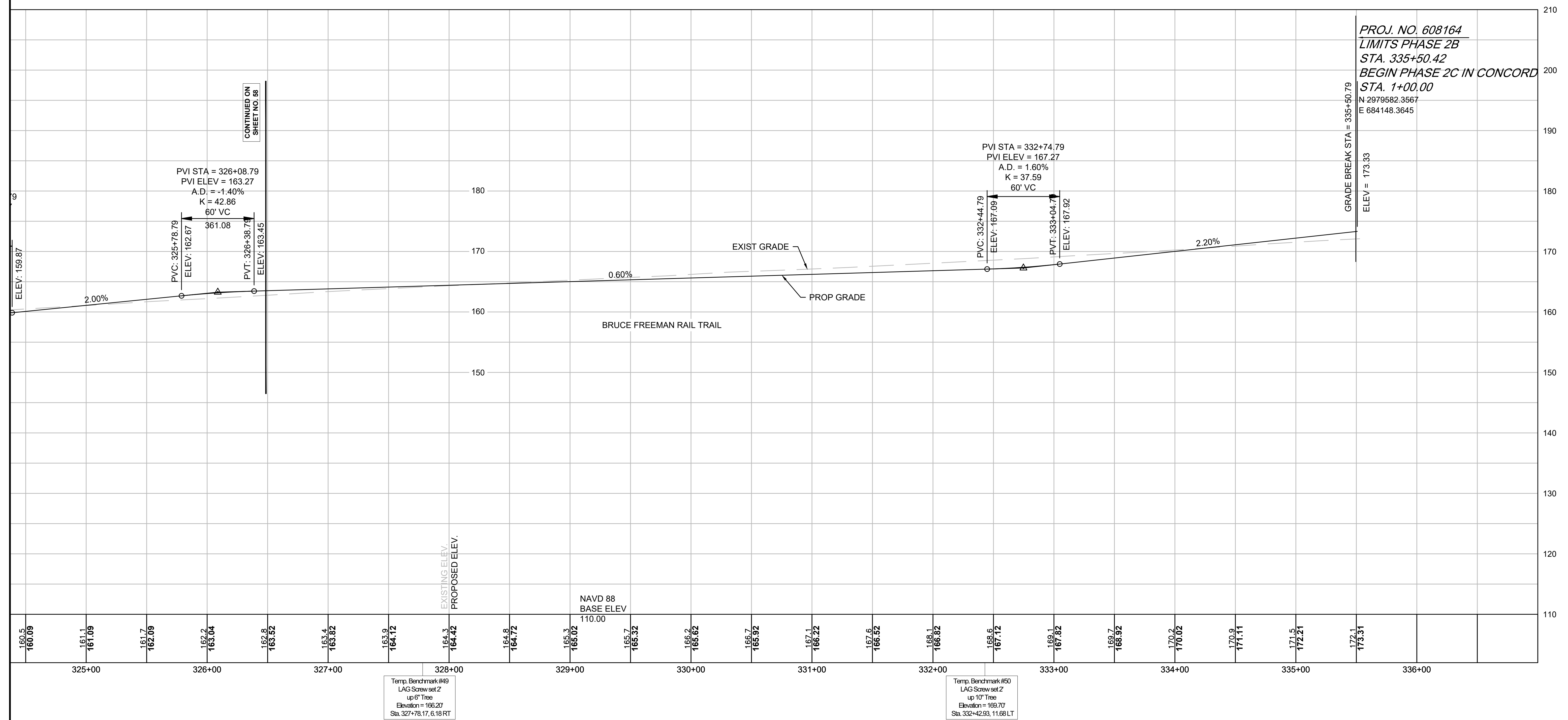
SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	58	318
PROJECT FILE NO.		608164	

**PROFILES**

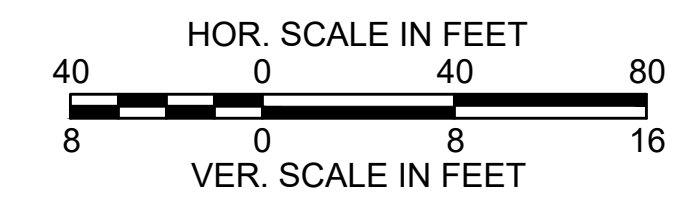


SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	59	318
PROJECT FILE NO.		608164	

**PROFILES**



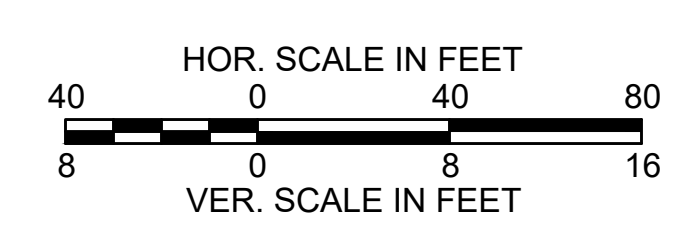
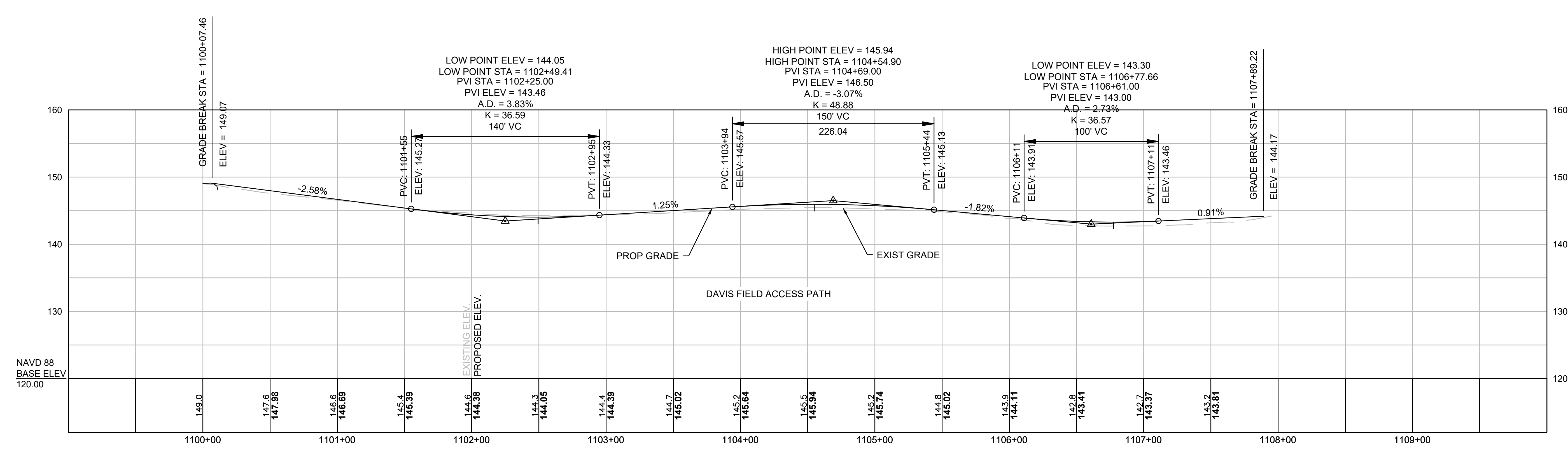
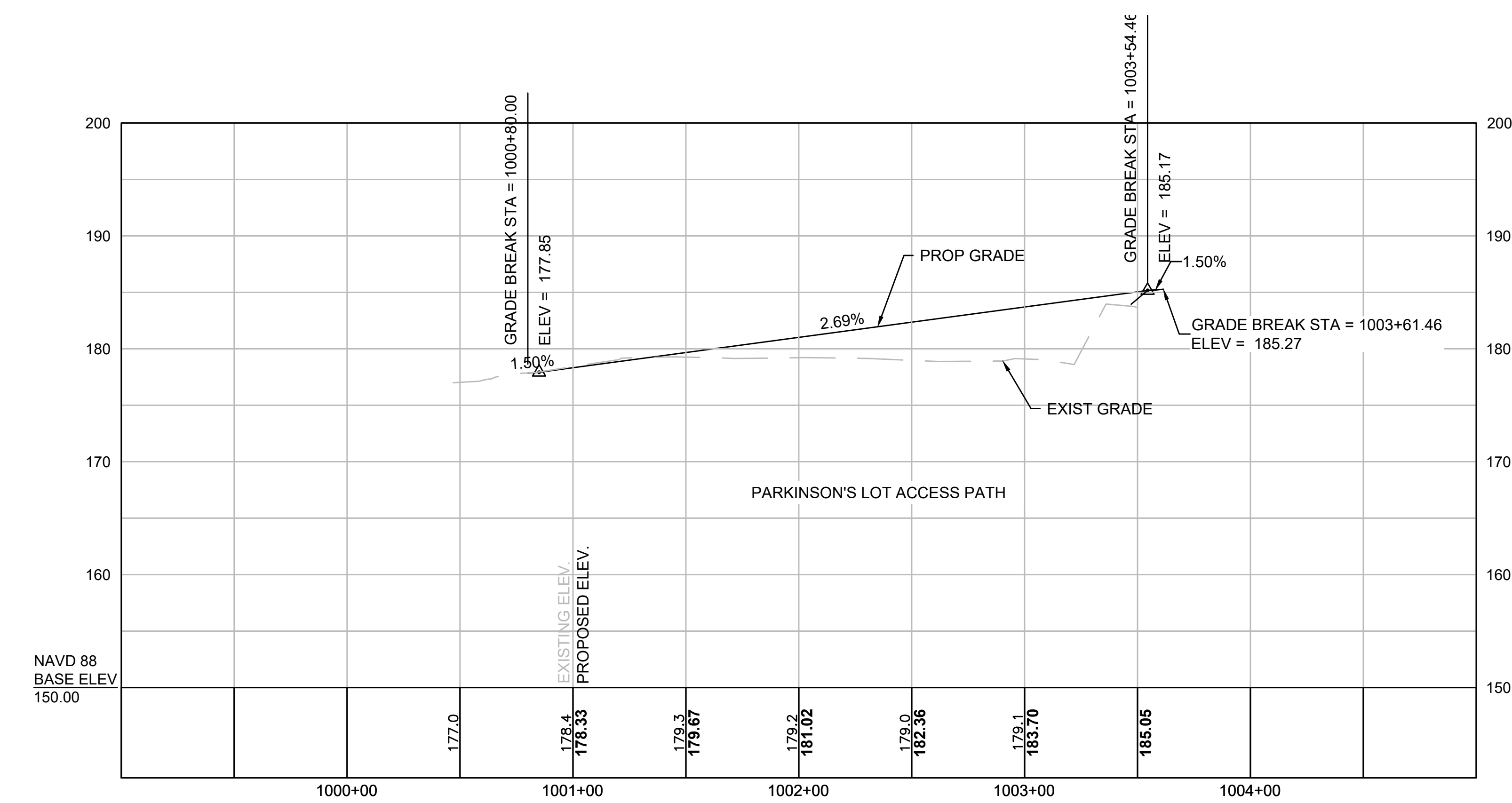
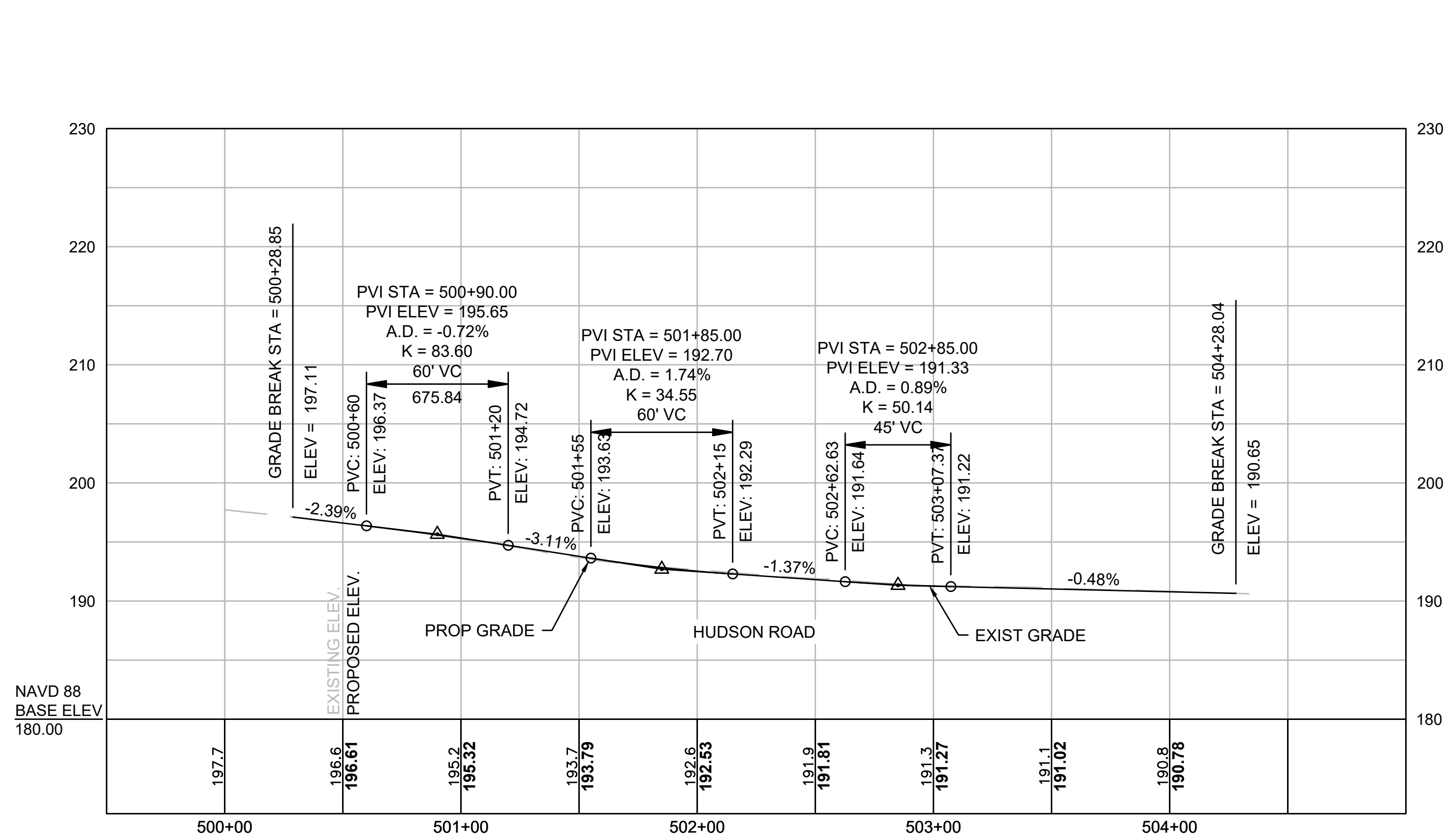
PROJ. NO. 608164  
 LIMITS PHASE 2B  
 STA. 335+50.42  
 BEGIN PHASE 2C IN CONCORD  
 STA. 1+00.00  
 N 2979582.3567  
 E 684148.3645



FOR CONSTRUCTION PLANS SEE SHEET NO. 39 & 40

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	60	318
PROJECT FILE NO.		608164	

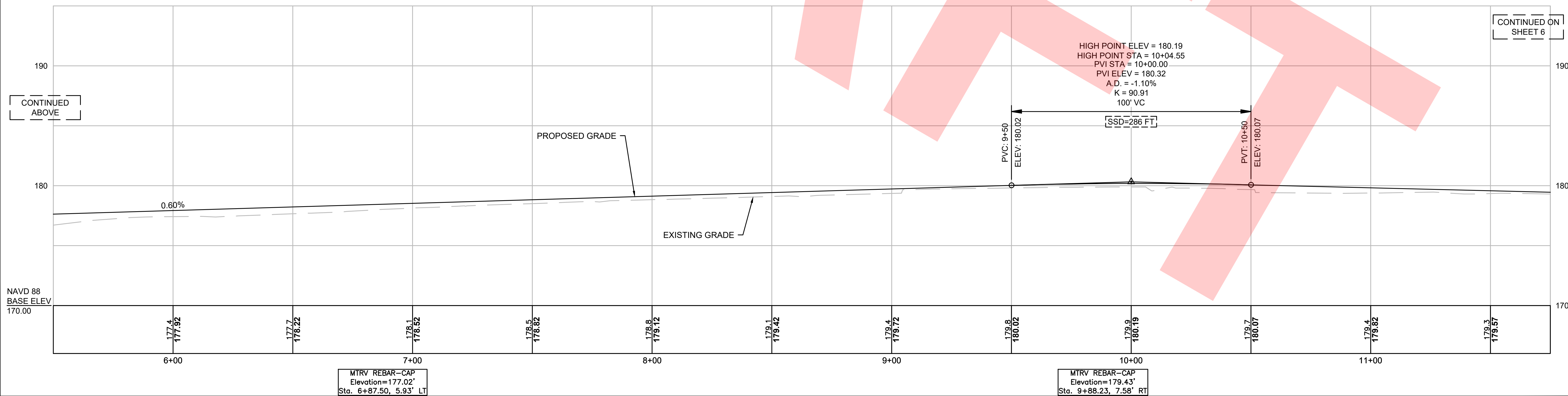
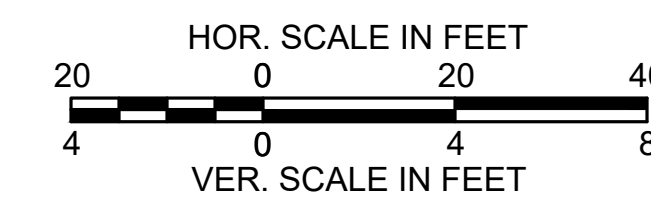
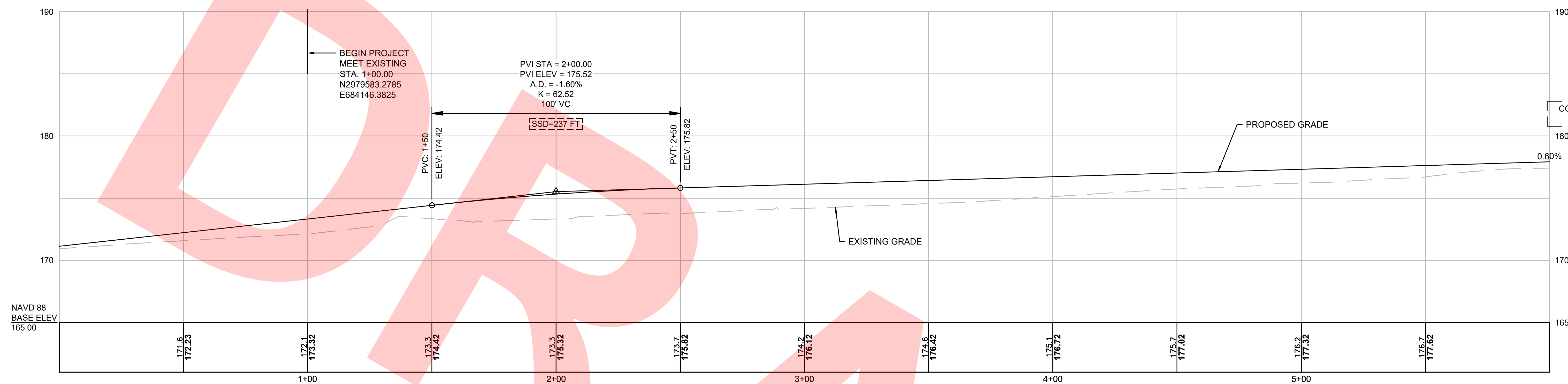
PROFILES



CONCORD  
BRUCE FREEMAN RAIL TRAIL PHASE 2D

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	-	5	32
PROJECT FILE NO.		608164	

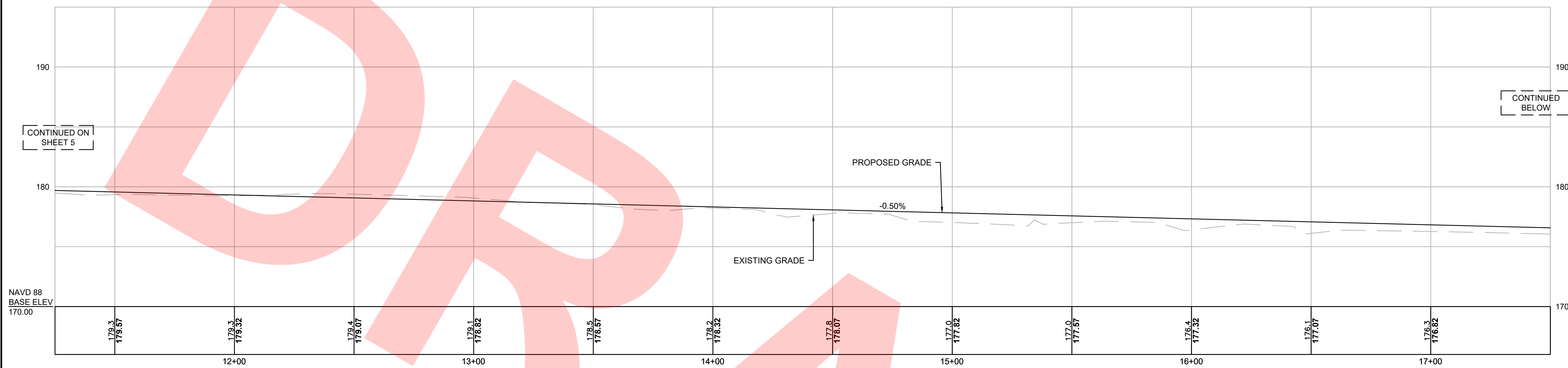
PROFILES



CONCORD  
BRUCE FREEMAN RAIL TRAIL PHASE 2D

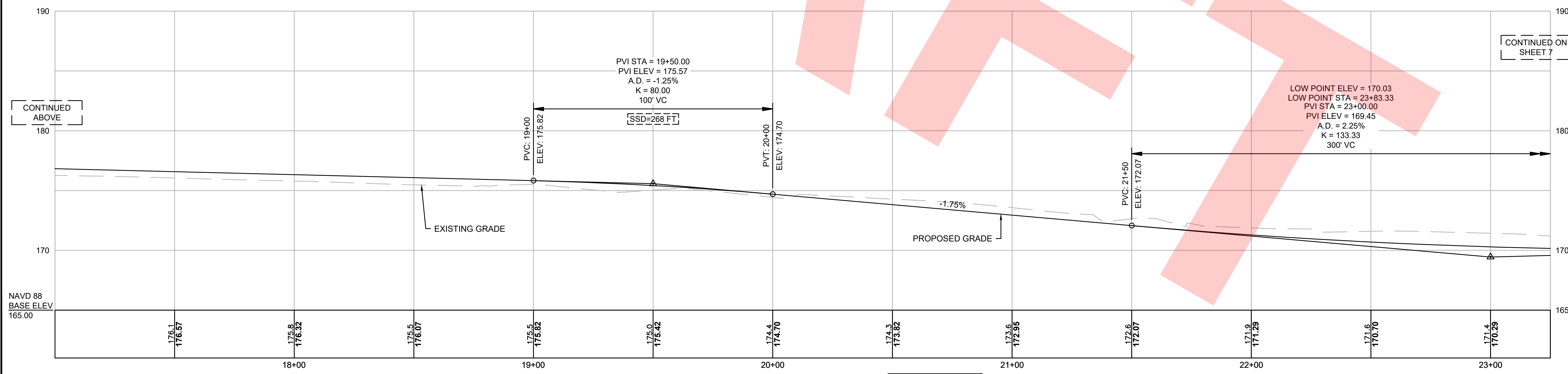
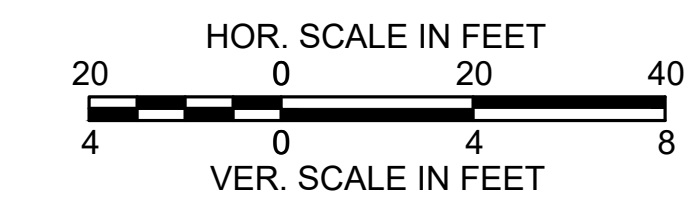
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	-	6	32
PROJECT FILE NO.		608164	

PROFILES



MTRV REBAR-CAP  
Elevation=178.67'  
Sta. 12+97.05, 8.62' RT

MTRV REBAR-CAP  
Elevation=176.22'  
Sta. 16+75.12, 9.49' RT



PVI STA = 19+50.00  
PVI ELEV = 175.57  
A.D. = -1.25%  
K = 80.00  
100' VC

S.S.D.=268 FT

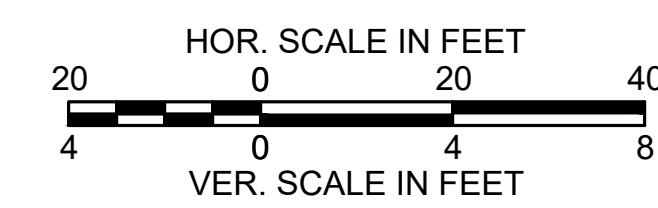
LOW POINT ELEV = 170.03  
LOW POINT STA = 23+83.33  
PVI STA = 23+00.00  
PVI ELEV = 169.45  
A.D. = 2.25%  
K = 133.33  
300' VC

MTRV REBAR-CAP  
Elevation=173.53'  
Sta. 20+67.85, 5.80' RT

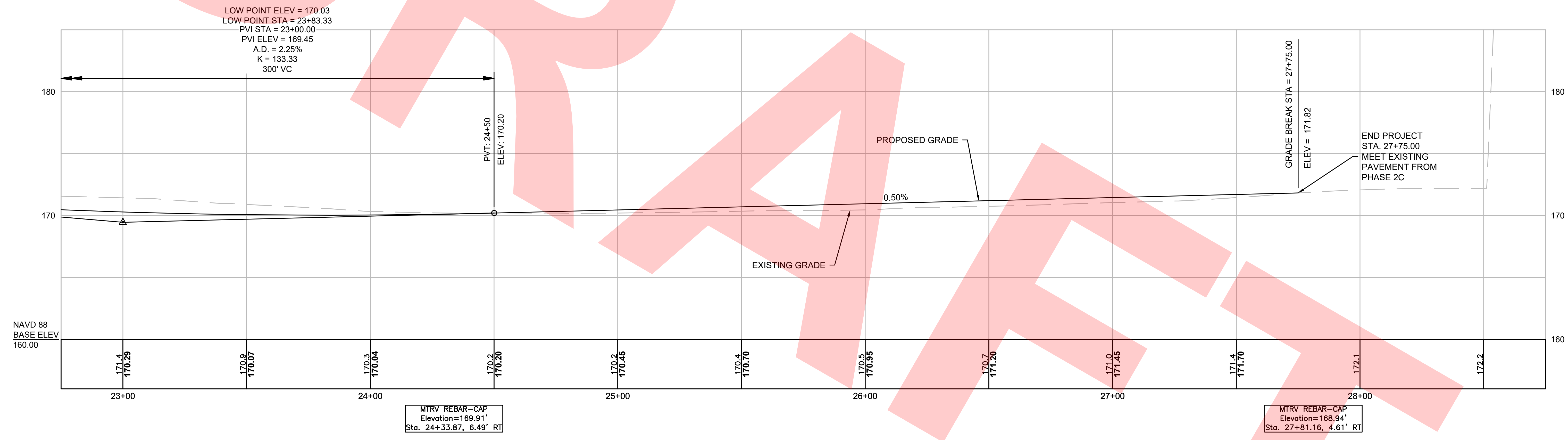
CONCORD  
BRUCE FREEMAN RAIL TRAIL PHASE 2D

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	-	7	32
PROJECT FILE NO.		608164	

PROFILES



CONTINUED ON  
SHEET 6



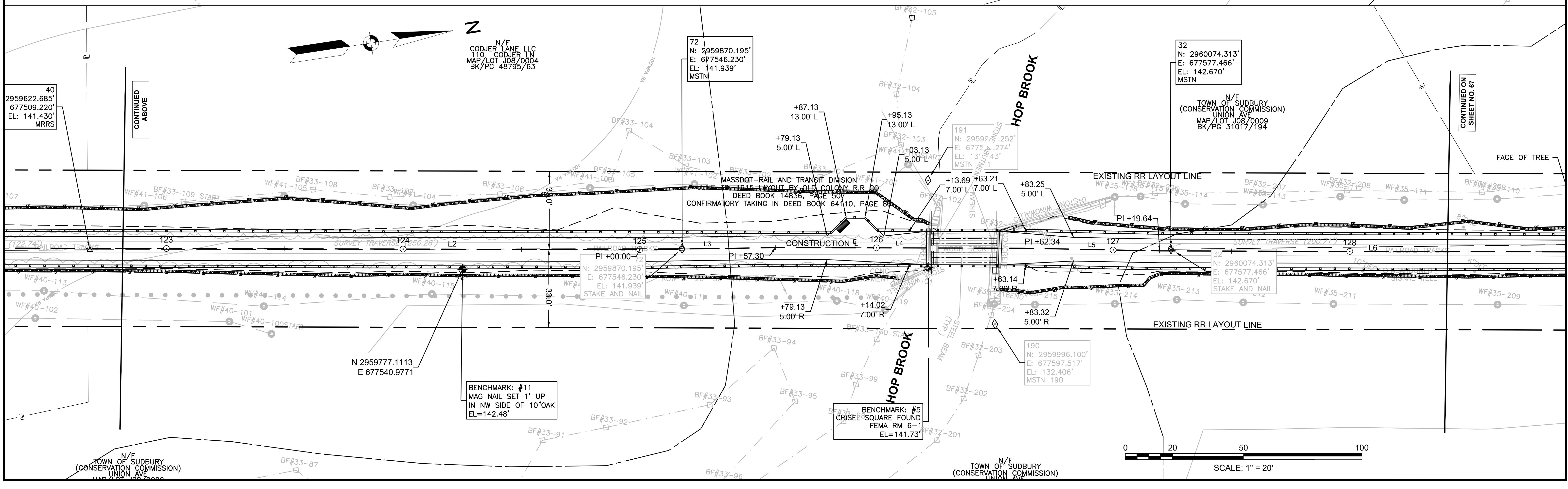
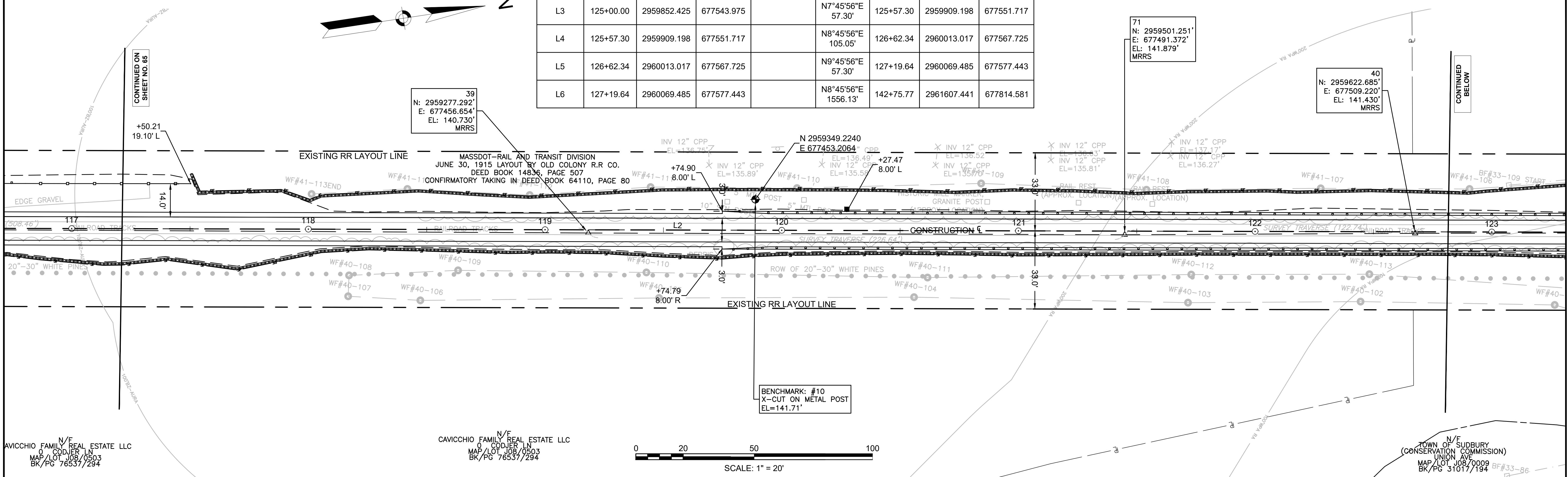






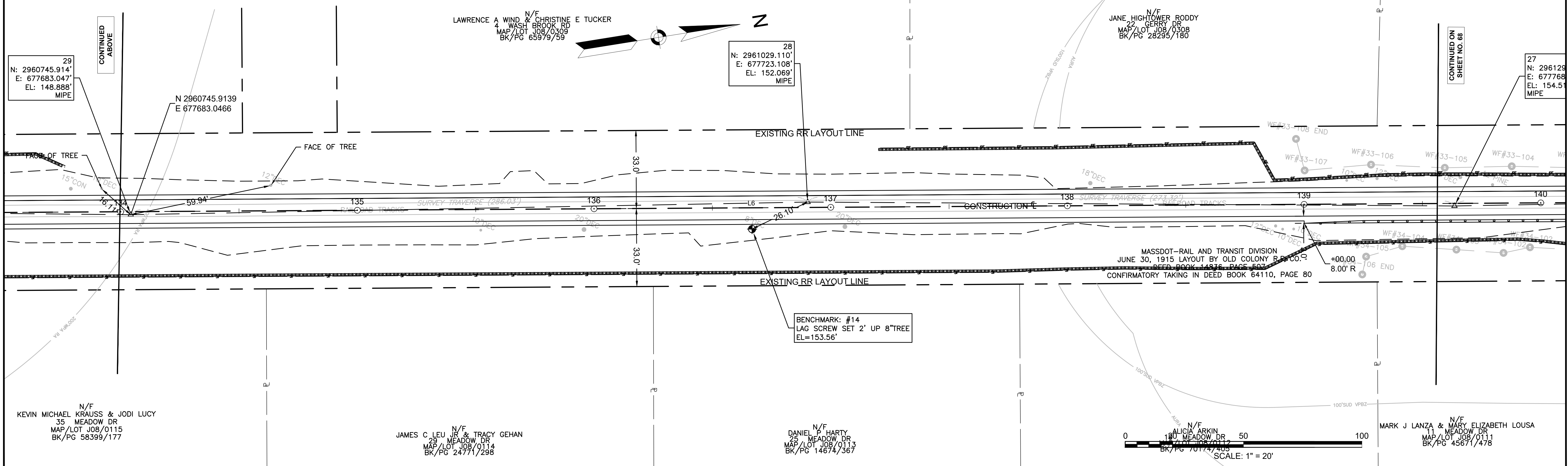
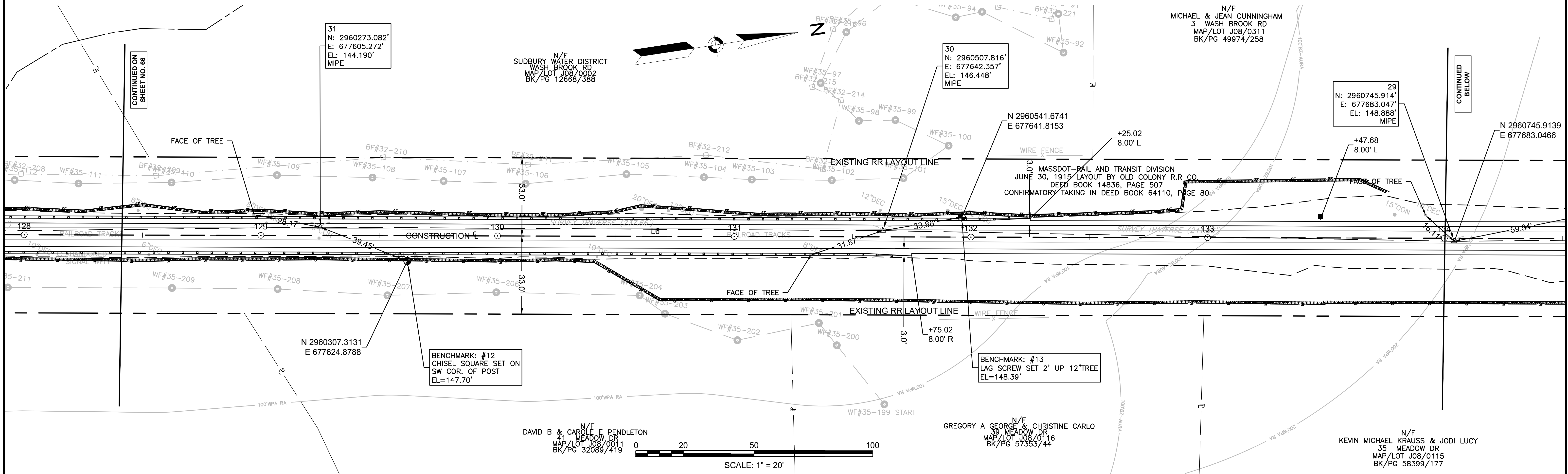
PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L2	108+11.69	2958183.831	677286.694		N8°45'56"E 1688.31'	125+00.00	2959852.425	677543.975
L3	125+00.00	2959852.425	677543.975		N7°45'56"E 57.30'	125+57.30	2959909.198	677551.717
L4	125+57.30	2959909.198	677551.717		N8°45'56"E 105.05'	126+62.34	2960013.017	677567.725
L5	126+62.34	2960013.017	677567.725		N9°45'56"E 57.30'	127+19.64	2960069.485	677577.443
L6	127+19.64	2960069.485	677577.443		N8°45'56"E 1556.13'	142+75.77	2961607.441	677814.581

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	66	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L6	127+19.64	2960069.485	677577.443		N8°45'56"E 1556.13'	142+75.77	2961607.441	677814.581

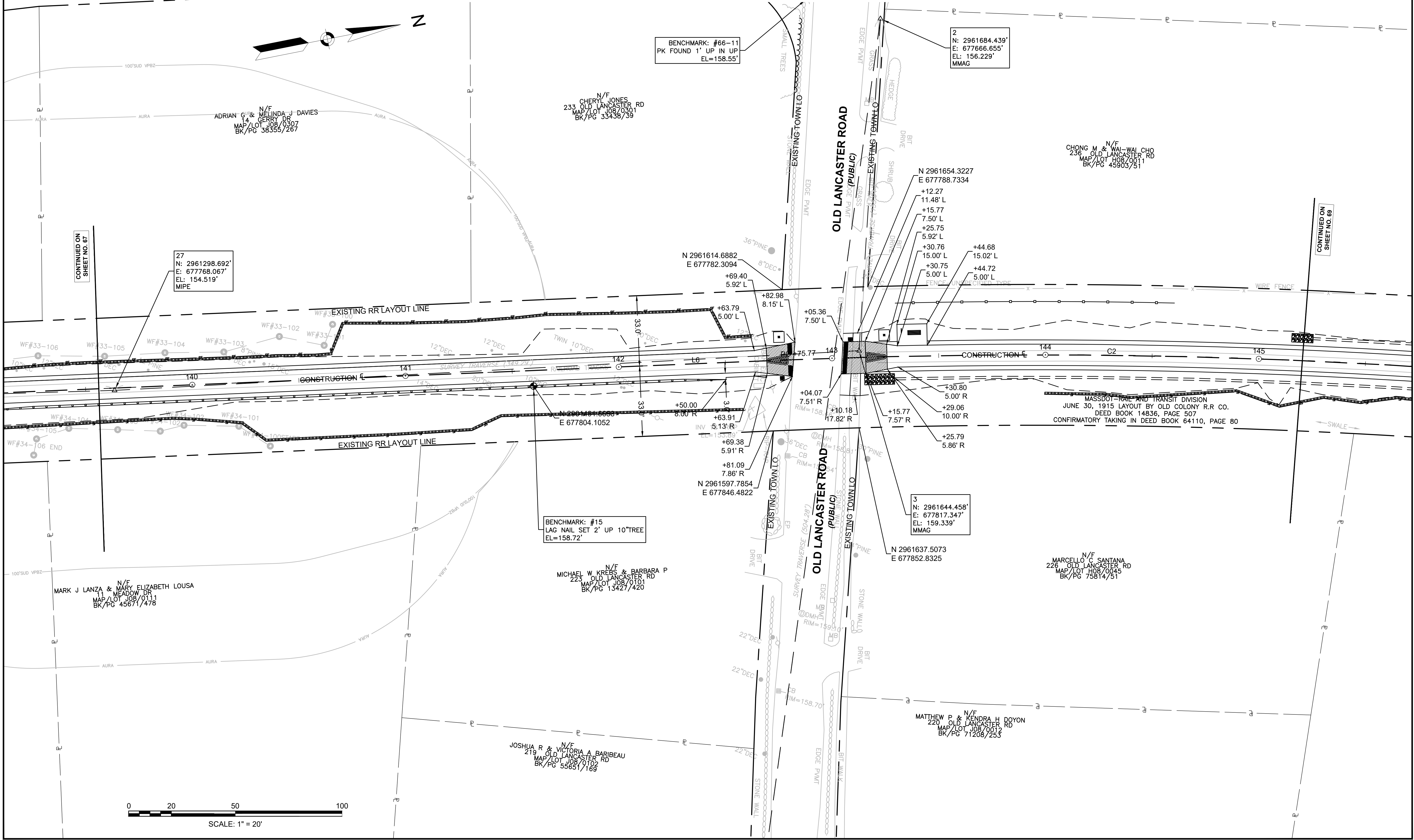
SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	67	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



608164\_HDCURB & BASELINE TIE PLANS.DWG Plotted on 12-May-2021 11:02 AM

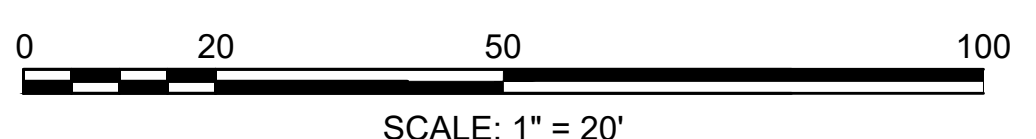
PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L6	127+19.64	2960069.485	677577.443		N8°45'56"E 1556.13'	142+75.77	2961607.441	677814.581
C2	142+75.77	2961607.441	677814.581	R=2865.00 Δ=16°09'23" L=807.88' T=406.64'		150+83.65	2962378.104	678047.899

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	68	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



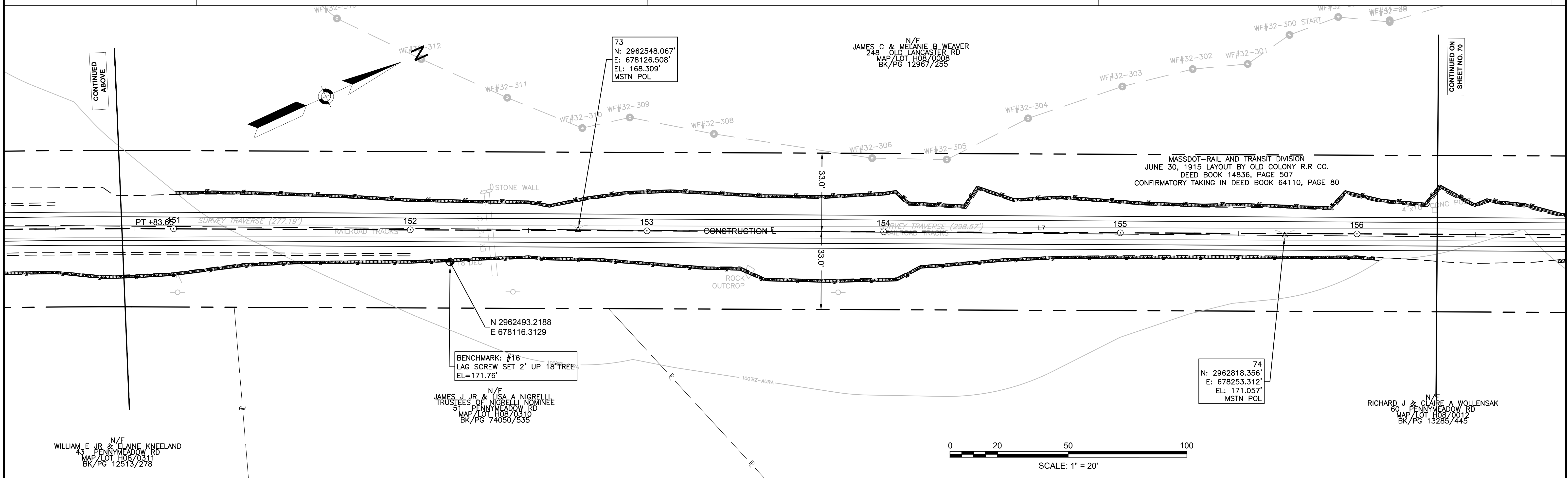
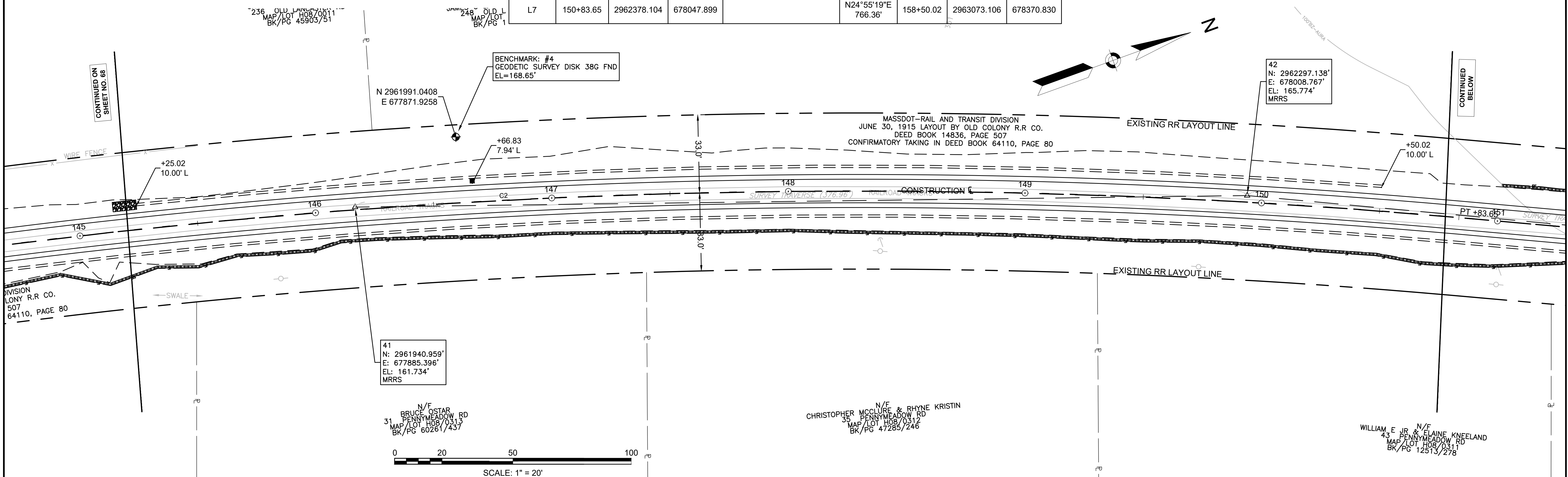
CONTINUED ON  
SHEET NO. 67

CONTINUED ON  
SHEET NO. 69



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C2	142+75.77	2961607.441	677814.581	R=2865.00' Δ=16°09'23" L=807.88' T=406.64'		150+83.65	2962378.104	678047.899
L7	150+83.65	2962378.104	678047.899		N24°55'19"E 766.36'	158+50.02	2963073.106	678370.830

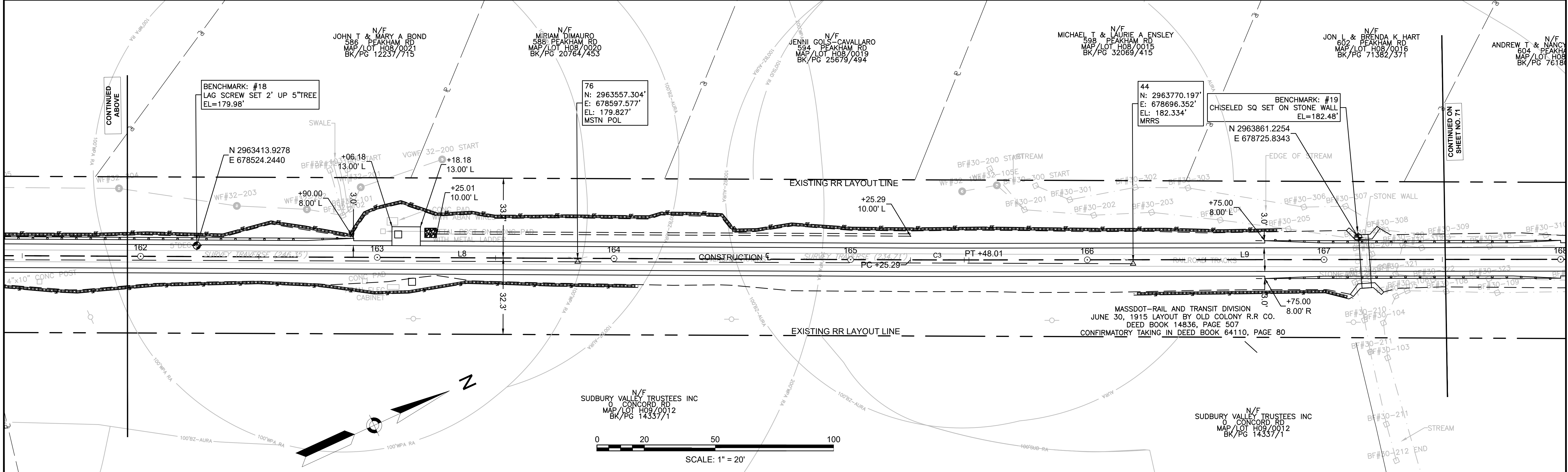
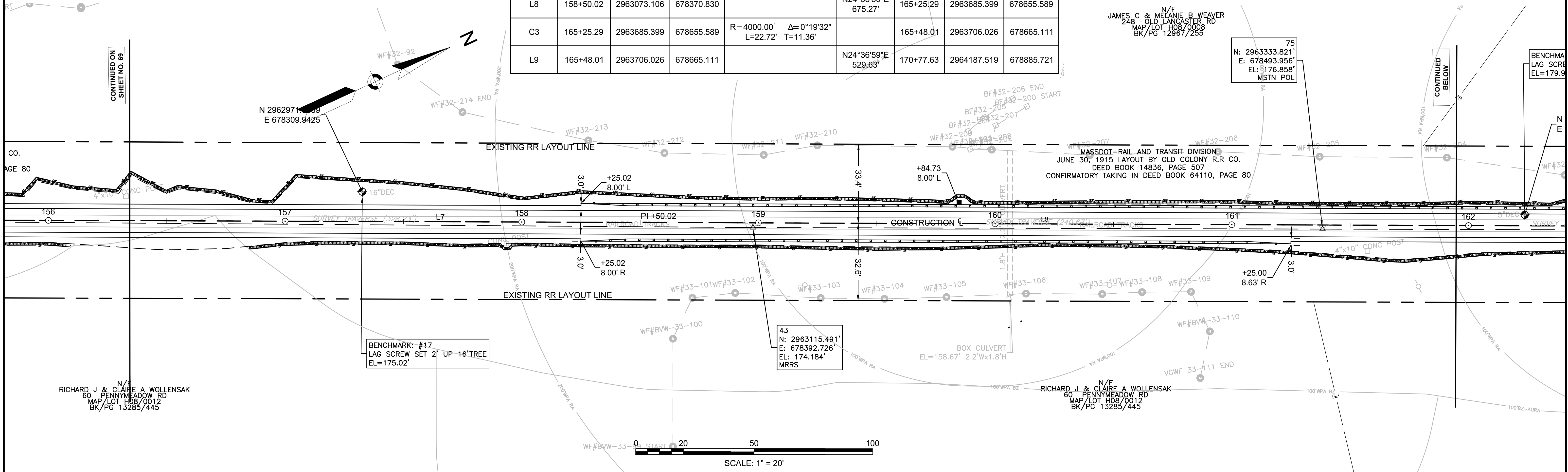
SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	69	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



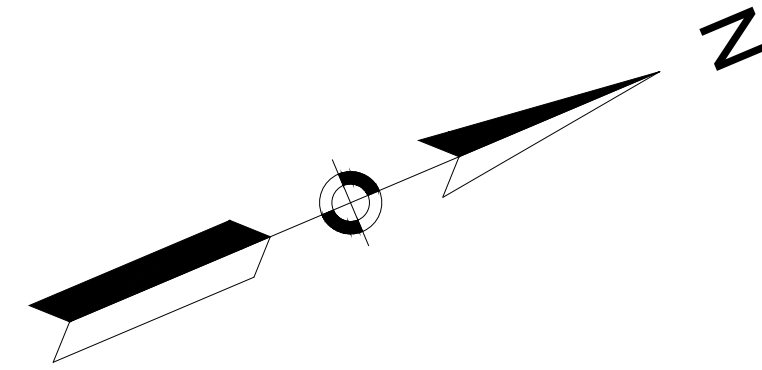
608164\_HDC/CURB & BASELINE TIE PLANS.DWG Plotted on 12-May-2021 11:03 AM

PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L7	150+83.65	2962378.104	678047.899		N24°55'19"E 766.36'	158+50.02	2963073.106	678370.830
L8	158+50.02	2963073.106	678370.830		N24°56'30"E 675.27'	165+25.29	2963685.399	678655.589
C3	165+25.29	2963685.399	678655.589	R=4000.00 L=22.72' T=11.36'		165+48.01	2963706.026	678665.111
L9	165+48.01	2963706.026	678665.111		N24°36'59"E 529.63'	170+77.63	2964187.519	678885.721

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	70	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			

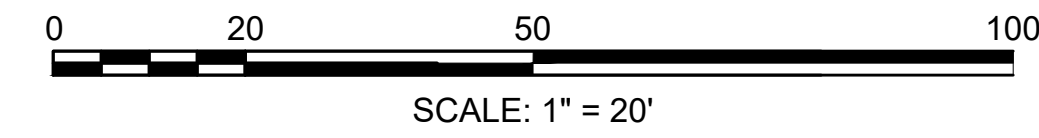
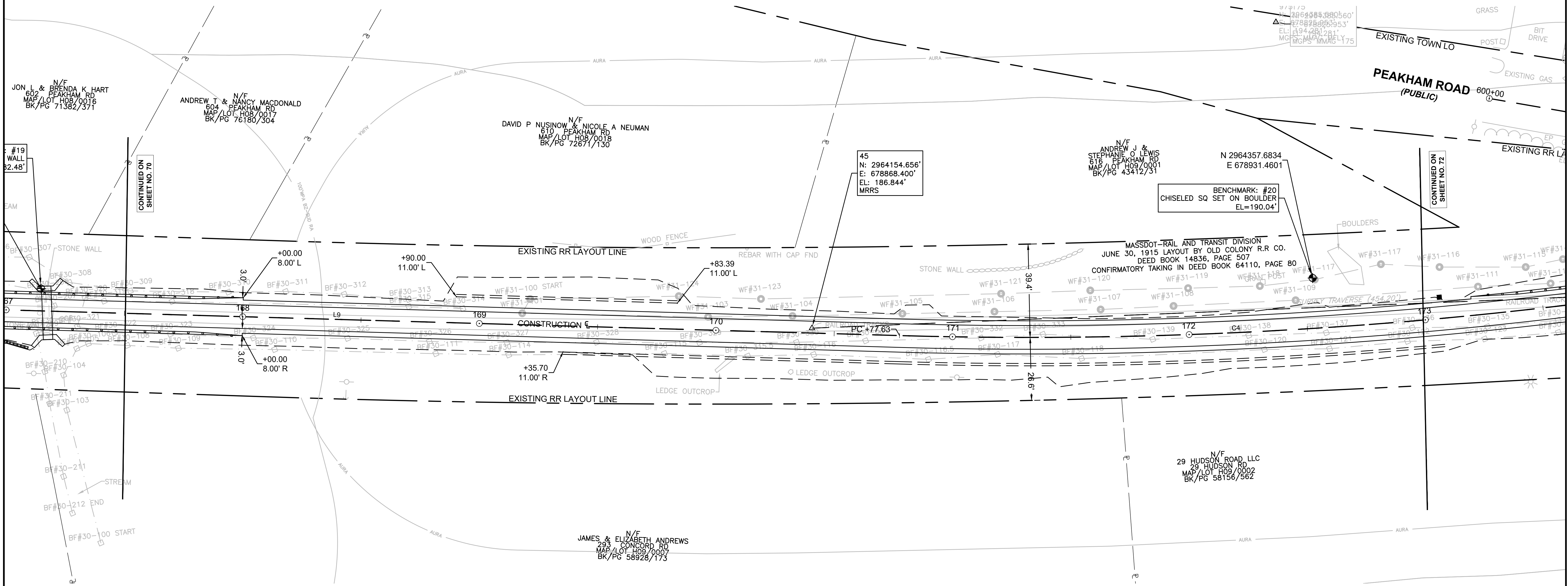


608164\_HDCURB & BASELINE TIE PLANS.DWG Plotted on 12-May-2021 11:04 AM



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L9	165+48.01	2963706.026	678665.111		N24°36'59"E 529.63'	170+77.63	2964187.519	678885.721
C4	170+77.63	2964187.519	678885.721	R=1900.00' Δ=10°57'17" L=363.27' T=182.19'		174+40.91	2964530.189	679004.642

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	71	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



PR-BFRT CONSTRUCTION BASELINE DATA

NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C4	170+77.63	2964187.519	678885.721	R=1900.00' Δ=10°57'17" L=363.27' T=182.19'		174+40.91	2964530.189	679004.642
C5	174+40.91	2964530.189	679004.642	R=250.00' Δ=6°08'42" L=26.81' T=13.42'		174+67.72	2964555.854	679012.359
L10	174+67.72	2964555.854	679012.359		N19°48'24"E 84.23'	175+51.95	2964635.105	679040.901
C6	175+51.95	2964635.105	679040.901	R=500.00' Δ=5°21'48" L=46.81' T=23.42'		175+98.76	2964678.335	679058.797
C7	175+98.76	2964678.335	679058.797	R=1050.00' Δ=5°22'01" L=98.36' T=49.21'		176+97.11	2964769.179	679096.401
L11	176+97.11	2964769.179	679096.401		N19°48'11"E 225.82'	179+22.94	2964981.648	679172.907

**CURVE TABLE**

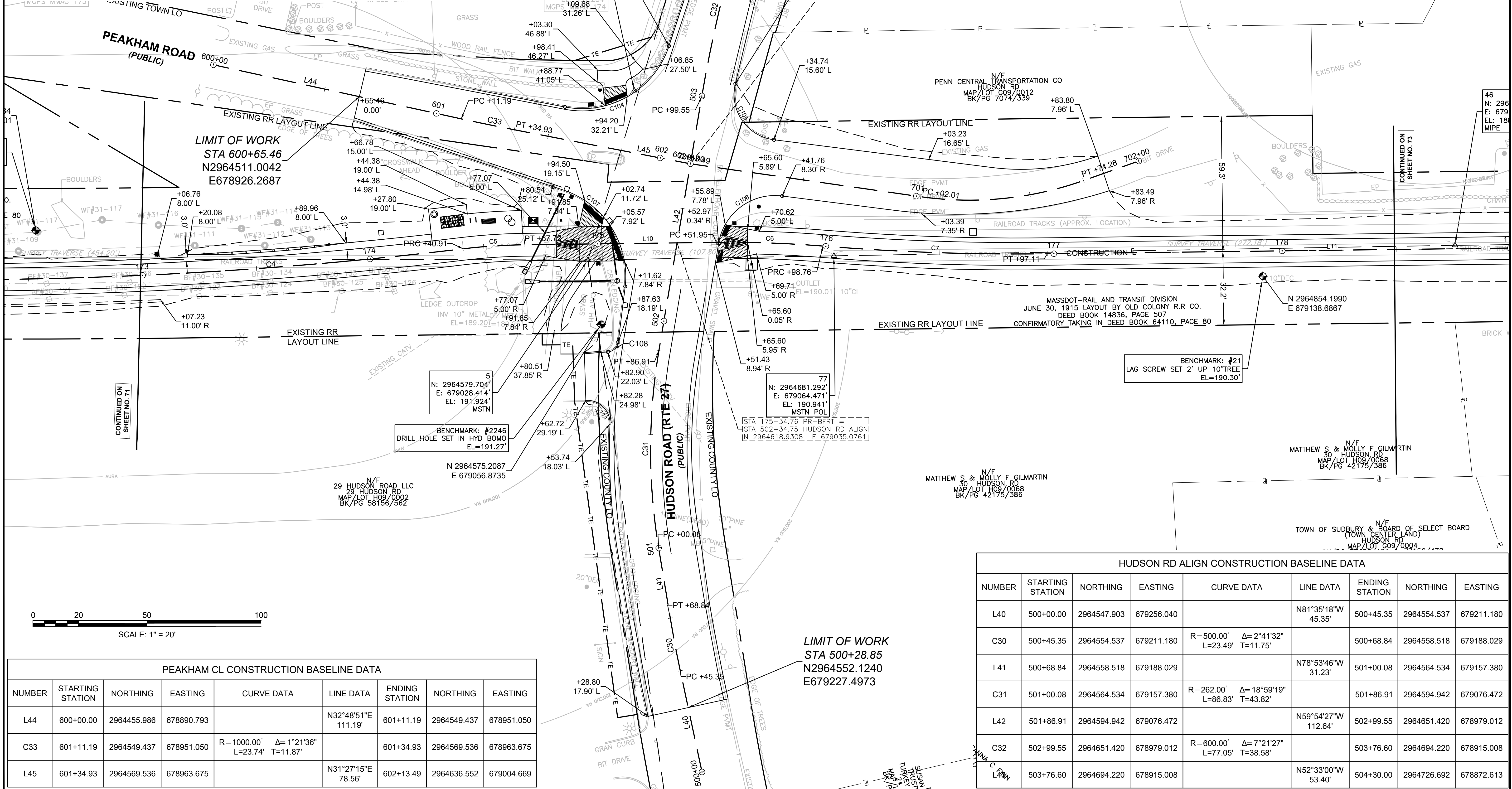
CURVE NO.	LENGTH	RADIUS	DELTA	TANGENT
C104	57.74	40.00	82°42'36.78"	35.21
C105	33.61	20.00	96°18'21.18"	22.32
C106	38.27	25.00	87°42'11.36"	24.02
C107	59.15	40.00	84°43'39.51"	36.48
C108	6.86	5.00	78°36'27.63"	4.09
C111	16.70	10.05	95°15'25.87"	11.01

**SUBURRY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	72	318

PROJECT FILE NO. 608164

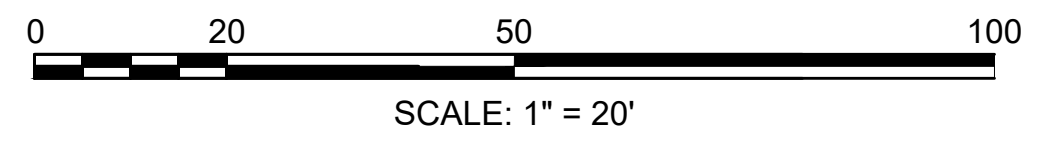
**CURB & BASELINE TIE PLANS**



**LIMIT OF WORK  
STA 600+65.46  
N2964511.0042  
E678926.2687**

**LIMIT OF WORK  
STA 504+26.44  
N2964724.5314  
E678875.4345**

**LIMIT OF WORK  
STA 500+28.85  
N2964552.1240  
E679227.4973**



PEAKHAM CL CONSTRUCTION BASELINE DATA

NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L44	600+00.00	2964455.986	678890.793		N32°48'51"E 111.19'	601+11.19	2964549.437	678951.050
C33	601+11.19	2964549.437	678951.050	R=1000.00' Δ=1°21'36" L=23.74' T=11.87'		601+34.93	2964569.536	678963.675
L45	601+34.93	2964569.536	678963.675		N31°27'15"E 78.56'	602+13.49	2964636.552	679004.669

HUDSON RD ALIGN CONSTRUCTION BASELINE DATA

NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L40	500+00.00	2964547.903	679256.040		N81°35'18"W 45.35'	500+45.35	2964554.537	679211.180
C30	500+45.35	2964554.537	679211.180	R=500.00' Δ=2°41'32" L=23.49' T=11.75'		500+68.84	2964558.518	679188.029
L41	500+68.84	2964558.518	679188.029		N78°53'46"W 31.23'	501+00.08	2964564.534	679157.380
C31	501+00.08	2964564.534	679157.380	R=262.00' Δ=18°59'19" L=86.83' T=43.82'		501+86.91	2964594.942	679076.472
L42	501+86.91	2964594.942	679076.472		N59°54'27"W 112.64'	502+99.55	2964651.420	678979.012
C32	502+99.55	2964651.420	678979.012	R=600.00' Δ=7°21'27" L=77.05' T=38.58'		503+76.60	2964694.220	678915.008
L43	503+76.60	2964694.220	678915.008		N52°33'00"W 53.40'	504+30.00	2964726.692	678872.613



PR-BFRT CONSTRUCTION BASELINE DATA

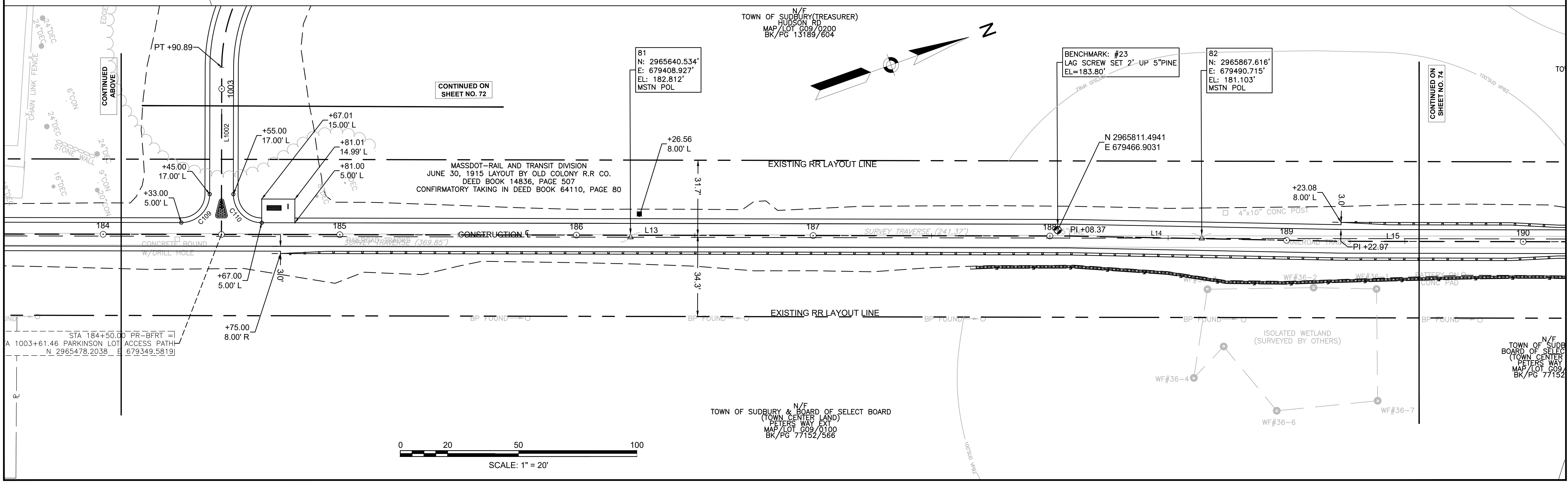
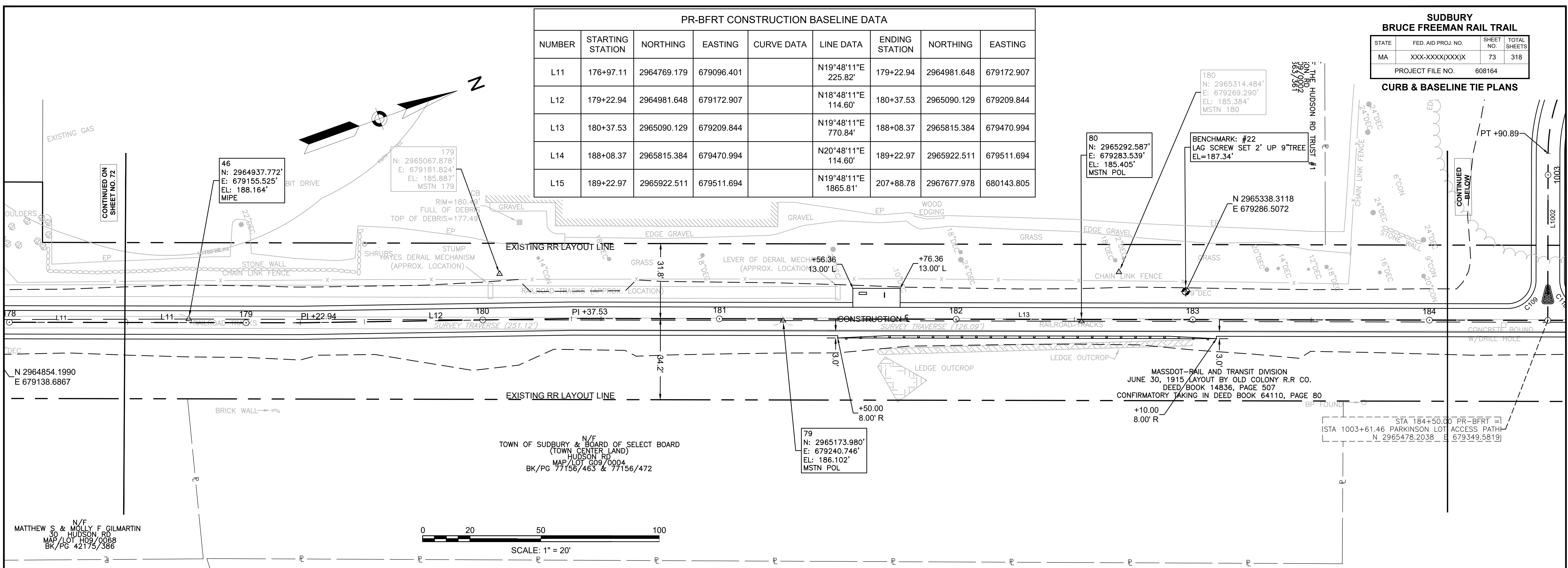
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L11	176+97.11	2964769.179	679096.401		N19°48'11"E 225.82'	179+22.94	2964981.648	679172.907
L12	179+22.94	2964981.648	679172.907		N18°48'11"E 114.60'	180+37.53	2965090.129	679209.844
L13	180+37.53	2965090.129	679209.844		N19°48'11"E 770.84'	188+08.37	2965815.384	679470.994
L14	188+08.37	2965815.384	679470.994		N20°48'11"E 114.60'	189+22.97	2965922.511	679511.694
L15	189+22.97	2965922.511	679511.694		N19°48'11"E 1865.81'	207+88.78	2967677.978	680143.805

SUDBURY  
BRUCE FREEMAN RAIL TRAIL

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	73	318

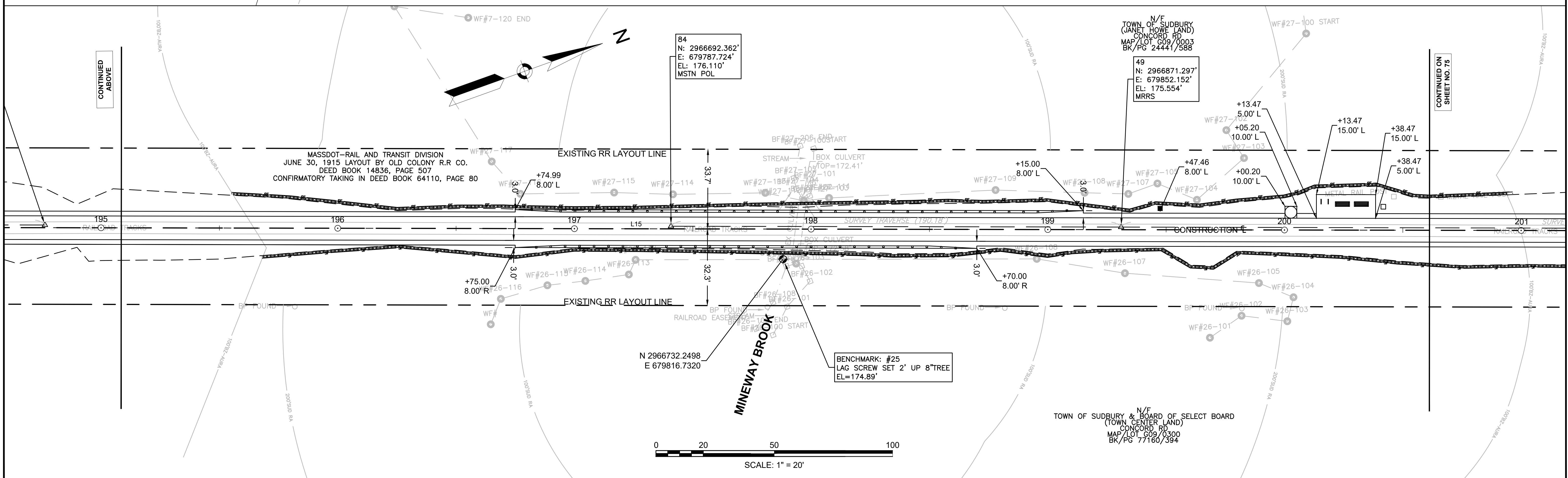
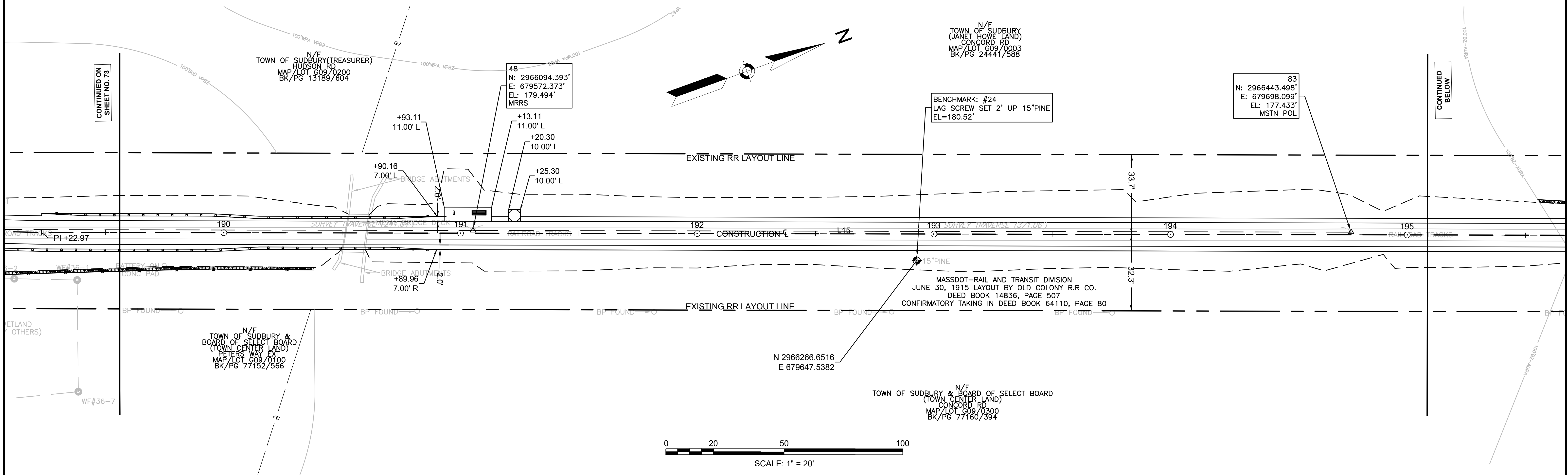
PROJECT FILE NO. 608164

CURB & BASELINE TIE PLANS



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L15	189+22.97	2965922.511	679511.694		N19°48'11"E 1865.81'	207+88.78	2967677.978	680143.805

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	74	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	75	318
PROJECT FILE NO.		608164	

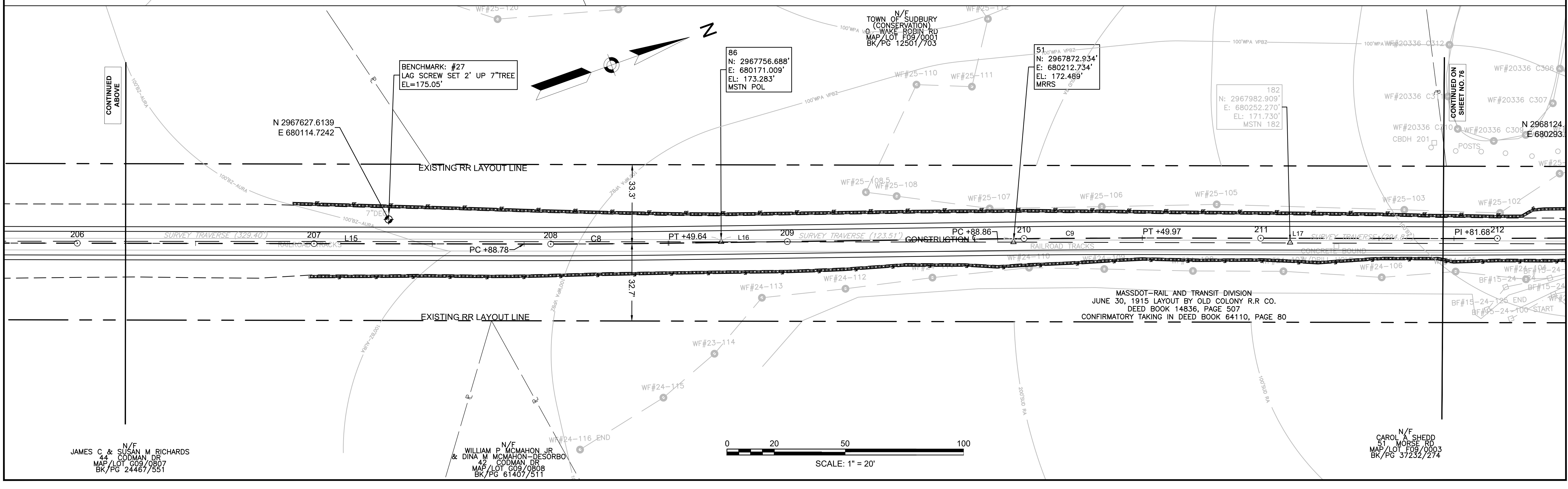
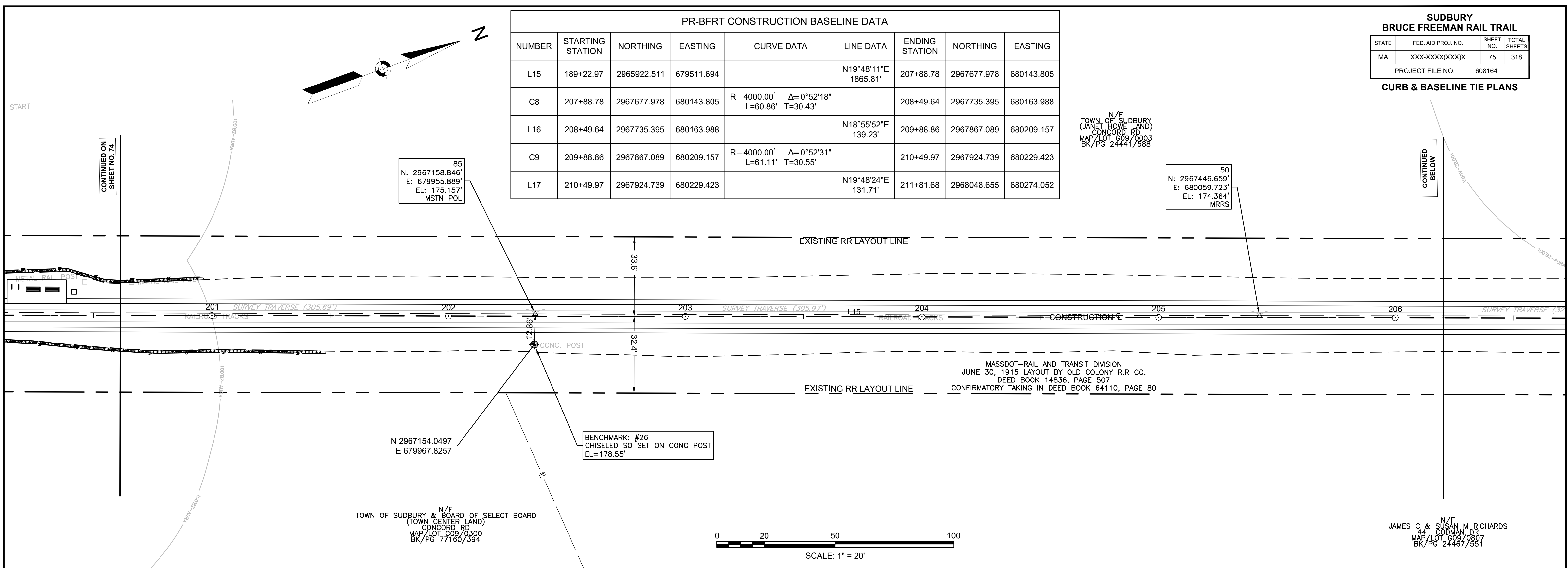
**CURB & BASELINE TIE PLANS**

**PR-BFRT CONSTRUCTION BASELINE DATA**

NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L15	189+22.97	2965922.511	679511.694		N19°48'11"E 1865.81'	207+88.78	2967677.978	680143.805
C8	207+88.78	2967677.978	680143.805	R=4000.00' Δ=0°52'18" L=60.86' T=30.43'		208+49.64	2967735.395	680163.988
L16	208+49.64	2967735.395	680163.988		N18°55'52"E 139.23'	209+88.86	2967867.089	680209.157
C9	209+88.86	2967867.089	680209.157	R=4000.00' Δ=0°52'31" L=61.11' T=30.55'		210+49.97	2967924.739	680229.423
L17	210+49.97	2967924.739	680229.423		N19°48'24"E 131.71'	211+81.68	2968048.655	680274.052

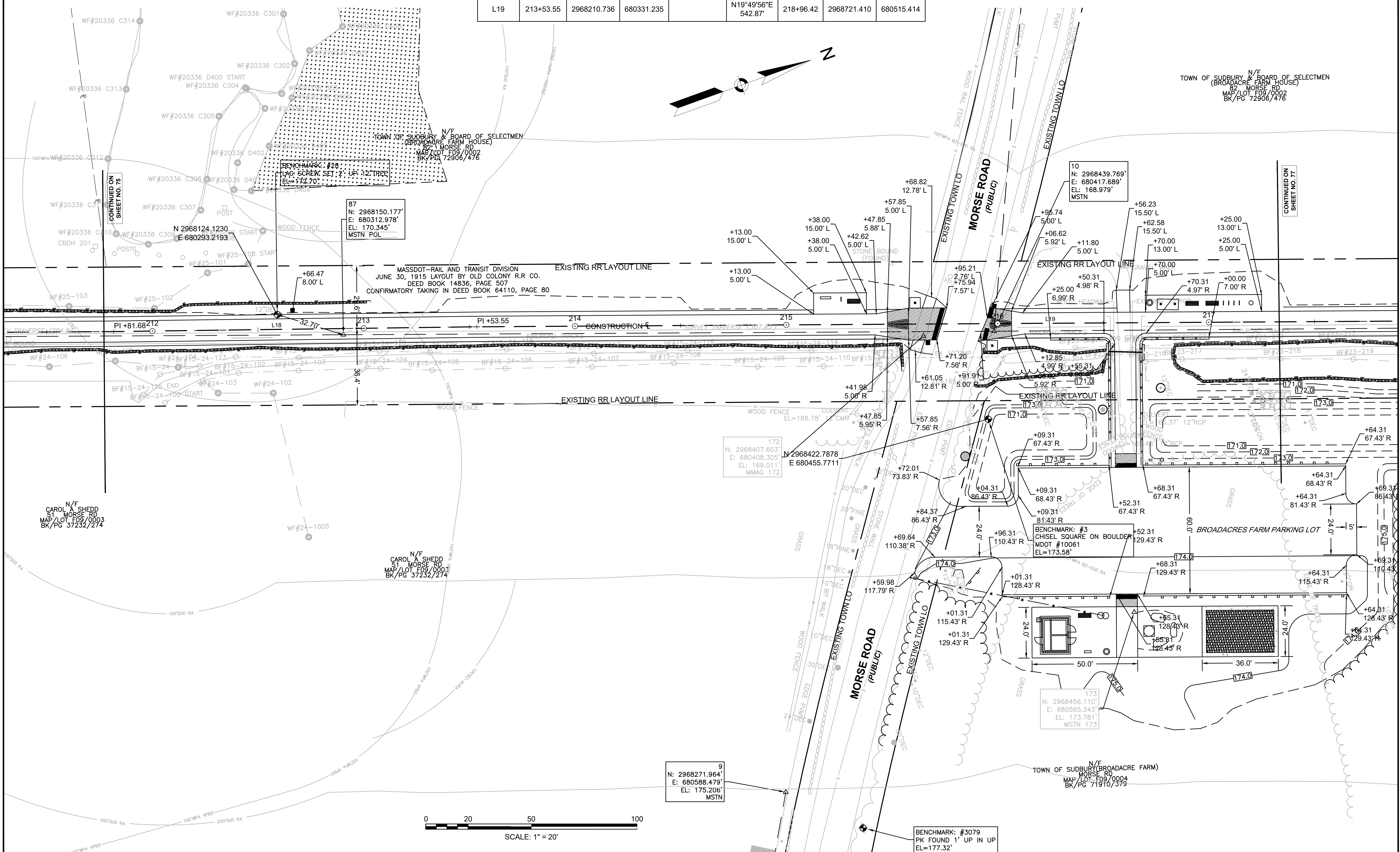
N/E  
TOWN OF SUDBURY  
(JANET HOWE LAND)  
CONCORD RD  
MAP/LOT G09/0003  
BK/PG 24441/588

50  
N: 2967446.659'  
E: 680059.723'  
EL: 174.364'  
MRRS



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L18	211+81.68	2968048.655	680274.052		N19°25'59"E 171.87'	213+53.55	2968210.736	680331.235
L19	213+53.55	2968210.736	680331.235		N19°49'56"E 542.87'	218+96.42	2968721.410	680515.414

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	76	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



9  
N: 2968271.964'  
E: 680588.479'  
EL: 175.206'  
MSTN

BENCHMARK: #3079  
PK FOUND 1' UP IN UP  
EL=177.32'

173  
N: 2968456.110'  
E: 680565.343'  
EL: 173.781'  
MSTN 173

172  
N: 2968407.603'  
E: 680408.305'  
EL: 169.011'  
MMAG 172

87  
N: 2968150.177'  
E: 680312.978'  
EL: 170.345'  
MSTN POL

10  
N: 2968439.769'  
E: 680417.689'  
EL: 168.979'  
MSTN

N/F  
CAROL A SHEDD  
51 MORSE RD  
MAP/LOT F09/0003  
BK/PG 37232/274

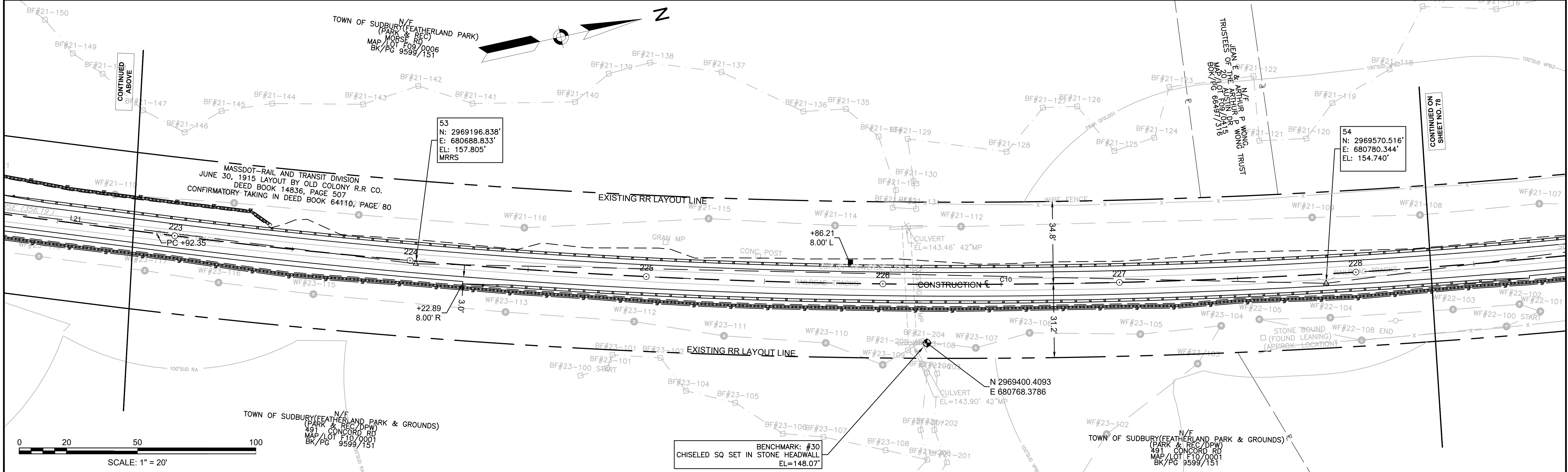
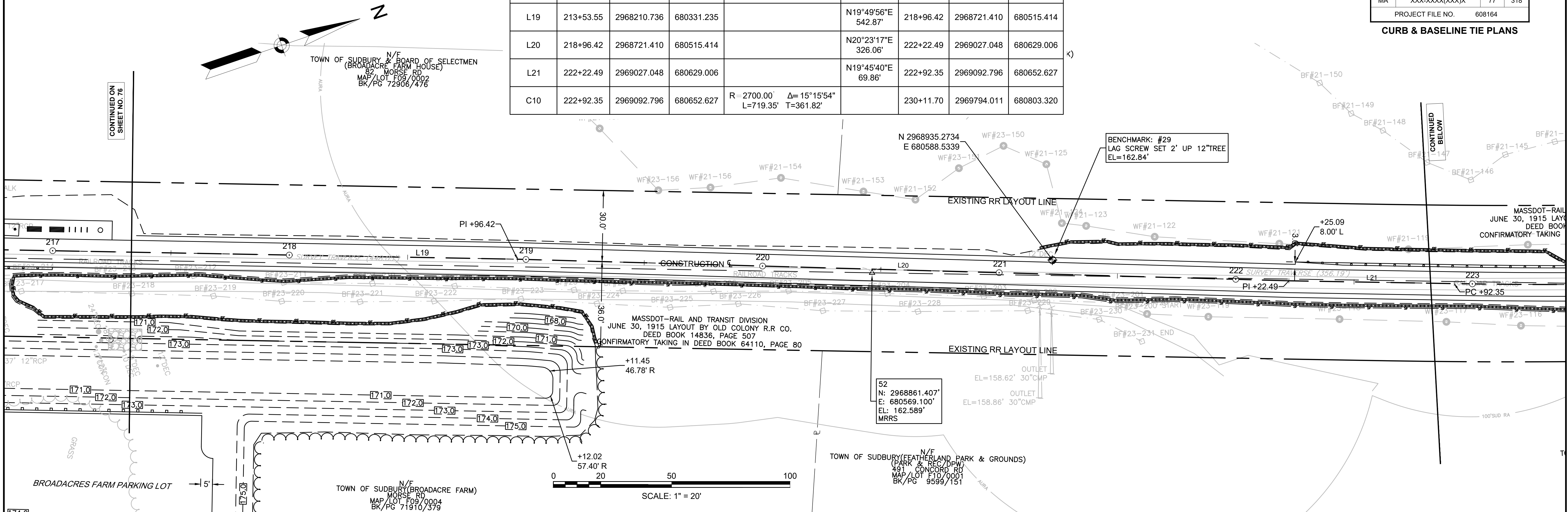
N/F  
CAROL A SHEDD  
51 MORSE RD  
MAP/LOT F09/0003  
BK/PG 37232/274

TOWN OF SUDBURY & BOARD OF SELECTMEN  
(BROADACRES FARM HOUSE)  
82 MORSE RD  
MAP/LOT F09/0002  
BK/PG 72906/476

N/F  
TOWN OF SUDBURY (BROADACRES FARM)  
MORSE RD  
MAP/LOT F09/0004  
BK/PG 71910/373

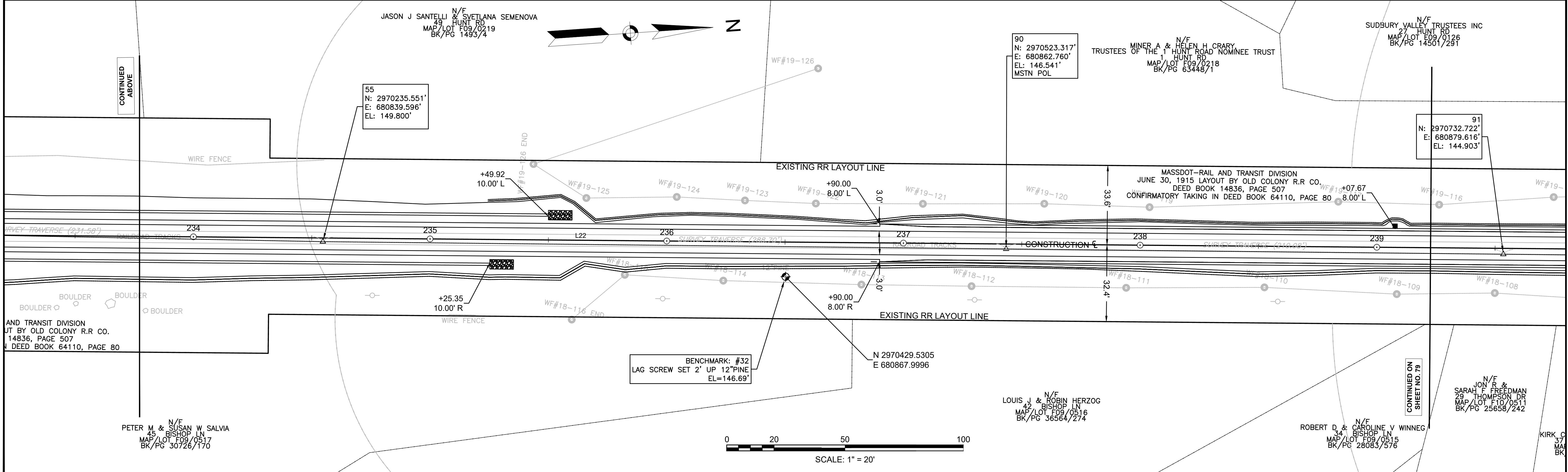
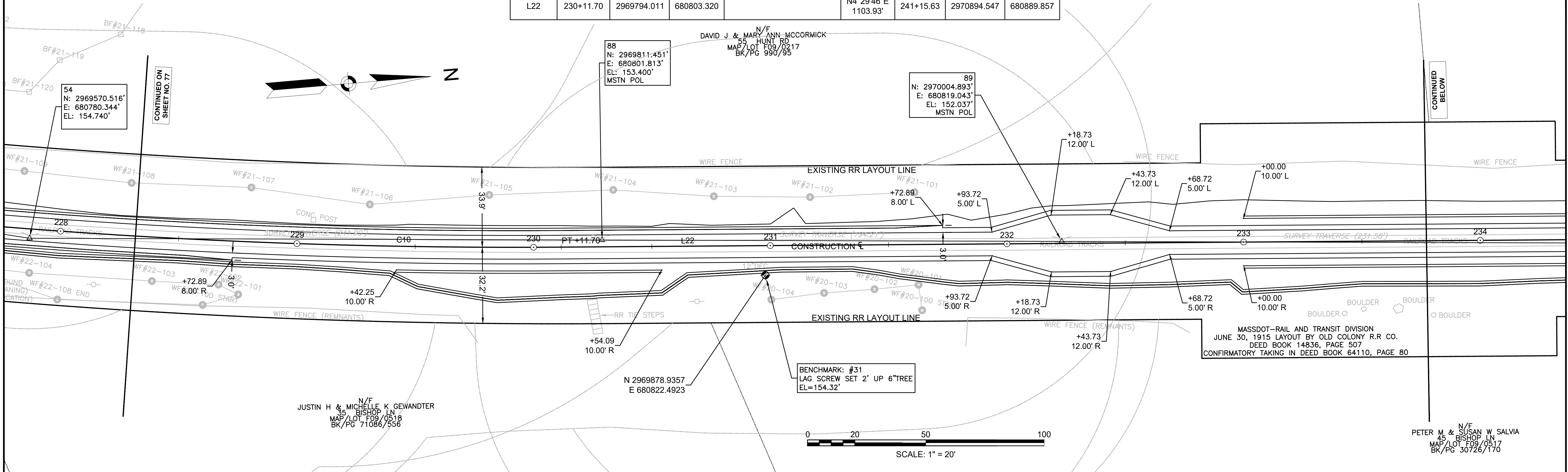
PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L19	213+53.55	2968210.736	680331.235		N19°49'56"E 542.87'	218+96.42	2968721.410	680515.414
L20	218+96.42	2968721.410	680515.414		N20°23'17"E 326.06'	222+22.49	2969027.048	680629.006
L21	222+22.49	2969027.048	680629.006		N19°45'40"E 69.86'	222+92.35	2969092.796	680652.627
C10	222+92.35	2969092.796	680652.627	R=2700.00' Δ=15°15'54" L=719.35' T=361.82'		230+11.70	2969794.011	680803.320

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	77	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C10	222+92.35	2969092.796	680652.627	R=2700.00' Δ=15°15'54" L=719.35' T=361.82'		230+11.70	2969794.011	680803.320
L22	230+11.70	2969794.011	680803.320		N4°29'46"E 1103.93'	241+15.63	2970894.547	680889.857

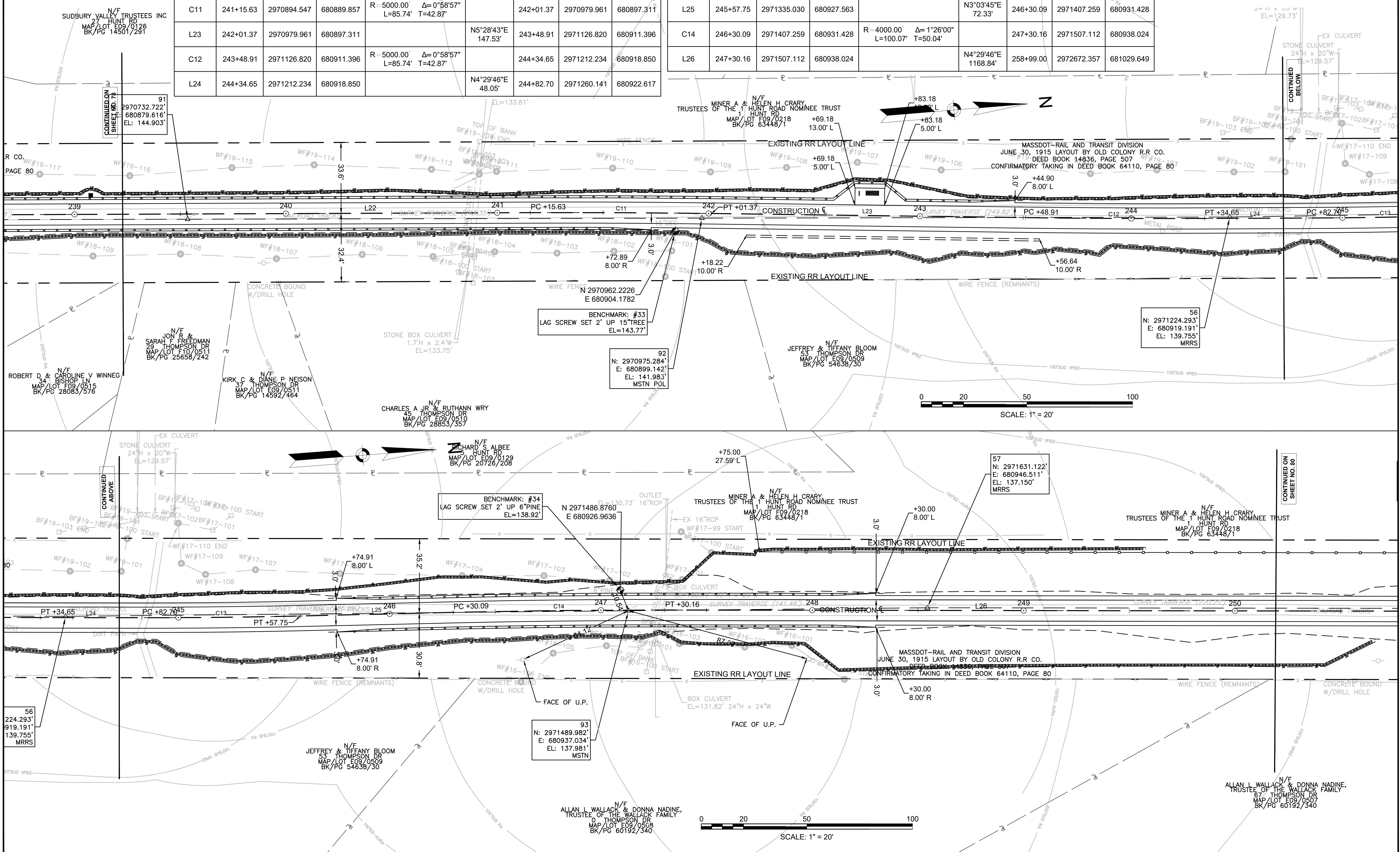
SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	78	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L22	230+11.70	2969794.011	680803.320			241+15.63	2970894.547	680889.857
C11	241+15.63	2970894.547	680889.857	R=5000.00' Δ=0°58'57" L=85.74' T=42.87'		242+01.37	2970979.961	680897.311
L23	242+01.37	2970979.961	680897.311			243+48.91	2971126.820	680911.396
C12	243+48.91	2971126.820	680911.396	R=5000.00' Δ=0°58'57" L=85.74' T=42.87'		244+34.65	2971212.234	680918.850
L24	244+34.65	2971212.234	680918.850			244+82.70	2971260.141	680922.617

PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C13	244+82.70	2971260.141	680922.617	R=3000.00' Δ=1°26'00" L=75.05' T=37.53'		245+57.75	2971335.030	680927.563
L25	245+57.75	2971335.030	680927.563			246+30.09	2971407.259	680931.428
C14	246+30.09	2971407.259	680931.428	R=4000.00' Δ=1°26'00" L=100.07' T=50.04'		247+30.16	2971507.112	680938.024
L26	247+30.16	2971507.112	680938.024			258+99.00	2972672.357	681029.649

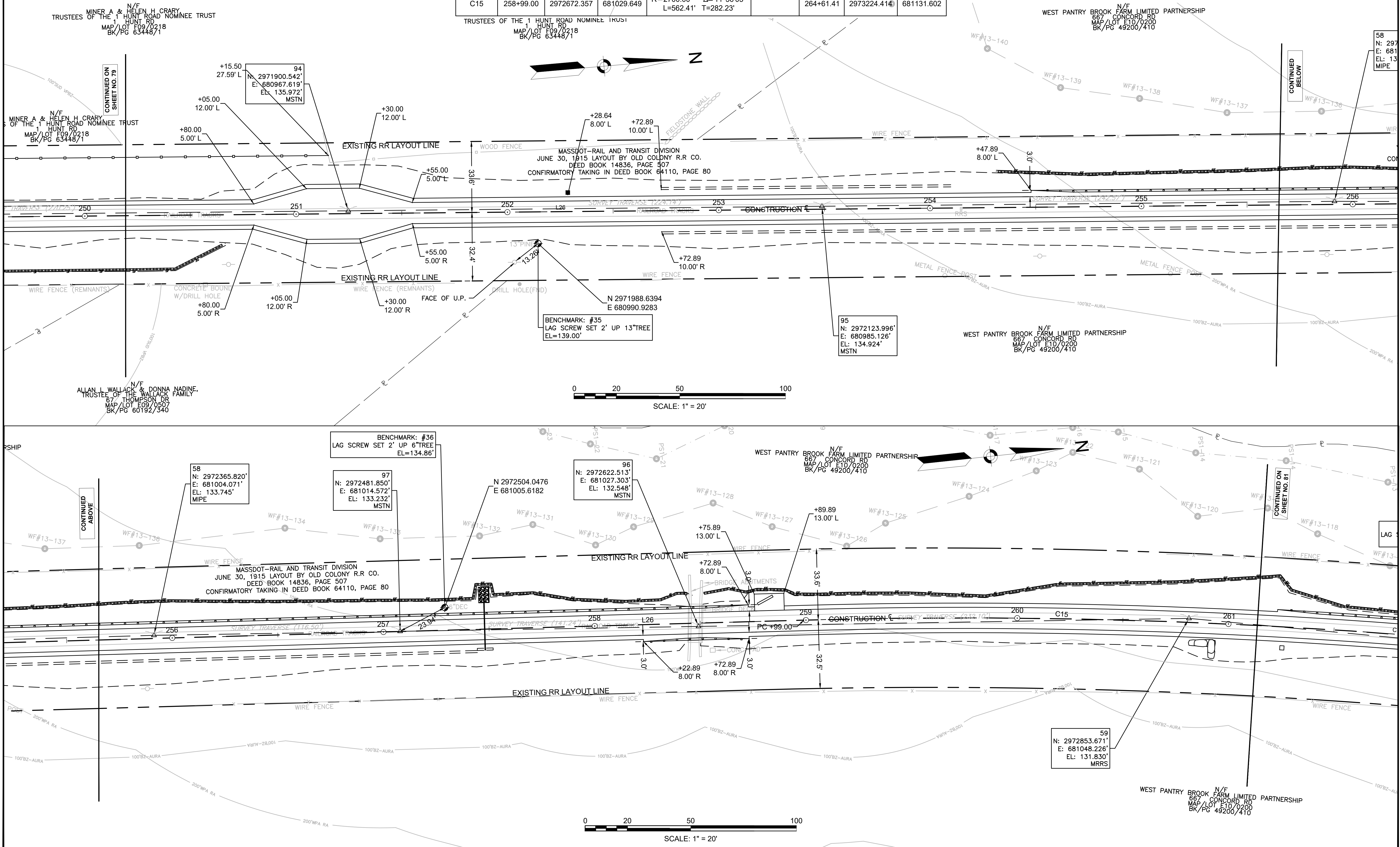
SUBURBY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	79	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



608164\_HDC(CURB & BASELINE TIE PLANS).DWG Plotted on 12-May-2021 11:10 AM

PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L26	247+30.16	2971507.112	680938.024		N4°29'46"E 1168.84'	258+99.00	2972672.357	681029.649
C15	258+99.00	2972672.357	681029.649	R=2700.00' L=562.41' Δ=11°56'05" T=282.23'		264+61.41	2973224.414	681131.602

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	80	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



N/F  
MINER A & HELEN H. CRARY  
TRUSTEES OF THE 1 HUNT ROAD NOMINEE TRUST  
MAP/LOT F09/0218  
BK/PG 63448/1

TRUSTEES OF THE 1 HUNT ROAD NOMINEE TRUST  
MAP/LOT F09/0218  
BK/PG 63448/1

N/F  
WEST PANTRY BROOK FARM LIMITED PARTNERSHIP  
667 CONCORD RD  
MAP/LOT F10/0200  
BK/PG 49200/410

N/F  
ALLAN L. WALLACK & DONNA NADINE,  
TRUSTEES OF THE WALLACK FAMILY  
67 THOMPSON DR  
MAP/LOT E09/0507  
BK/PG 60192/340

N/F  
WEST PANTRY BROOK FARM LIMITED PARTNERSHIP  
667 CONCORD RD  
MAP/LOT F10/0200  
BK/PG 49200/410

N/F  
WEST PANTRY BROOK FARM LIMITED PARTNERSHIP  
667 CONCORD RD  
MAP/LOT F10/0200  
BK/PG 49200/410

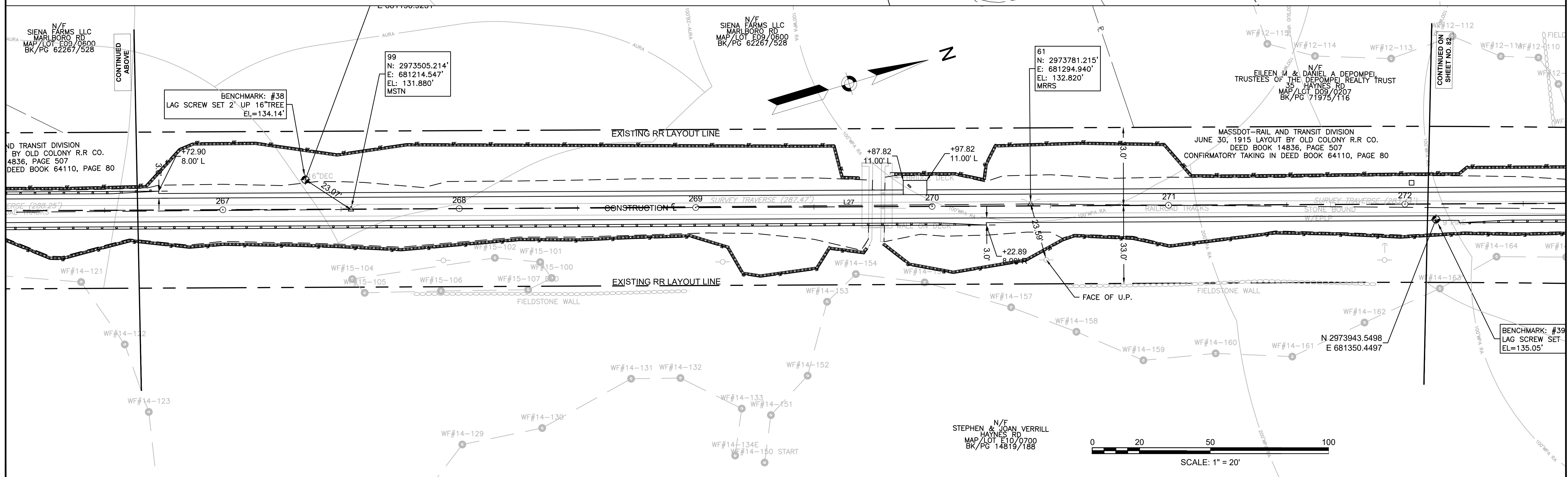
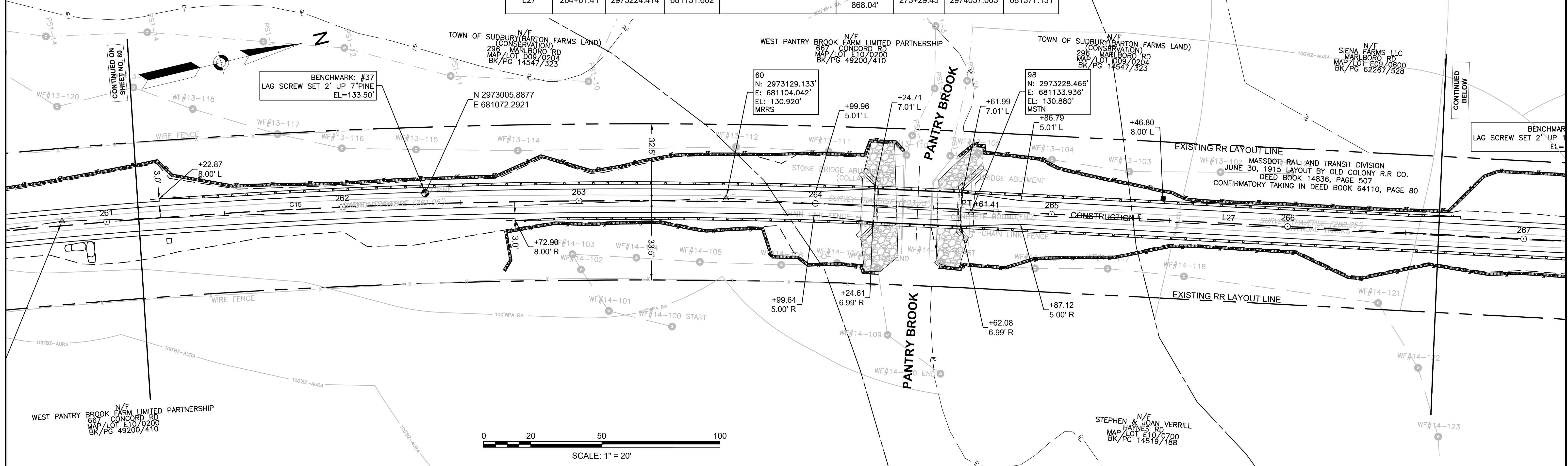
58  
N: 297  
E: 681  
EL: 13  
MIPE

LAG



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C15	258+99.00	2972672.357	681029.649	R-2700.00' Δ=11°56'05" L=562.41' T=282.23'		264+61.41	2973224.414	681131.602
L27	264+61.41	2973224.414	681131.602		N16°25'51"E 868.04'	273+29.45	2974057.003	681377.131

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	81	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



PR-BFRT CONSTRUCTION BASELINE DATA

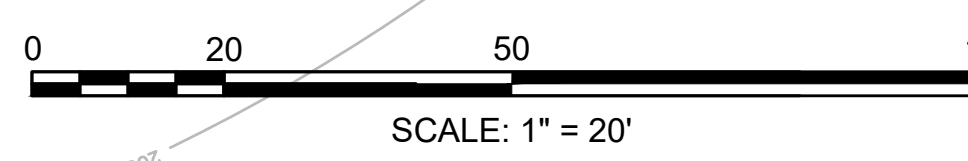
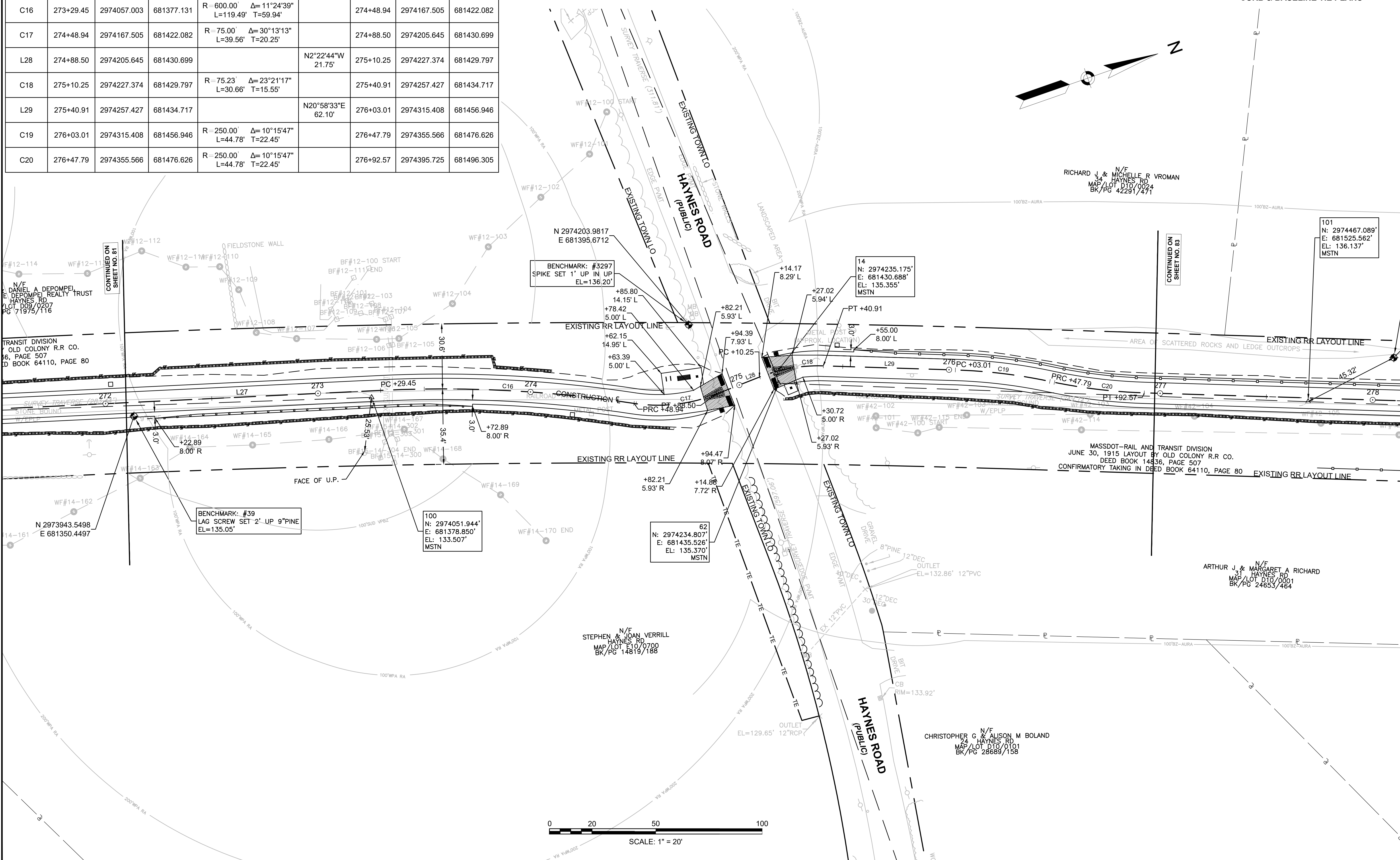
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L27	264+61.41	2973224.414	681131.602		N16°25'51"E 868.04'	273+29.45	2974057.003	681377.131
C16	273+29.45	2974057.003	681377.131	R=600.00' Δ=11°24'39" L=119.49' T=59.94'		274+48.94	2974167.505	681422.082
C17	274+48.94	2974167.505	681422.082	R=75.00' Δ=30°13'13" L=39.56' T=20.25'		274+88.50	2974205.645	681430.699
L28	274+88.50	2974205.645	681430.699		N2°22'44"W 21.75'	275+10.25	2974227.374	681429.797
C18	275+10.25	2974227.374	681429.797	R=75.23' Δ=23°21'17" L=30.66' T=15.55'		275+40.91	2974257.427	681434.717
L29	275+40.91	2974257.427	681434.717		N20°58'33"E 62.10'	276+03.01	2974315.408	681456.946
C19	276+03.01	2974315.408	681456.946	R=250.00' Δ=10°15'47" L=44.78' T=22.45'		276+47.79	2974355.566	681476.626
C20	276+47.79	2974355.566	681476.626	R=250.00' Δ=10°15'47" L=44.78' T=22.45'		276+92.57	2974395.725	681496.305

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	82	318

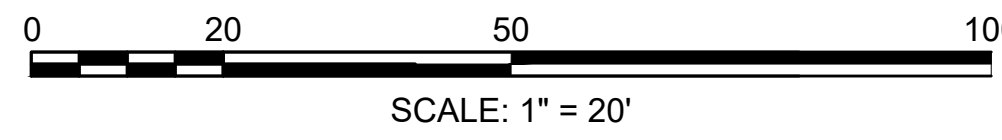
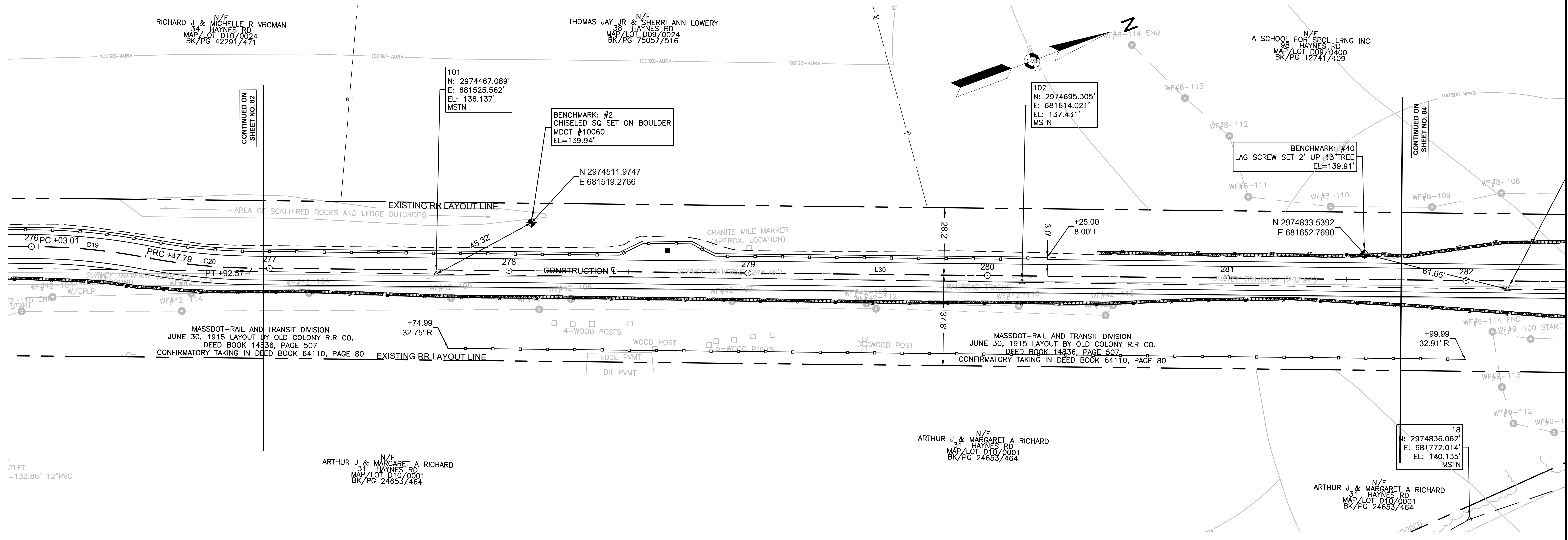
PROJECT FILE NO. 608164

**CURB & BASELINE TIE PLANS**



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L30	276+92.57	2974395.725	681496.305		N20°58'33"E 621.40'	283+13.96	2974975.941	681718.749

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	83	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



PR-BFRT CONSTRUCTION BASELINE DATA

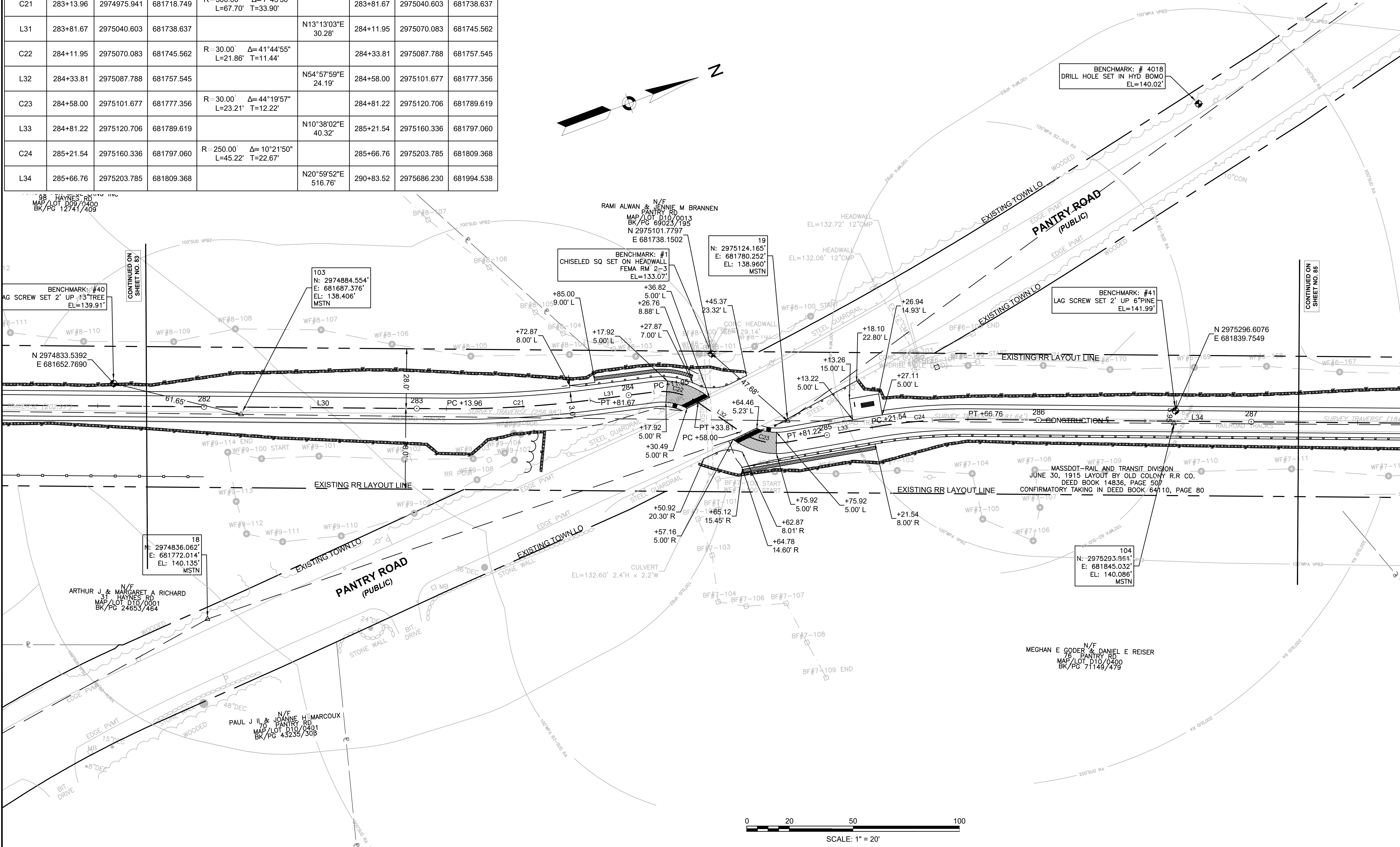
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L30	276+92.57	2974395.725	681496.305		N20°58'33"E 621.40'	283+13.96	2974975.941	681718.749
C21	283+13.96	2974975.941	681718.749	R=500.00' Δ=7°45'30" L=67.70' T=33.90'		283+81.67	2975040.603	681738.637
L31	283+81.67	2975040.603	681738.637		N13°13'03"E 30.28'	284+11.95	2975070.083	681745.562
C22	284+11.95	2975070.083	681745.562	R=30.00' Δ=41°44'55" L=21.86' T=11.44'		284+33.81	2975087.788	681757.545
L32	284+33.81	2975087.788	681757.545		N54°57'59"E 24.19'	284+58.00	2975101.677	681777.356
C23	284+58.00	2975101.677	681777.356	R=30.00' Δ=44°19'57" L=23.21' T=12.22'		284+81.22	2975120.706	681789.619
L33	284+81.22	2975120.706	681789.619		N10°38'02"E 40.32'	285+21.54	2975160.336	681797.060
C24	285+21.54	2975160.336	681797.060	R=250.00' Δ=10°21'50" L=45.22' T=22.67'		285+66.76	2975203.785	681809.368
L34	285+66.76	2975203.785	681809.368		N20°59'52"E 516.76'	290+83.52	2975686.230	681994.538

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	84	318

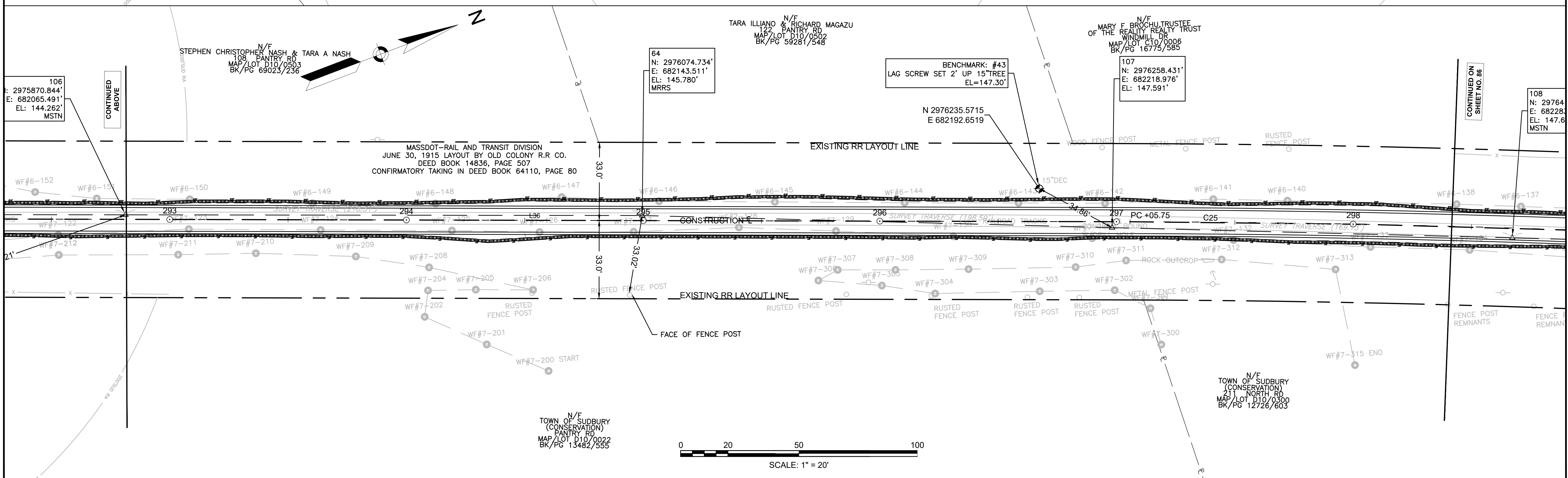
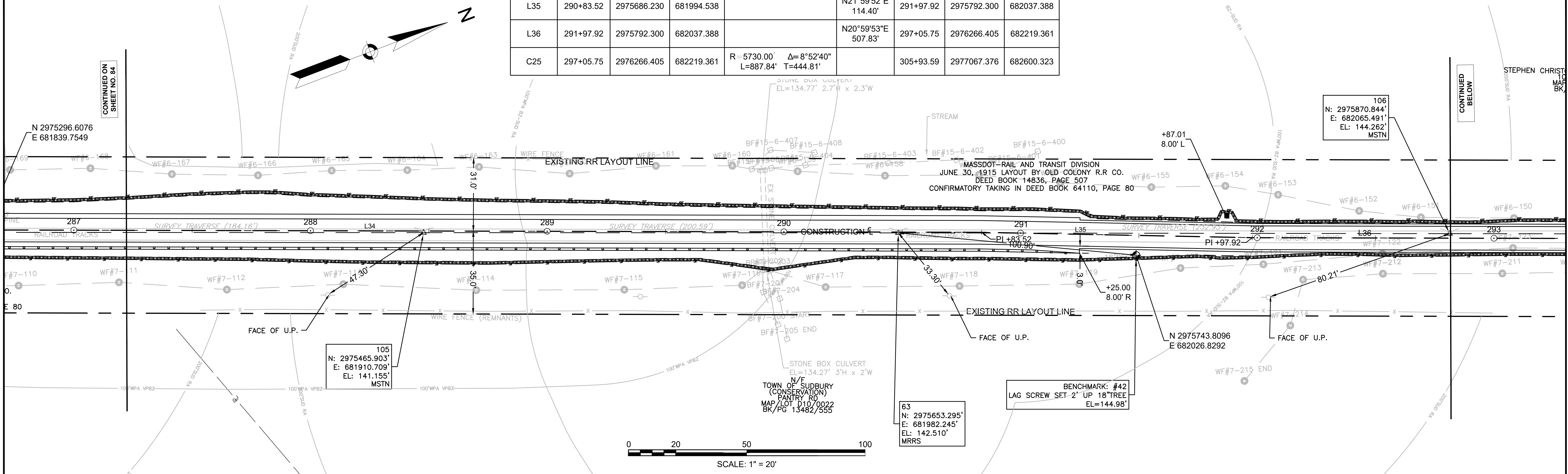
PROJECT FILE NO. 608164

**CURB & BASELINE TIE PLANS**



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L34	285+66.76	2975203.785	681809.368		N20°59'52"E 516.76'	290+83.52	2975686.230	681994.538
L35	290+83.52	2975686.230	681994.538		N21°59'52"E 114.40'	291+97.92	2975792.300	682037.388
L36	291+97.92	2975792.300	682037.388		N20°59'53"E 507.83'	297+05.75	2976266.405	682219.361
C25	297+05.75	2976266.405	682219.361	R=5730.00 L=887.84'	Δ=8°52'40" T=444.81'	305+93.59	2977067.376	682600.323

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	85	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



608164\_HDCURB & BASELINE TIE PLANS.DWG Plotted on 12-May-2021 11:13 AM

PR-BFRT CONSTRUCTION BASELINE DATA

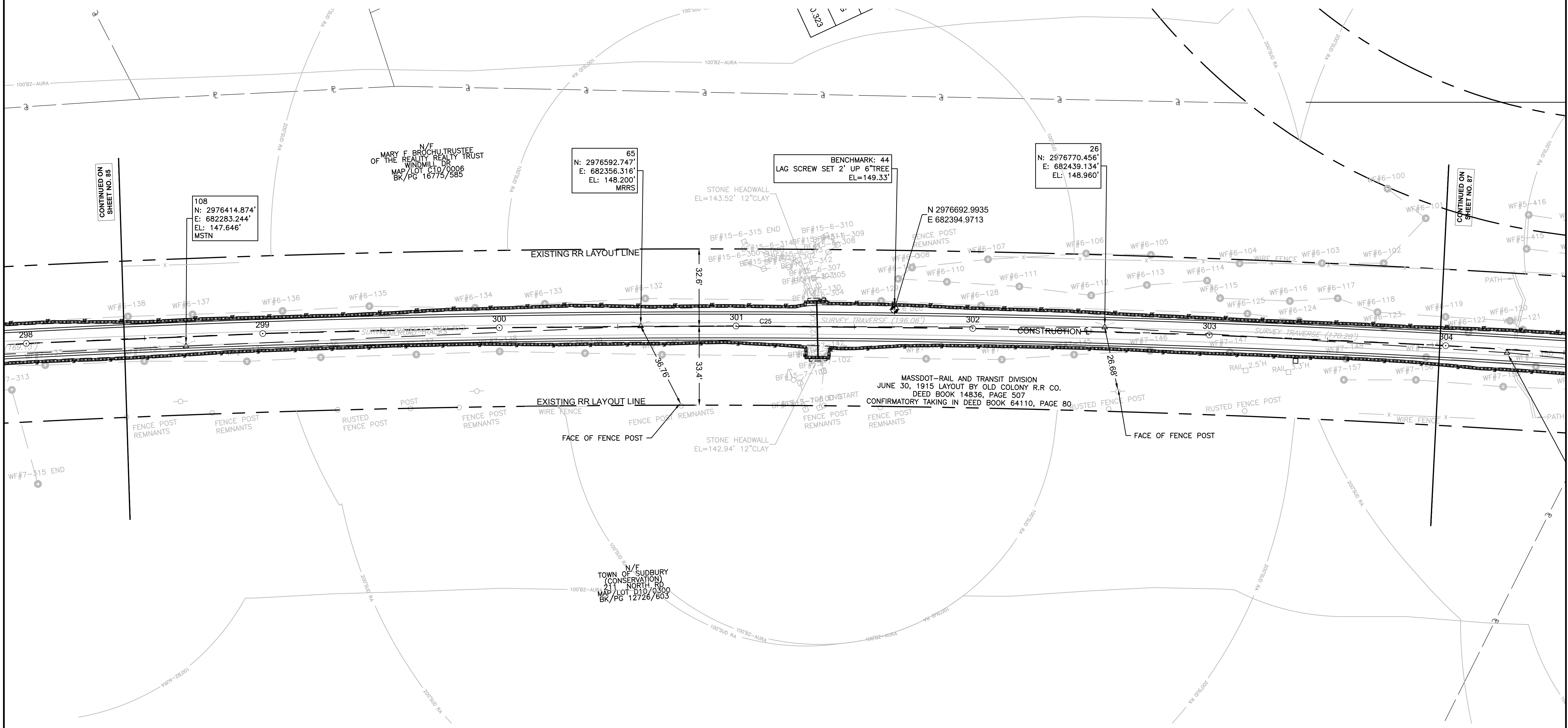
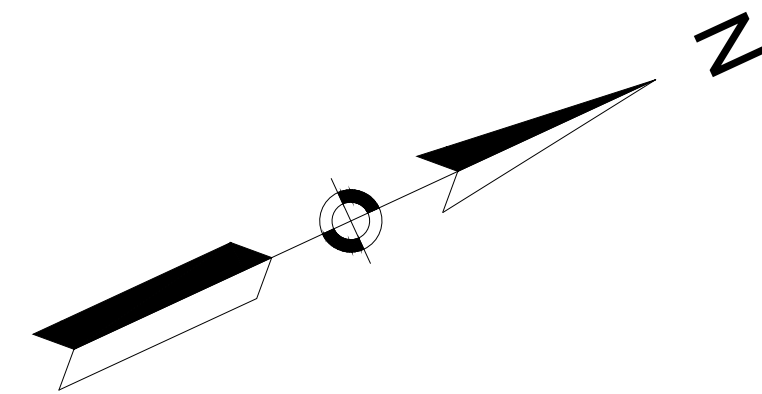
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C25	297+05.75	2976266.405	682219.361	R=5730.00 L=887.84' Δ=8°52'40" T=444.81'		305+93.59	2977067.376	682600.323

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	86	318

PROJECT FILE NO. 608164

**CURB & BASELINE TIE PLANS**



CONTINUED ON SHEET NO. 85

CONTINUED ON SHEET NO. 87

108  
N: 2976414.874'  
E: 682283.244'  
EL: 147.646'  
MSTN

65  
N: 2976592.747'  
E: 682356.316'  
EL: 148.200'  
MRRS

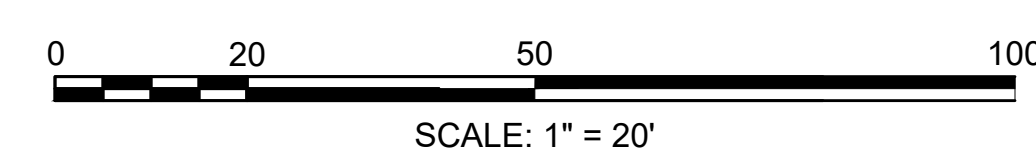
BENCHMARK: 44  
LAG SCREW SET 2' UP 6" TREE  
EL=149.33'

26  
N: 2976770.456'  
E: 682439.134'  
EL: 148.960'

N 2976692.9935  
E 682394.9713

MASSDOT-RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

N/E  
TOWN OF SUDBURY  
(CONSERVATION)  
211 NORTH RD  
MAP LOT D10/0300  
BK/PG 12726/603



PR-BFRT CONSTRUCTION BASELINE DATA

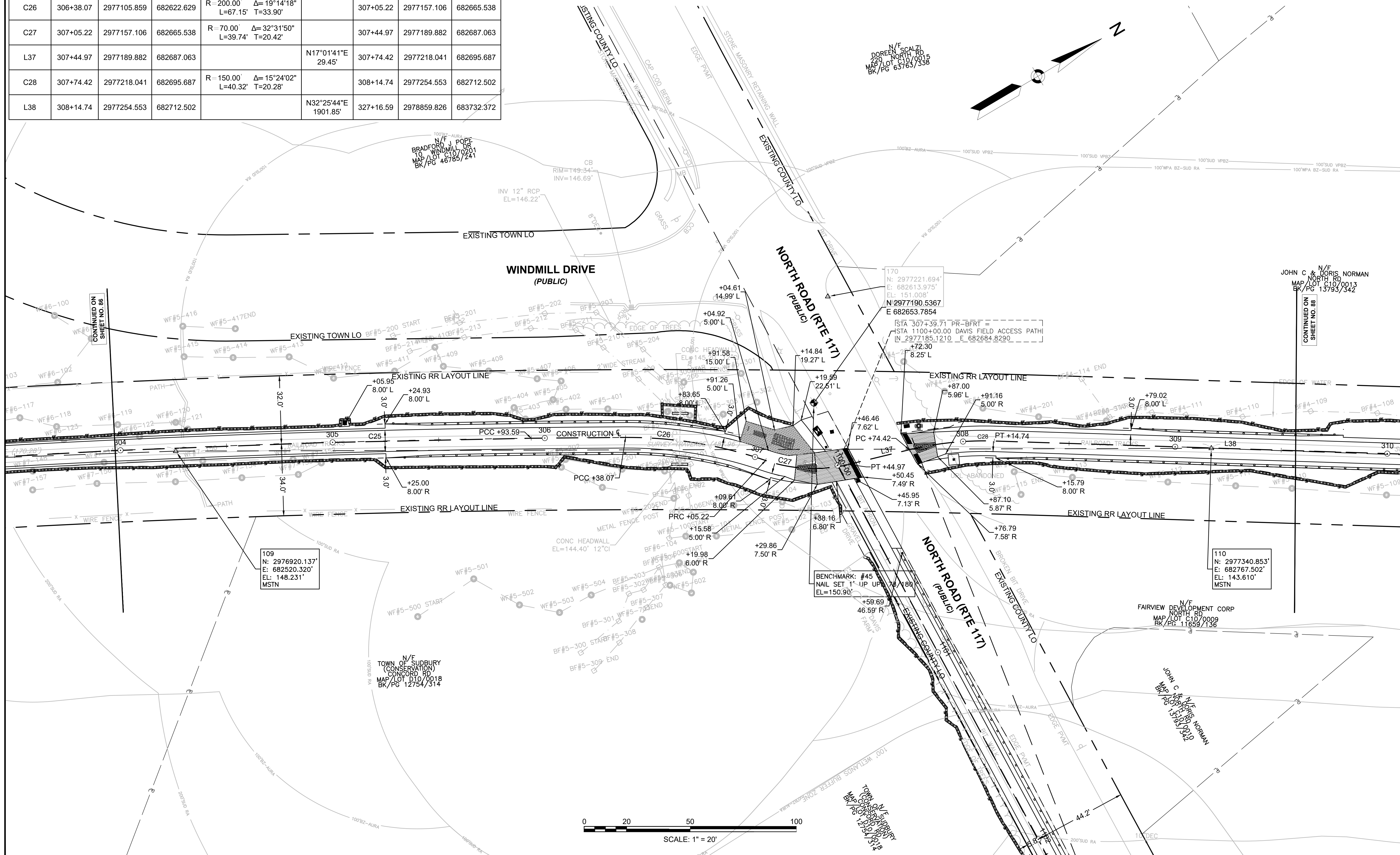
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C25	297+05.75	2976266.405	682219.361	R = 5730.00' Δ= 8°52'40" L=887.84' T=444.81'		305+93.59	2977067.376	682600.323
C26	306+38.07	2977105.859	682622.629	R = 200.00' Δ= 19°14'18" L=67.15' T=33.90'		307+05.22	2977157.106	682665.538
C27	307+05.22	2977157.106	682665.538	R = 70.00' Δ= 32°31'50" L=39.74' T=20.42'		307+44.97	2977189.882	682687.063
L37	307+44.97	2977189.882	682687.063		N17°01'41"E 29.45'	307+74.42	2977218.041	682695.687
C28	307+74.42	2977218.041	682695.687	R = 150.00' Δ= 15°24'02" L=40.32' T=20.28'		308+14.74	2977254.553	682712.502
L38	308+14.74	2977254.553	682712.502		N32°25'44"E 1901.85'	327+16.59	2978859.826	683732.372

**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	87	318

PROJECT FILE NO. 608164

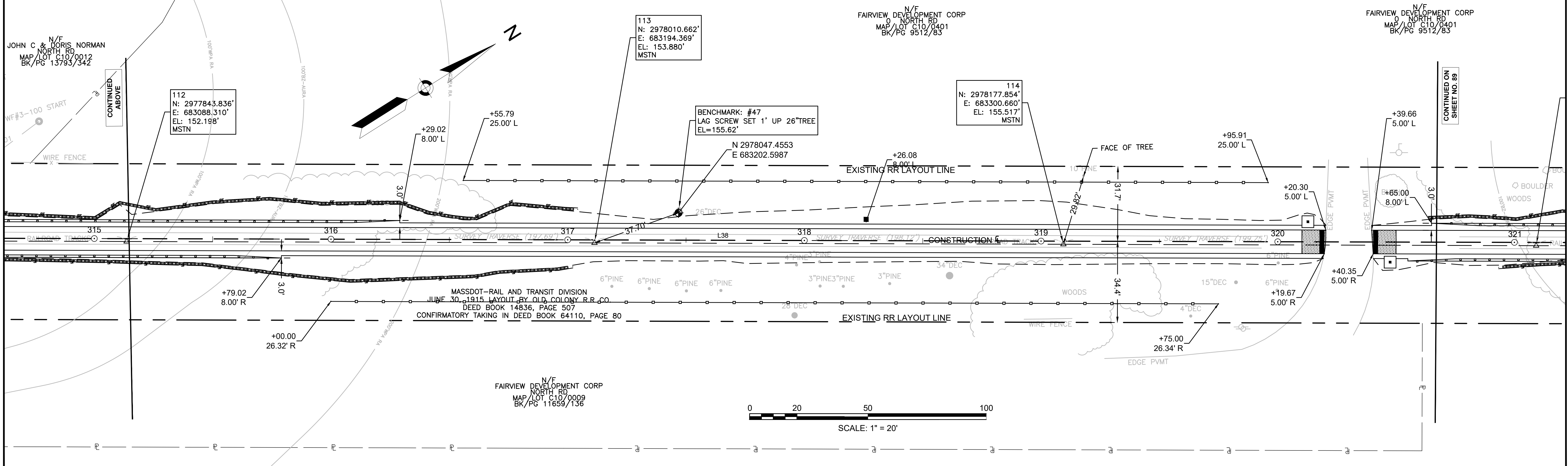
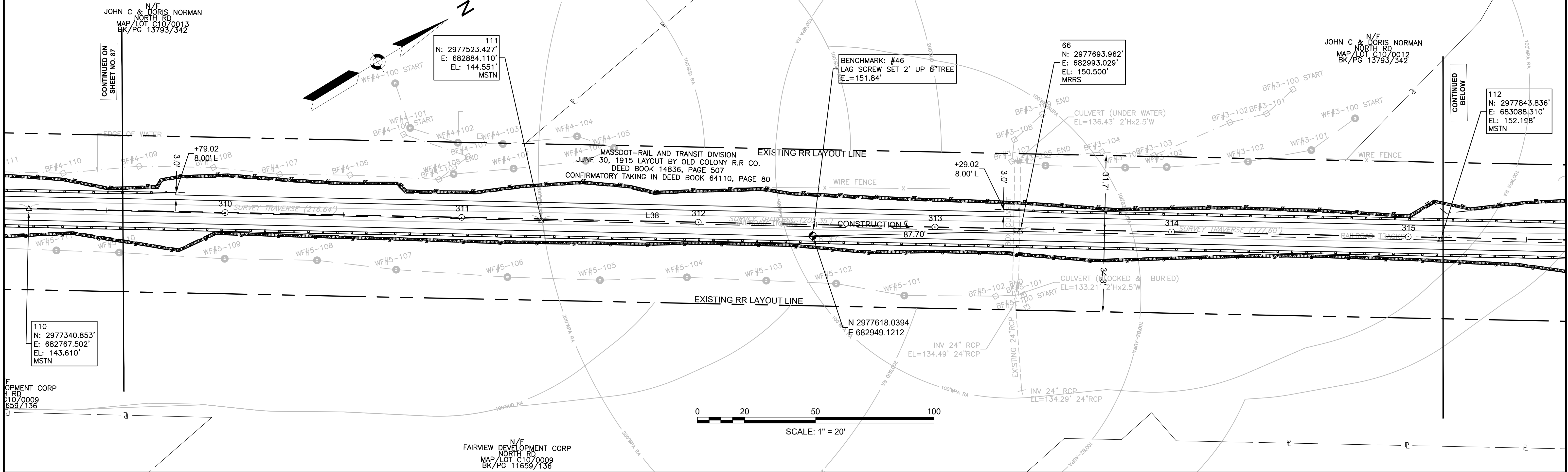
**CURB & BASELINE TIE PLANS**



PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L38	308+14.74	2977254.553	682712.502		N32°25'44"E 1901.85'	327+16.59	2978859.826	683732.372

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	88	318
PROJECT FILE NO.		608164	

**CURB & BASELINE TIE PLANS**





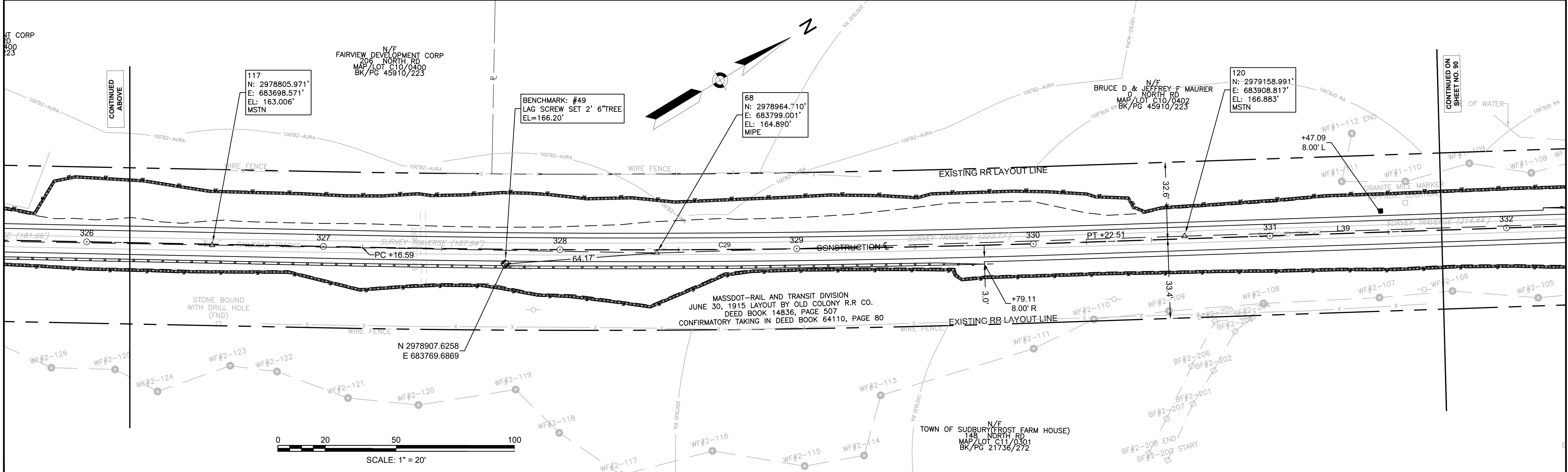
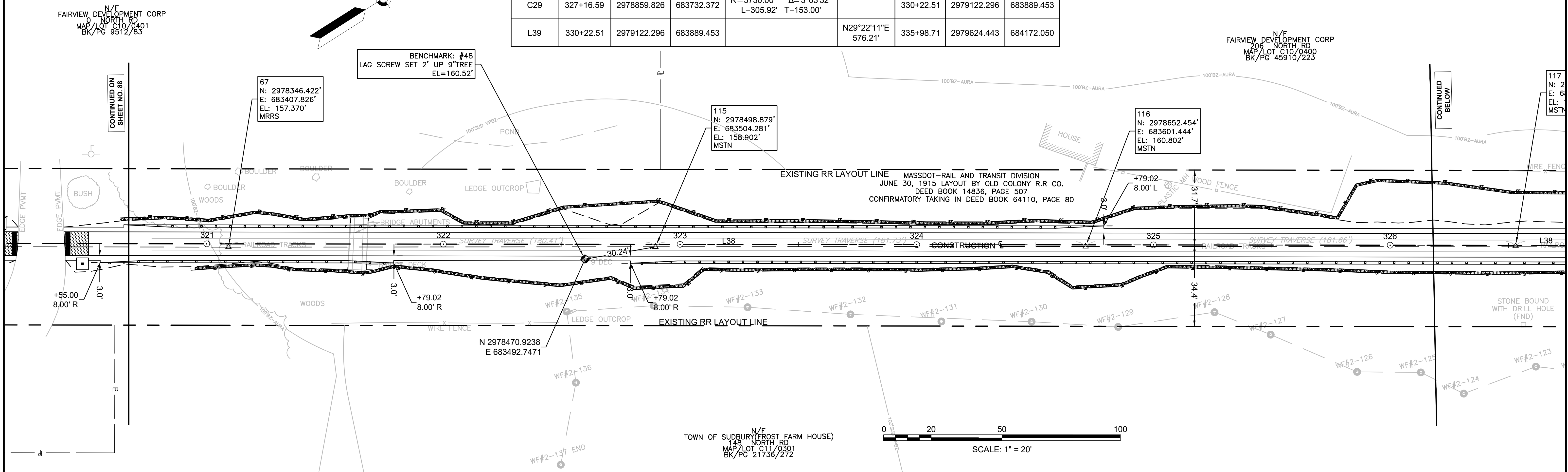
**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

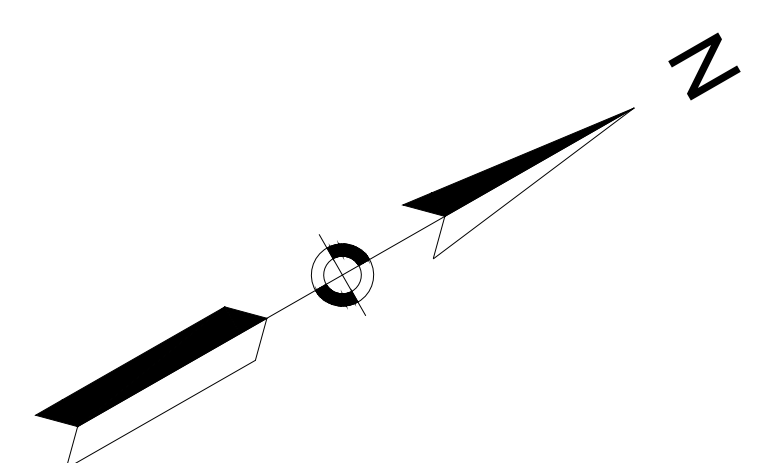
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	89	318

PROJECT FILE NO. 608164

**CURB & BASELINE TIE PLANS**

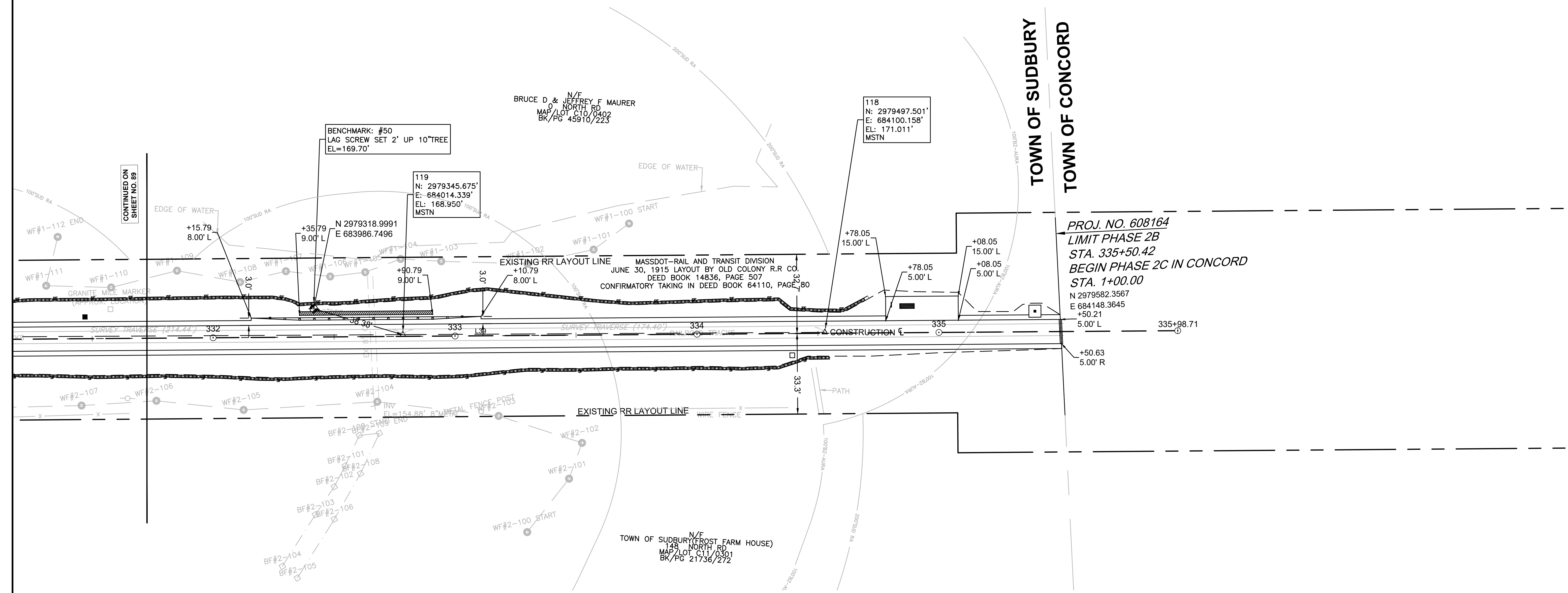
PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L38	308+14.74	2977254.553	682712.502		N32°25'44"E 1901.85'	327+16.59	2978859.826	683732.372
C29	327+16.59	2978859.826	683732.372	R=5730.00' Δ=3°03'32" L=305.92' T=153.00'		330+22.51	2979122.296	683889.453
L39	330+22.51	2979122.296	683889.453		N29°22'11"E 576.21'	335+98.71	2979624.443	684172.050



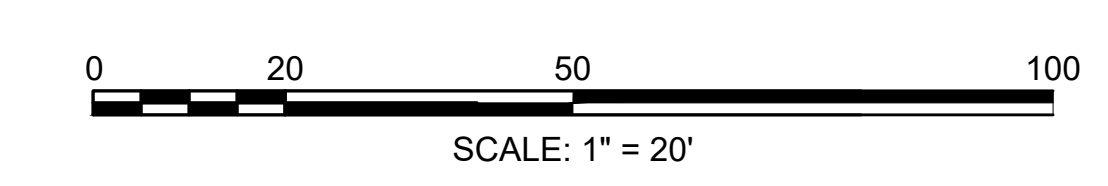


PR-BFRT CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L39	330+22.51	2979122.296	683889.453		N29°22'11"E 576.21'	335+98.71	2979624.443	684172.050

SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	90	318
PROJECT FILE NO.		608164	
CURB & BASELINE TIE PLANS			



PROJ. NO. 608164  
 LIMIT PHASE 2B  
 STA. 335+50.42  
 BEGIN PHASE 2C IN CONCORD  
 STA. 1+00.00  
 N 2979582.3567  
 E 684148.3645  
 +50.21  
 5.00' L  
 335+98.71  
 +50.63  
 5.00' R



PARKINSON LOT ACCESS PATH CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L1001	1000+00.00	2965485.946	679063.812		N41°30'12"E 94.96'	1000+94.96	2965557.063	679126.739
C1001	1000+94.96	2965557.063	679126.739	R=70.00' Δ=114°20'20" L=139.69' T=108.49'		1002+34.65	2965539.324	679243.032
L1002	1002+90.89	2965502.111	679283.187		S70°11'49"E 70.57'	1003+61.46	2965478.204	679349.582

01  
N: 2965640.534'  
E: 679408.927'  
EL: 182.812'  
MSTN POL

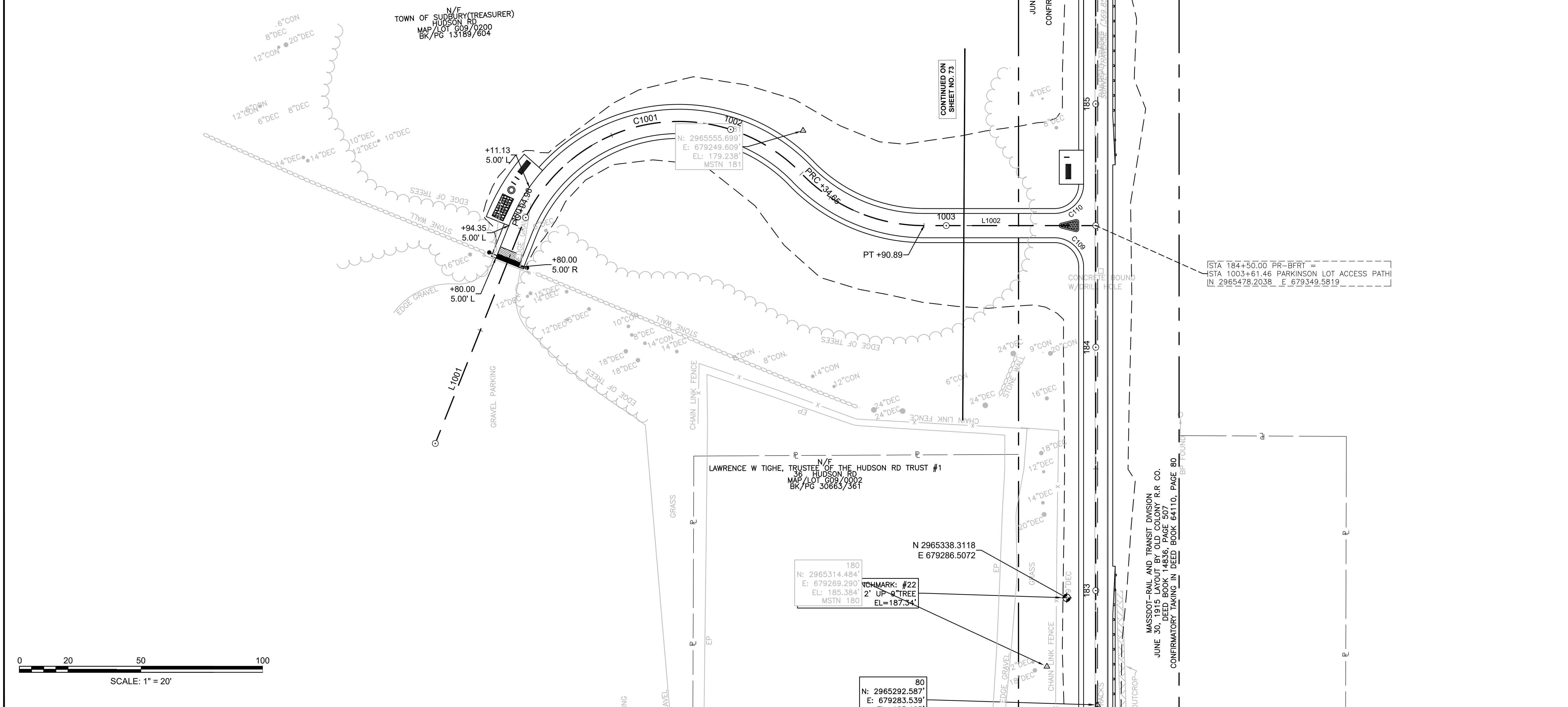
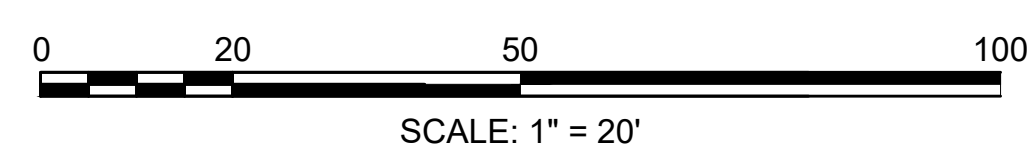
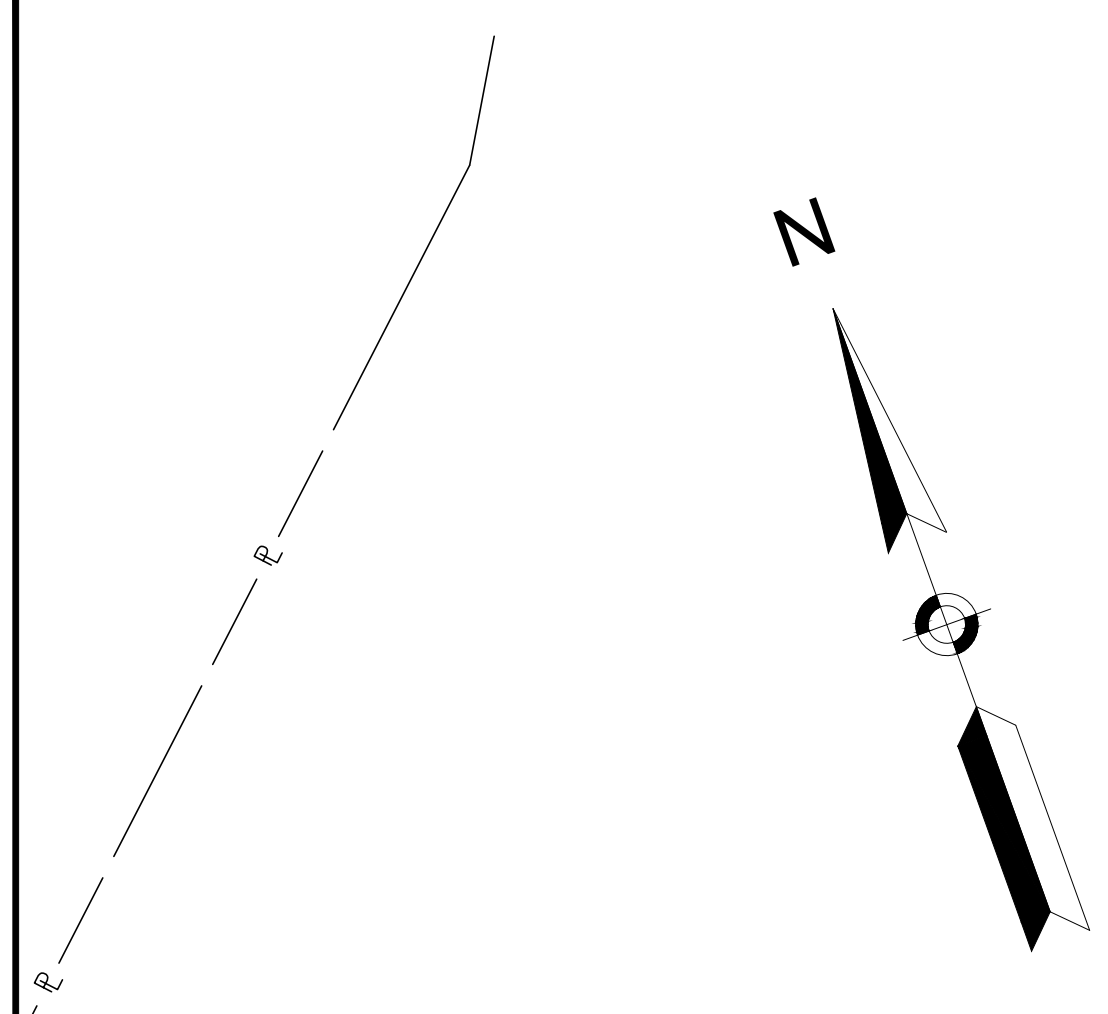
MASSDOT-RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

CONTINUED ON  
SHEET NO. 73

STA 184+50.00 PR-BFRT =  
STA 1003+61.46 PARKINSON LOT ACCESS PATH  
IN 2965478.2038 E 679349.5819

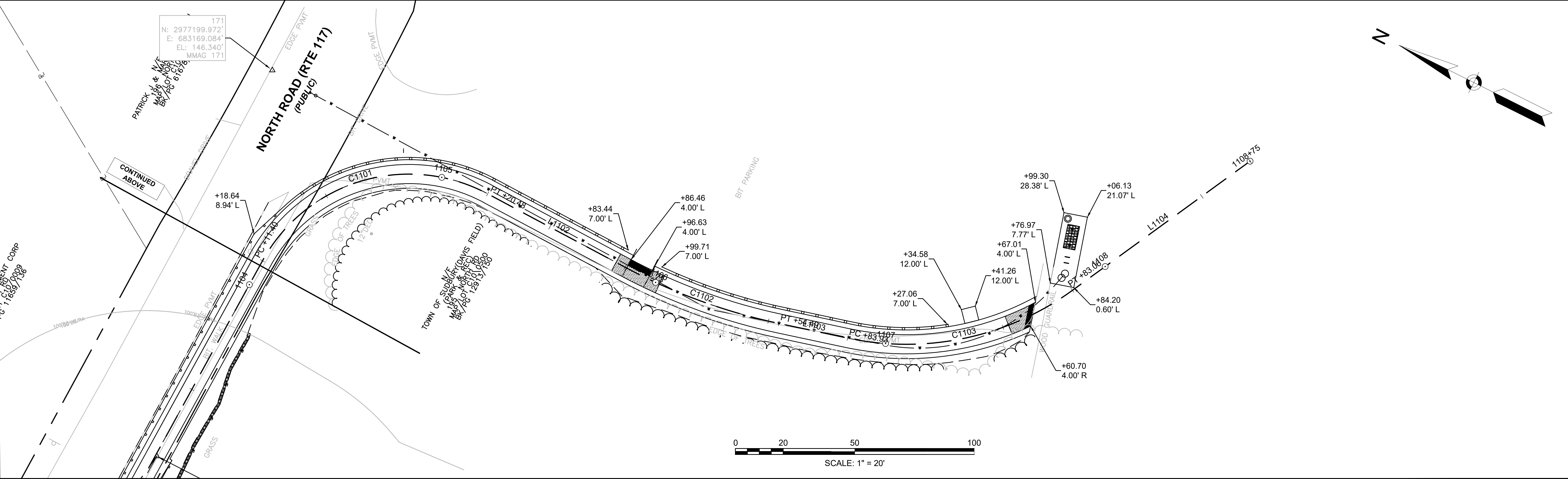
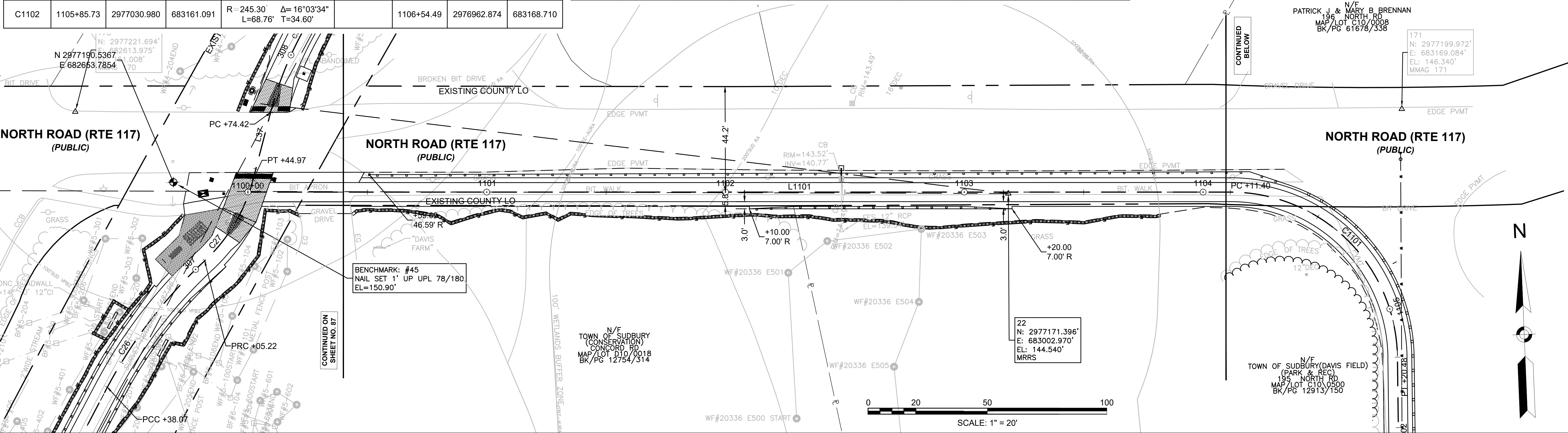
180  
N: 2965314.484'  
E: 679269.290'  
EL: 185.384'  
MSTN 180  
CHECKMARK: #22  
2' UP OAK TREE  
EL=187.34'

80  
N: 2965292.587'  
E: 679283.539'  
EL: 185.185'



DAVIS FIELD ACCESS PATH CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L1101	1100+00.00	2977185.121	682684.829		S87°38'12"E 411.40'	1104+11.40	2977168.156	683095.882
C1101	1104+11.40	2977168.156	683095.882	R=70.00' Δ=89°17'00" L=109.08' T=69.13'		1105+20.48	2977096.204	683162.966
L1102	1105+20.48	2977096.204	683162.966		S1°38'48"W 65.25'	1105+85.73	2977030.980	683161.091
C1102	1105+85.73	2977030.980	683161.091	R=245.30' Δ=16°03'34" L=68.76' T=34.60'		1106+54.49	2976962.874	683168.710

DAVIS FIELD ACCESS PATH CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L1103	1106+54.49	2976962.874	683168.710		S14°24'46"E 29.44'	1106+83.93	2976934.361	683176.038
C1103	1106+83.93	2976934.361	683176.038	R=118.28' Δ=48°01'17" L=99.14' T=52.69'		1107+83.06	2976858.948	683235.860
L1104	1107+83.06	2976858.948	683235.860		S62°26'03"E 91.93'	1108+75.00	2976816.405	683317.356



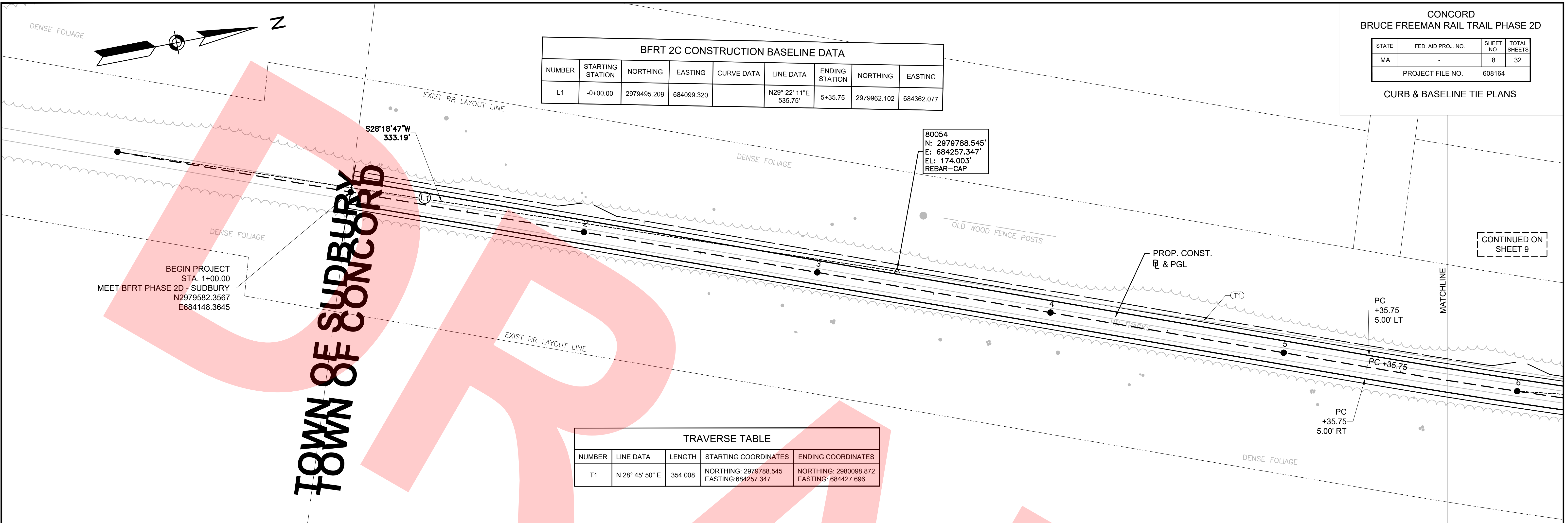
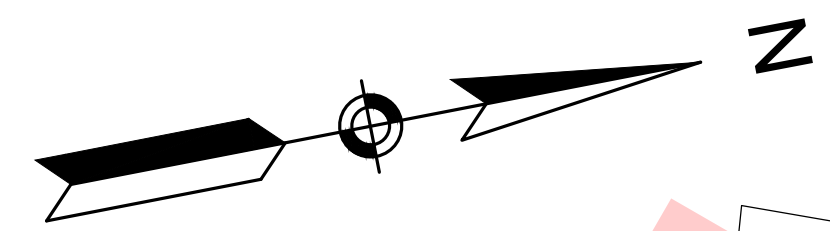
CONCORD  
BRUCE FREEMAN RAIL TRAIL PHASE 2D

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	-	8	32
PROJECT FILE NO.		608164	

CURB & BASELINE TIE PLANS

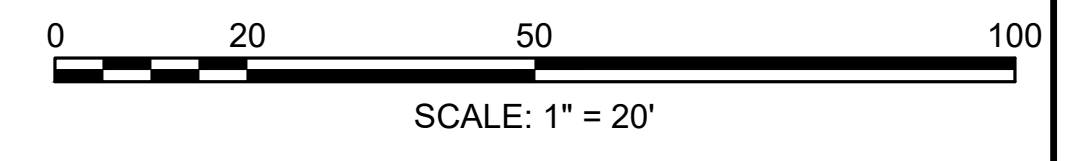
BFR2 2C CONSTRUCTION BASELINE DATA

NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L1	-0+00.00	2979495.209	684099.320		N29° 22' 11"E 535.75'	5+35.75	2979962.102	684362.077



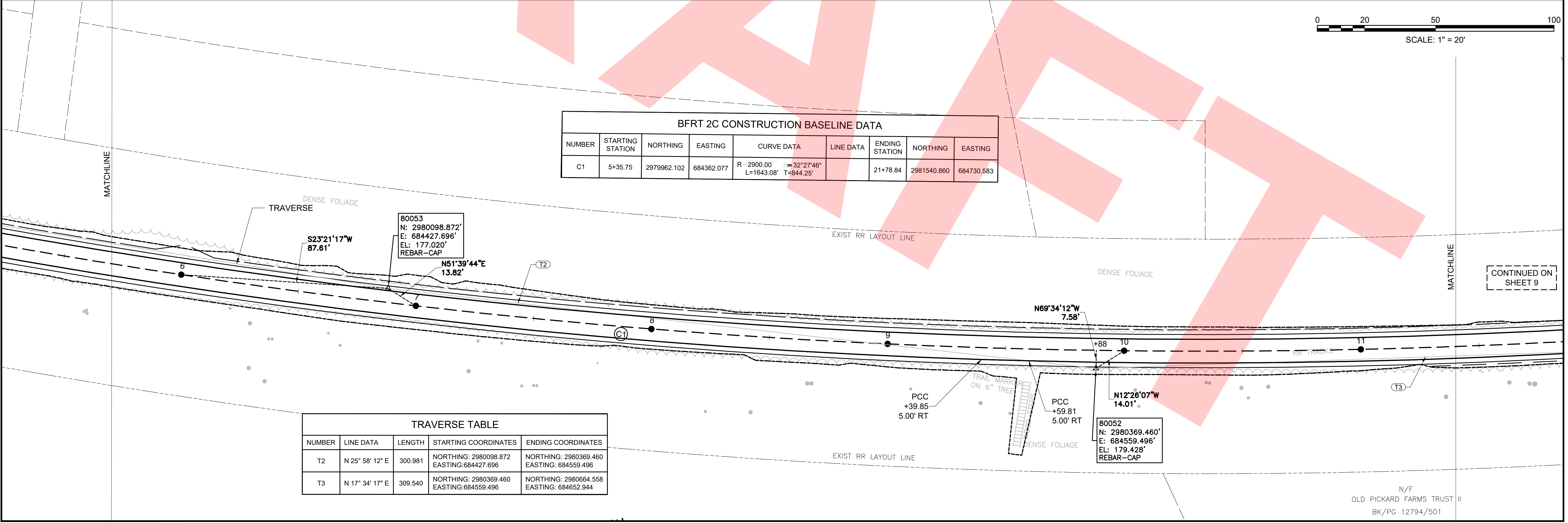
TRAVERSE TABLE

NUMBER	LINE DATA	LENGTH	STARTING COORDINATES	ENDING COORDINATES
T1	N 28° 45' 50" E	354.008	NORTHING: 2979788.545 EASTING: 684257.347	NORTHING: 2980098.872 EASTING: 684427.696



BFR2 2C CONSTRUCTION BASELINE DATA

NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C1	5+35.75	2979962.102	684362.077	R=2900.00' / Δ=32°27'46" L=1643.08' T=844.25'		21+78.84	2981540.860	684730.583



TRAVERSE TABLE

NUMBER	LINE DATA	LENGTH	STARTING COORDINATES	ENDING COORDINATES
T2	N 25° 58' 12" E	300.981	NORTHING: 2980098.872 EASTING: 684427.696	NORTHING: 2980369.460 EASTING: 684559.496
T3	N 17° 34' 17" E	309.540	NORTHING: 2980369.460 EASTING: 684559.496	NORTHING: 2980664.558 EASTING: 684652.944

N/F  
OLD PICKARD FARMS TRUST II  
BK/PG 12794/501

TRAVERSE TABLE

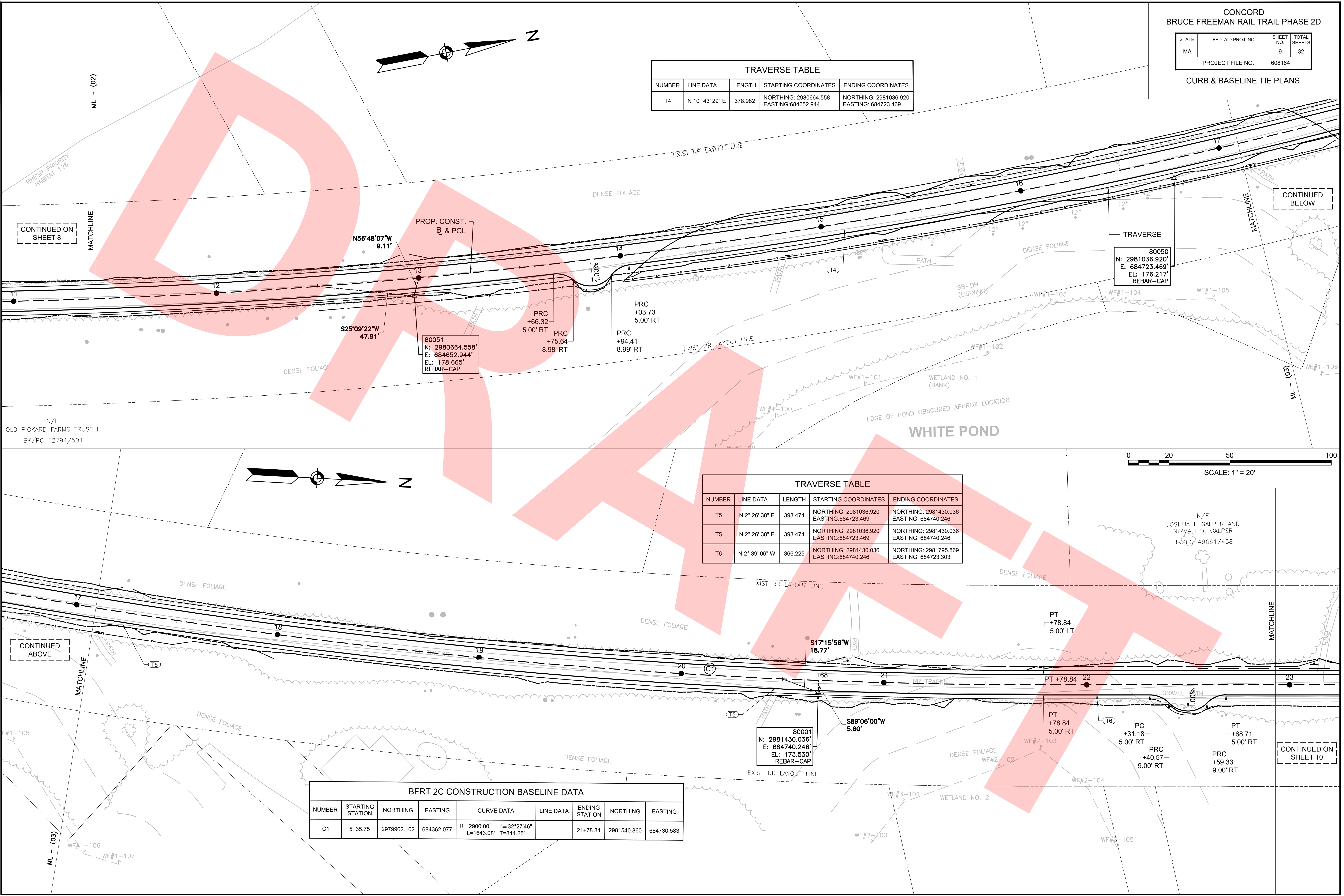
NUMBER	LINE DATA	LENGTH	STARTING COORDINATES	ENDING COORDINATES
T4	N 10° 43' 29" E	378.982	NORTHING: 2980664.558 EASTING: 684652.944	NORTHING: 2981036.920 EASTING: 684723.469

TRAVERSE TABLE

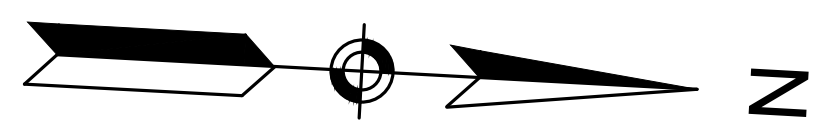
NUMBER	LINE DATA	LENGTH	STARTING COORDINATES	ENDING COORDINATES
T5	N 2° 26' 38" E	393.474	NORTHING: 2981036.920 EASTING: 684723.469	NORTHING: 2981430.036 EASTING: 684740.246
T5	N 2° 26' 38" E	393.474	NORTHING: 2981036.920 EASTING: 684723.469	NORTHING: 2981430.036 EASTING: 684740.246
T6	N 2° 39' 06" W	366.225	NORTHING: 2981430.036 EASTING: 684740.246	NORTHING: 2981795.869 EASTING: 684723.303

BFRT 2C CONSTRUCTION BASELINE DATA

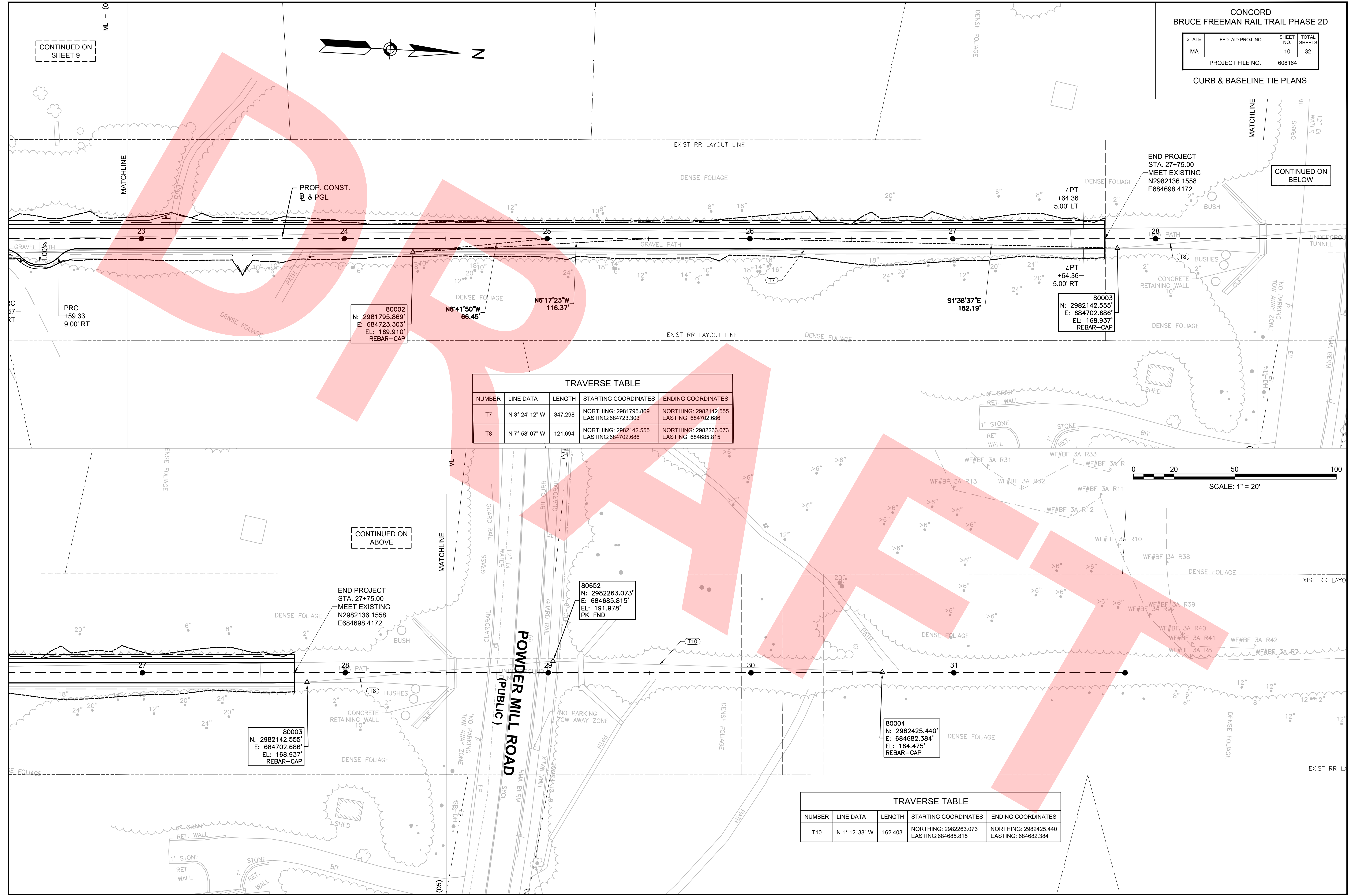
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C1	5+35.75	2979962.102	684362.077	R=2900.00' Δ=32°27'46" L=1643.08' T=844.25'		21+78.84	2981540.860	684730.583



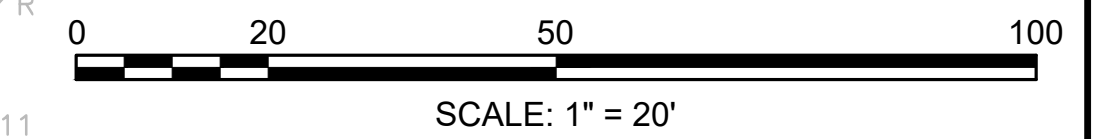
CONTINUED ON SHEET 9



CONTINUED ON BELOW



TRAVERSE TABLE				
NUMBER	LINE DATA	LENGTH	STARTING COORDINATES	ENDING COORDINATES
T7	N 3° 24' 12" W	347.298	NORTHING: 2981795.869 EASTING: 684723.303	NORTHING: 2982142.555 EASTING: 684702.686
T8	N 7° 58' 07" W	121.694	NORTHING: 2982142.555 EASTING: 684702.686	NORTHING: 2982263.073 EASTING: 684685.815



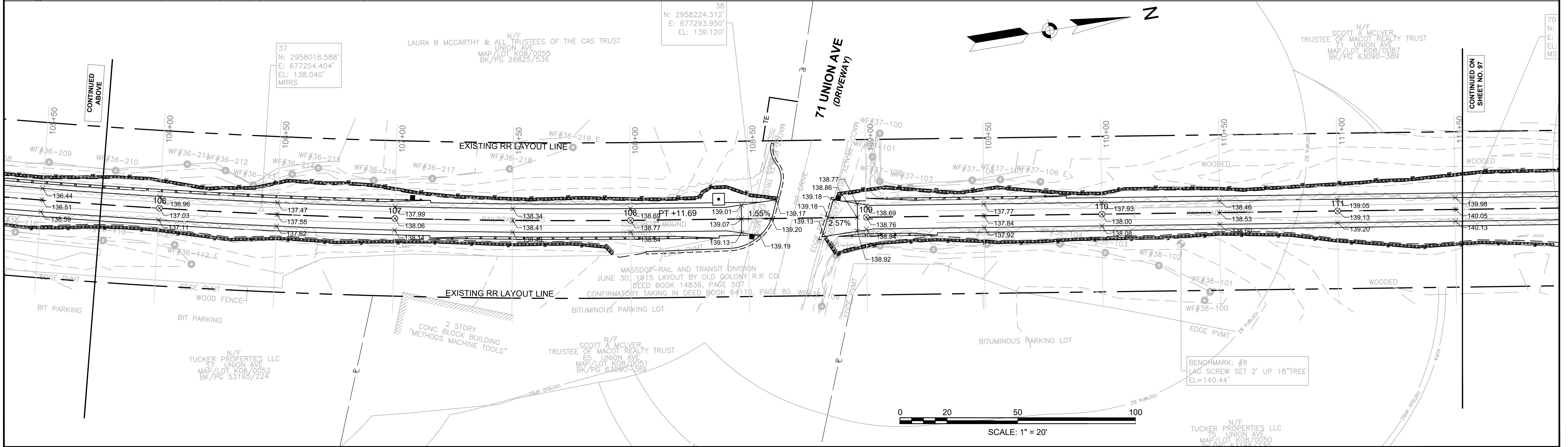
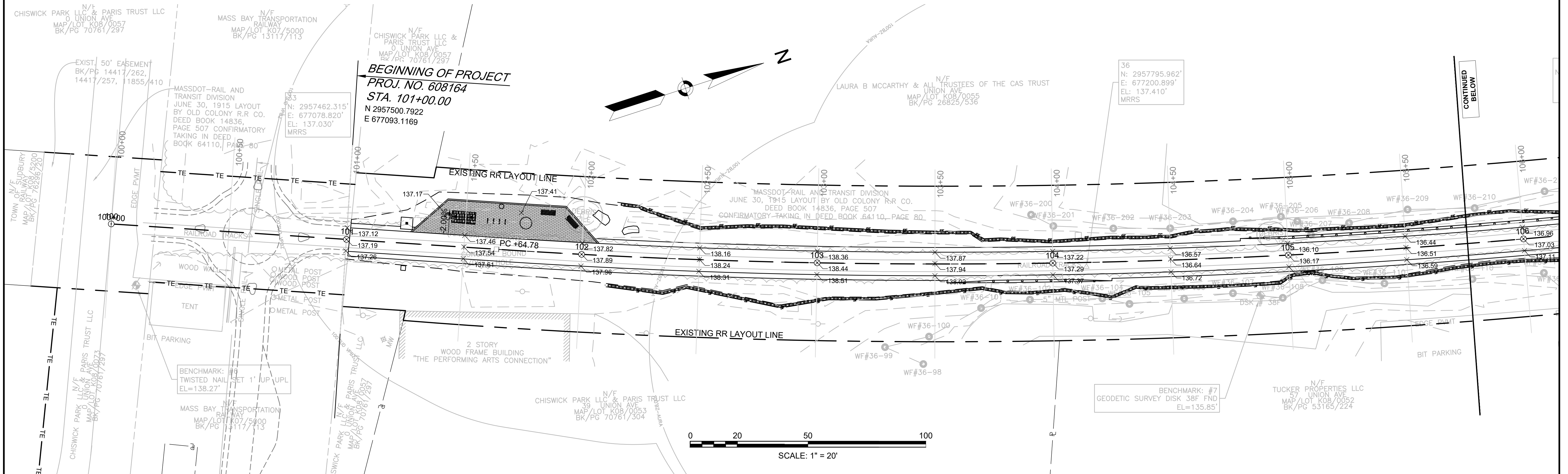
CONTINUED ON ABOVE

80003  
N: 2982142.555'  
E: 684702.686'  
EL: 168.937'  
REBAR-CAP

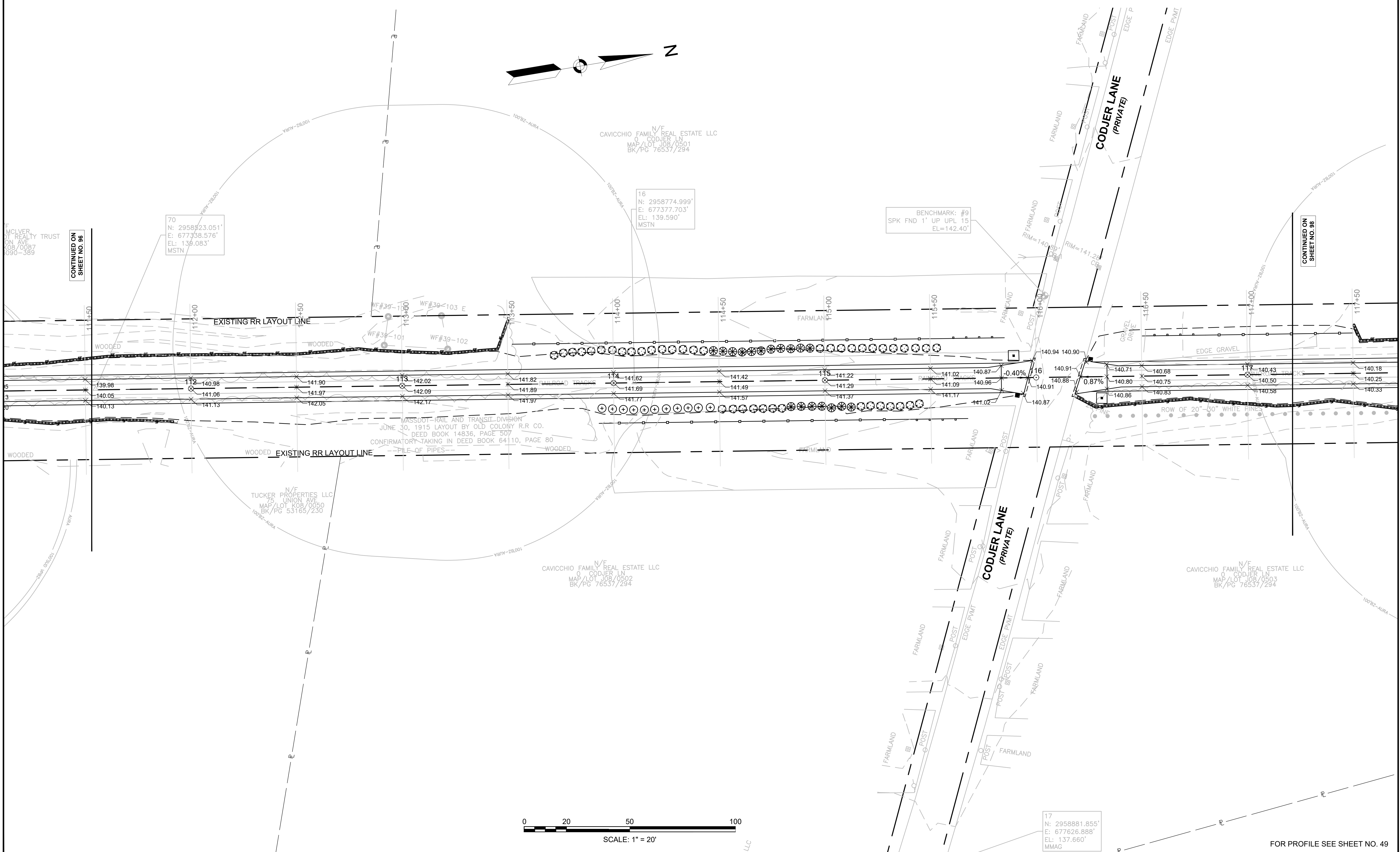
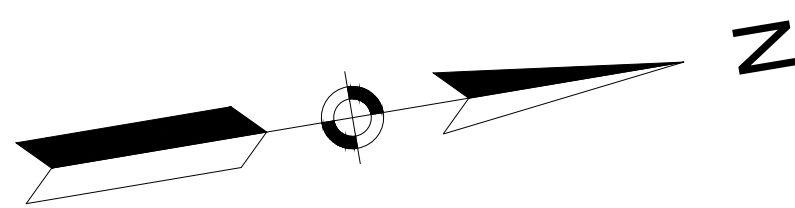
80652  
N: 2982263.073'  
E: 684685.815'  
EL: 191.978'  
PK FND

80004  
N: 2982425.440'  
E: 684682.384'  
EL: 164.475'  
REBAR-CAP

TRAVERSE TABLE				
NUMBER	LINE DATA	LENGTH	STARTING COORDINATES	ENDING COORDINATES
T10	N 1° 12' 38" W	162.403	NORTHING: 2982263.073 EASTING: 684685.815	NORTHING: 2982425.440 EASTING: 684682.384

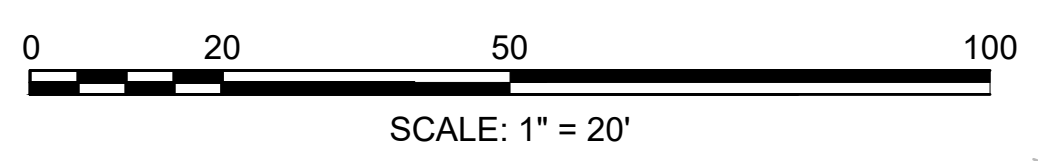






CONTINUED ON SHEET NO. 96

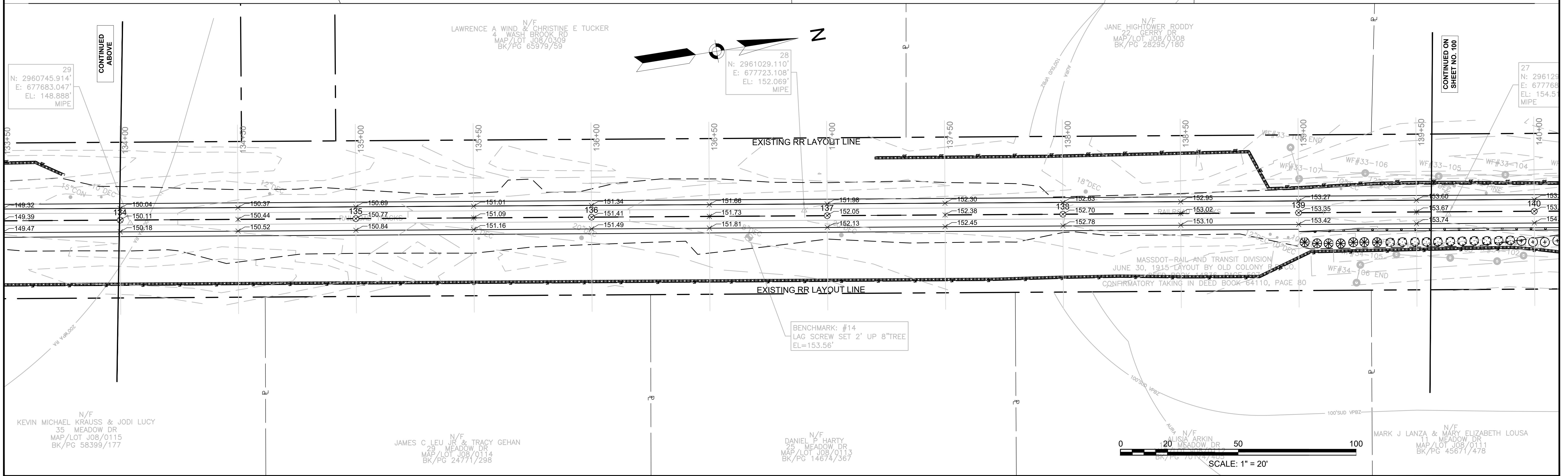
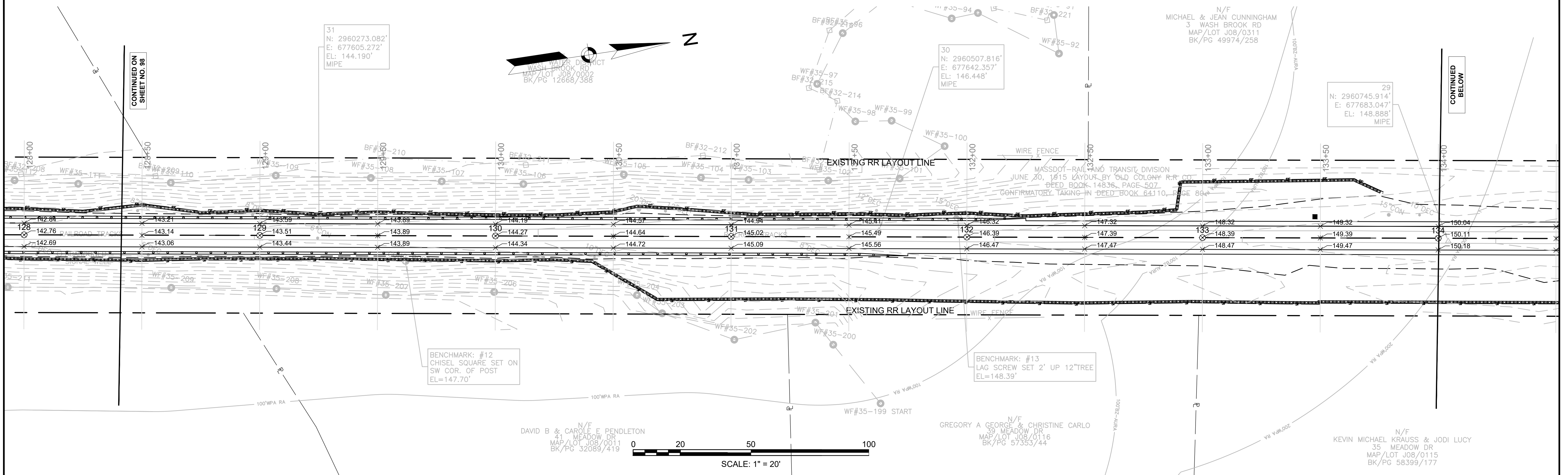
CONTINUED ON SHEET NO. 98

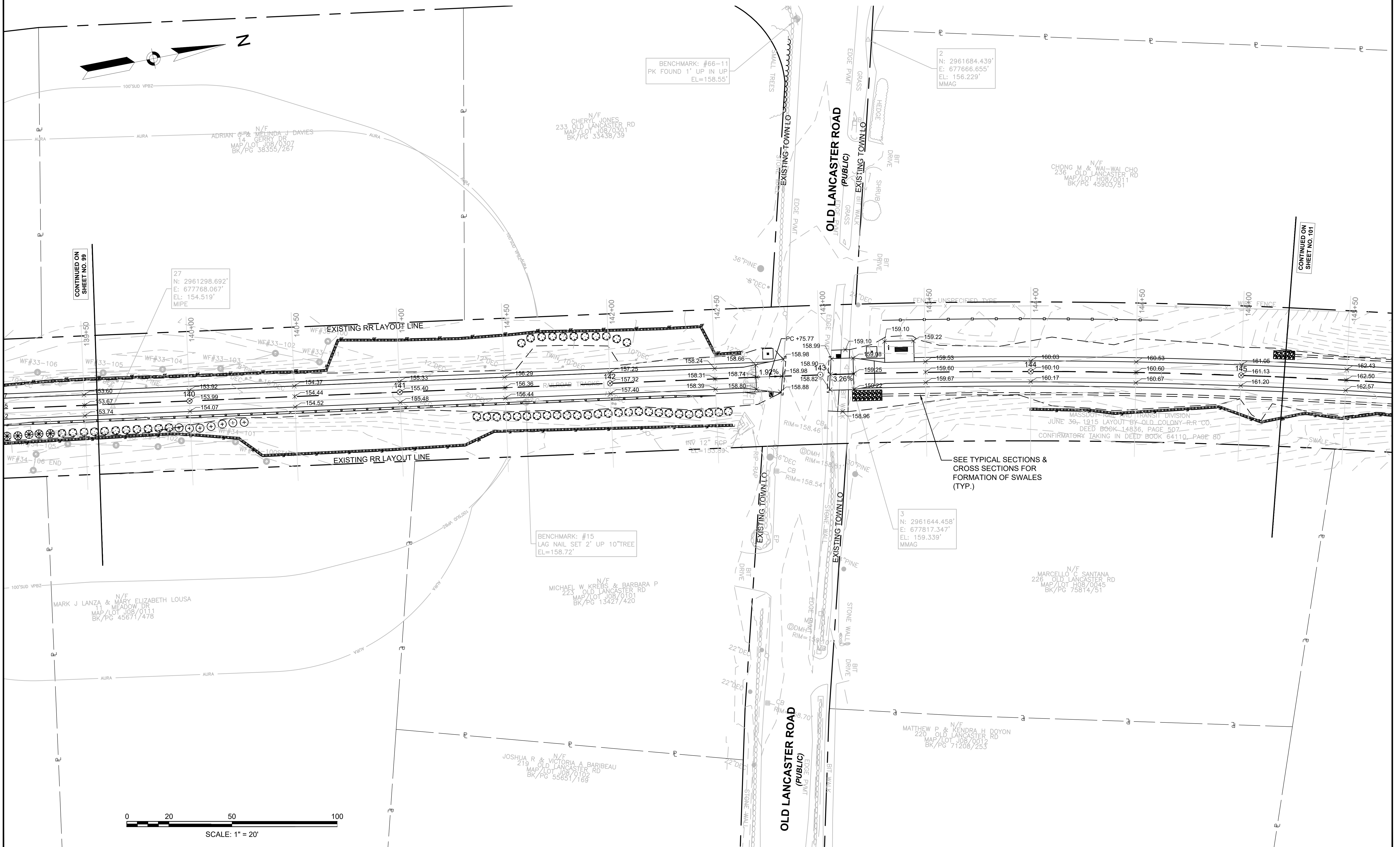
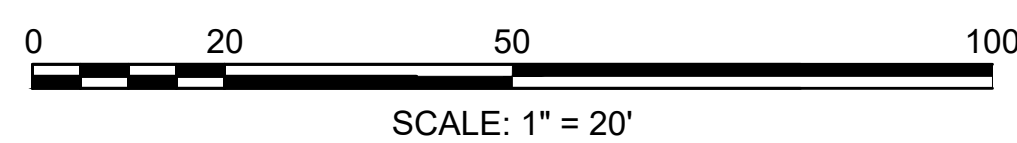
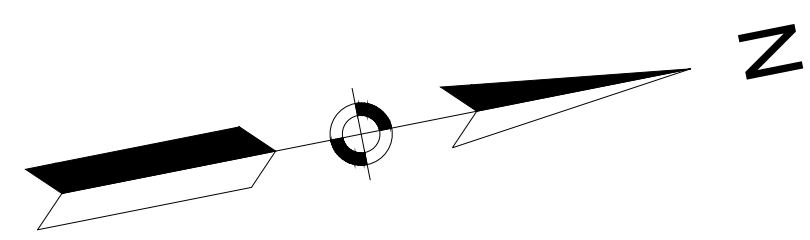


17  
N: 2958881.855'  
E: 677626.888'  
EL: 137.660'  
MMAG

FOR PROFILE SEE SHEET NO. 49







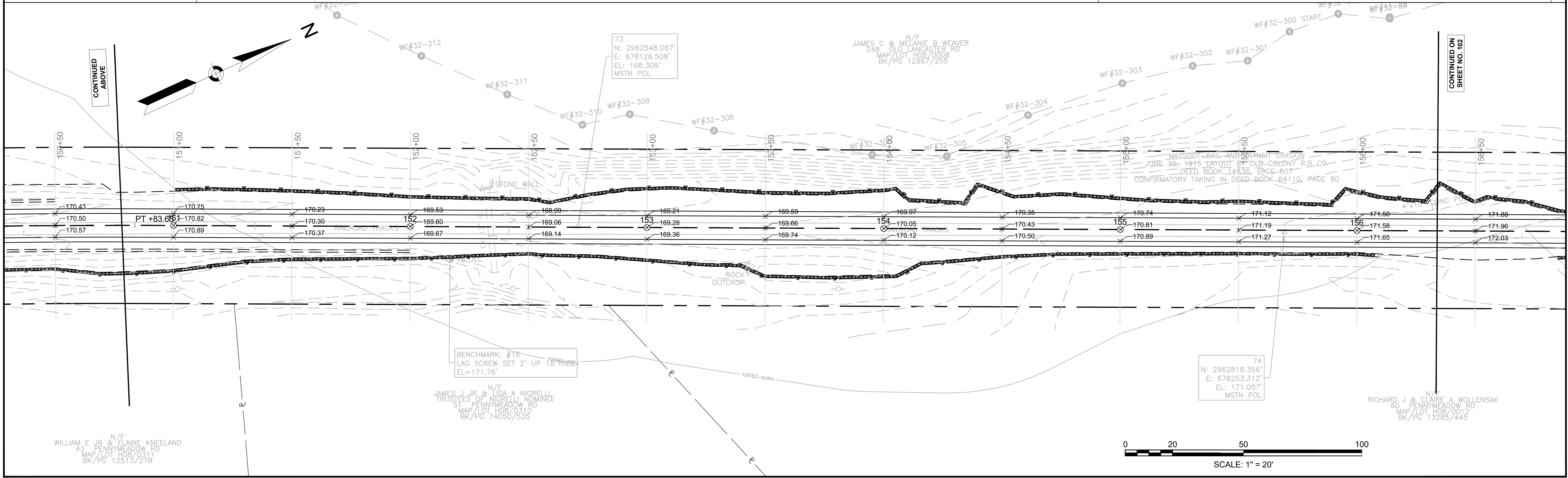
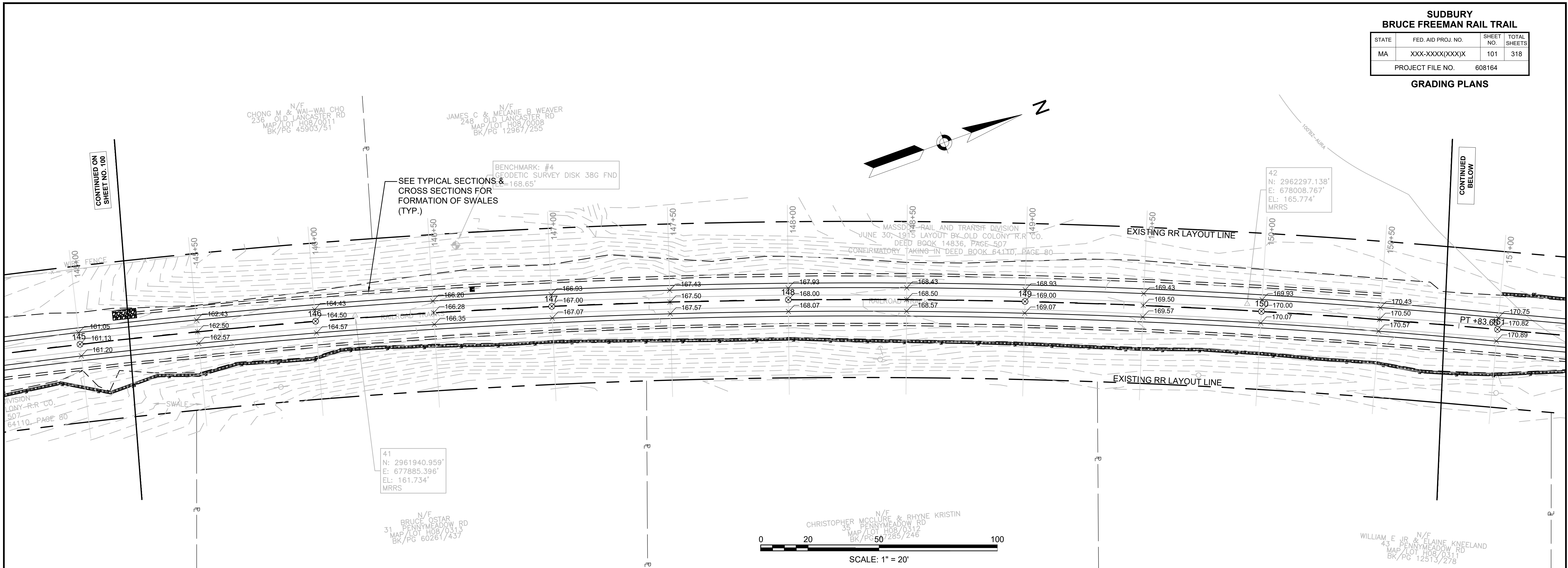
BENCHMARK: #66-11  
 PK FOUND 1' UP IN UP  
 EL=158.55'

2  
 N: 2961684.439'  
 E: 677666.655'  
 EL: 156.229'  
 MMAG

27  
 N: 2961298.692'  
 E: 677768.067'  
 EL: 154.519'  
 MIPE

BENCHMARK: #15  
 LAG NAIL SET 2' UP 10" TREE  
 EL=158.72'

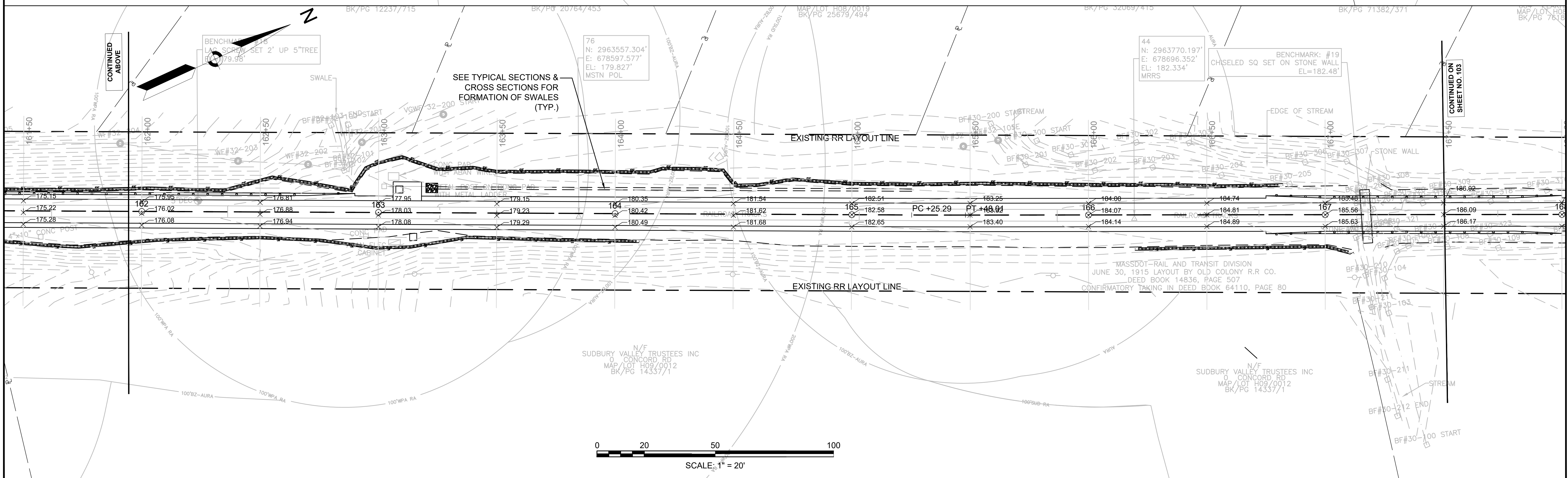
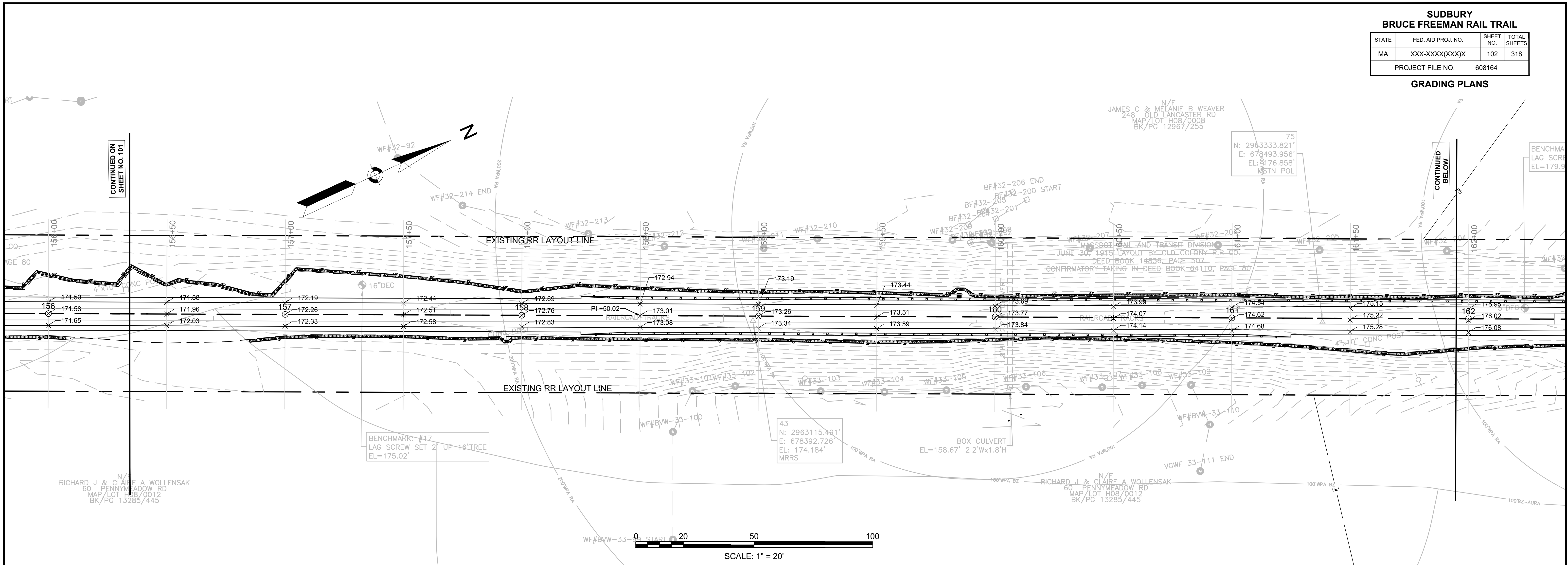
3  
 N: 2961644.458'  
 E: 677817.347'  
 EL: 159.339'  
 MMAG



**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	102	318
PROJECT FILE NO.		608164	

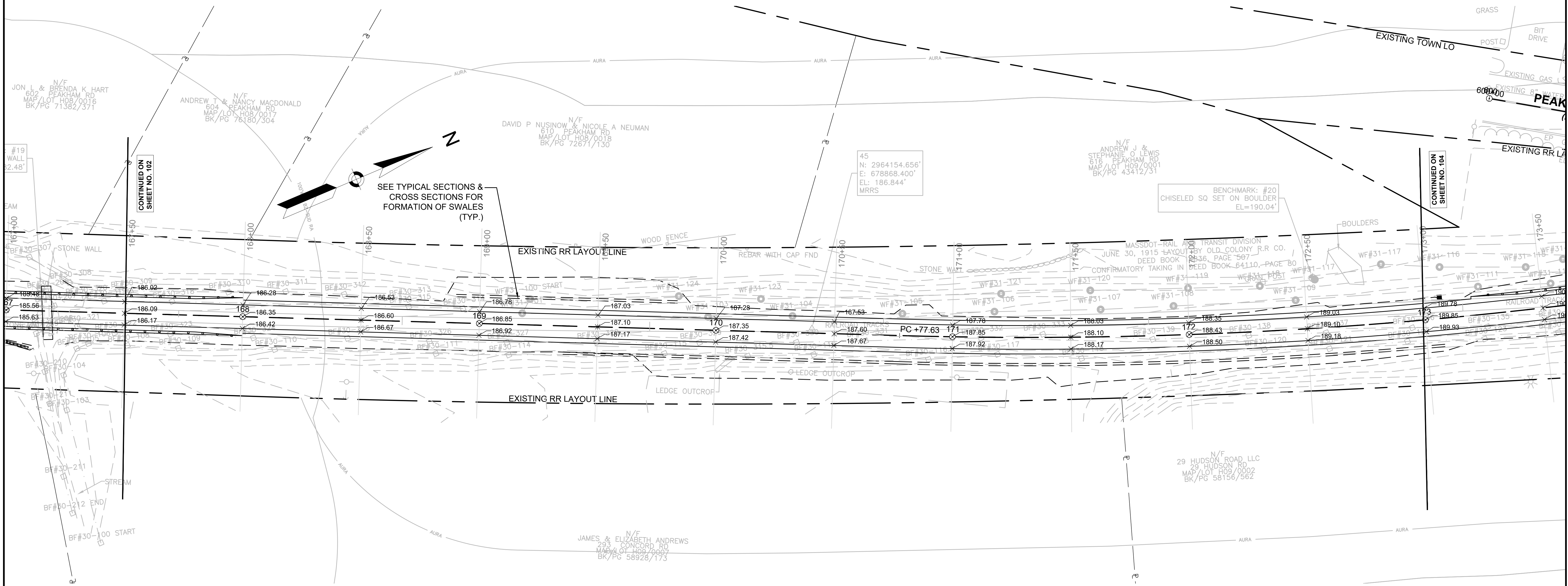
**GRADING PLANS**



**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	103	318
PROJECT FILE NO.		608164	

**GRADING PLANS**



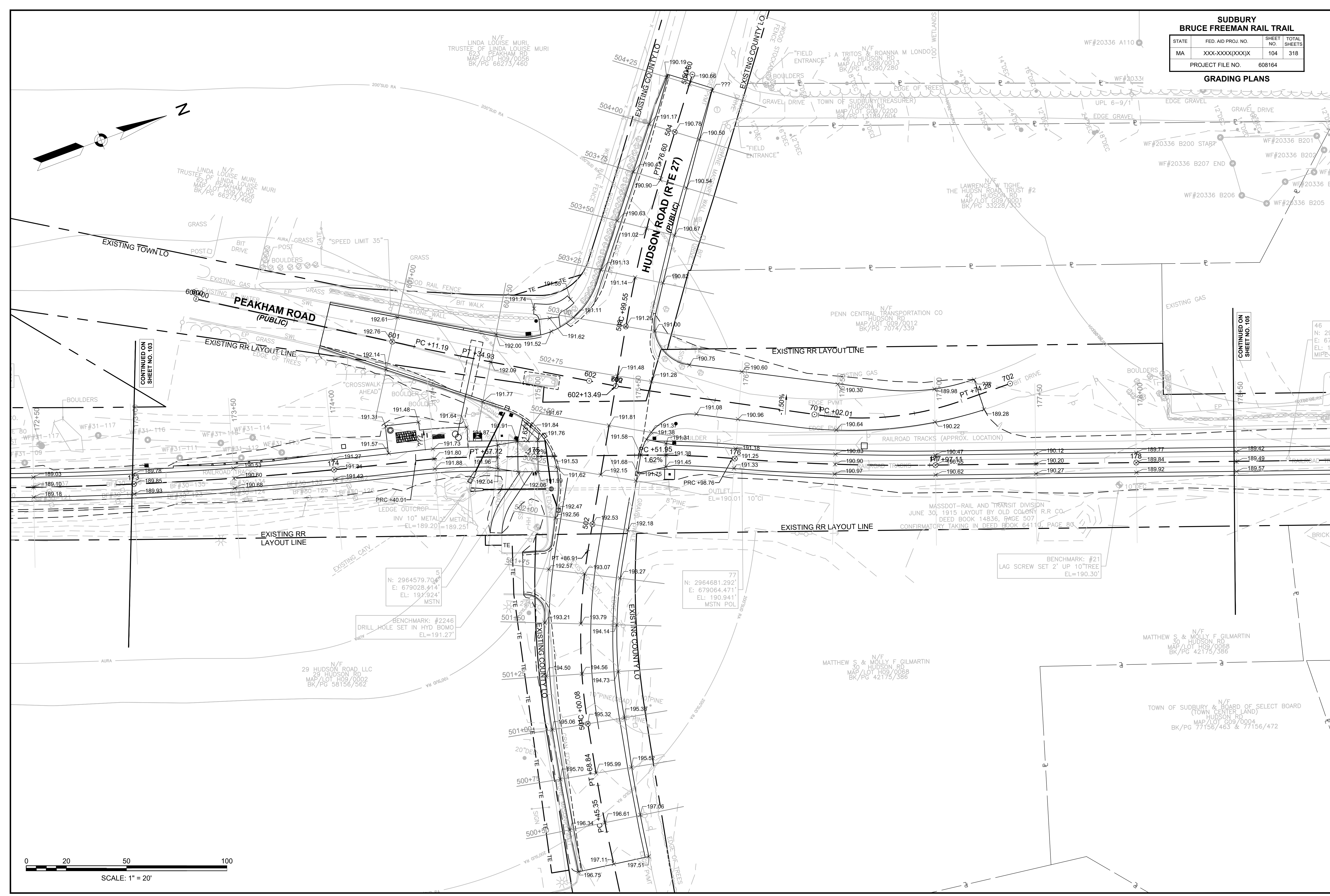
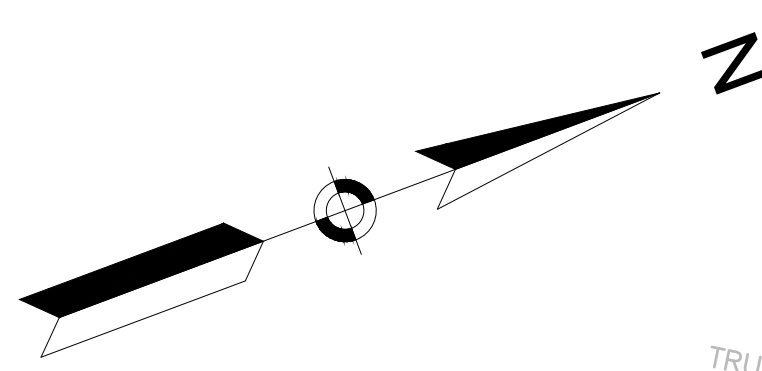
CONTINUED ON SHEET NO. 102

CONTINUED ON SHEET NO. 104

**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

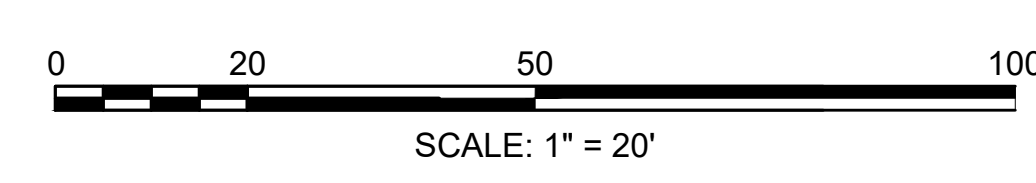
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	104	318
PROJECT FILE NO.		608164	

**GRADING PLANS**



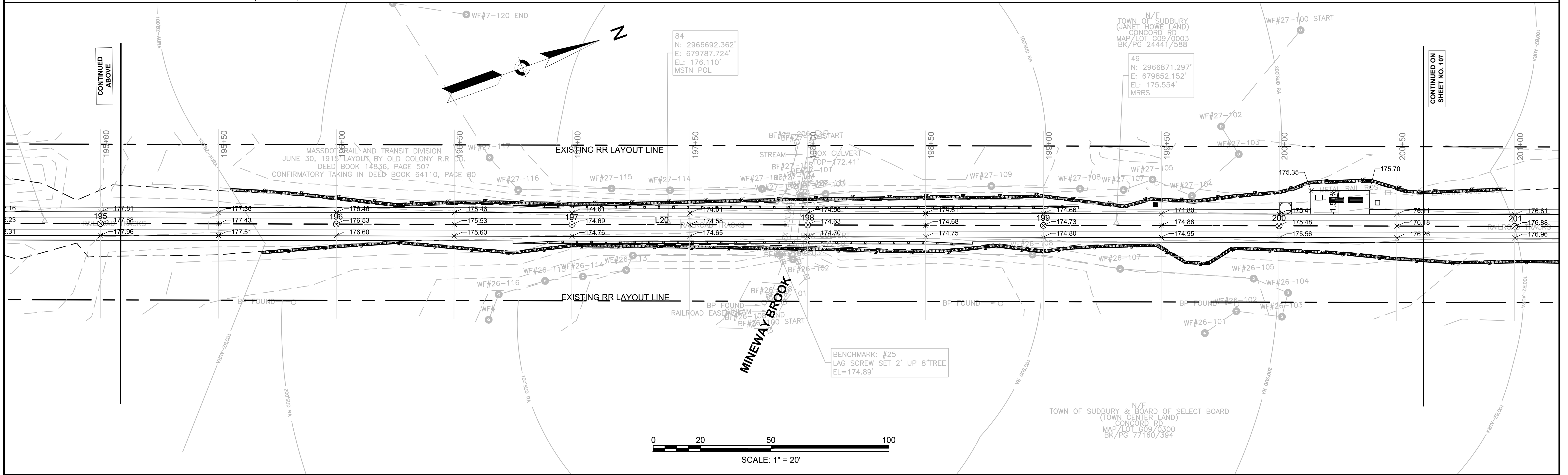
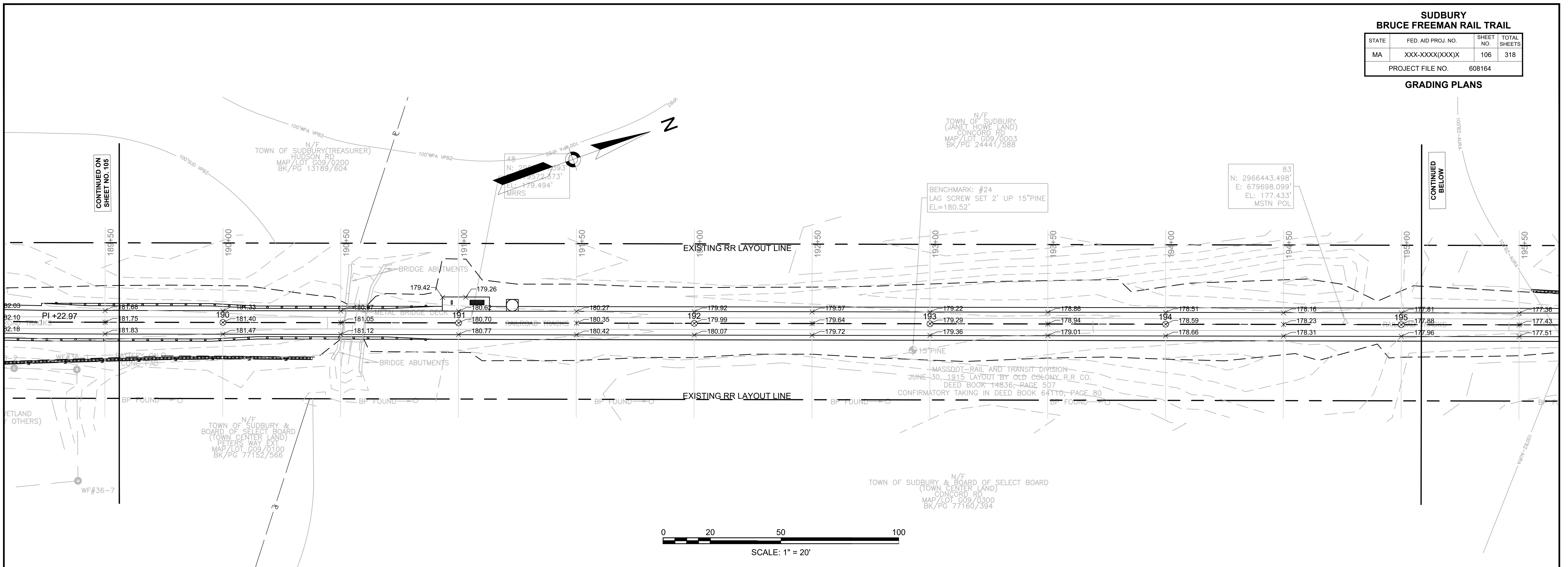
CONTINUED ON  
SHEET NO. 103

CONTINUED ON  
SHEET NO. 105







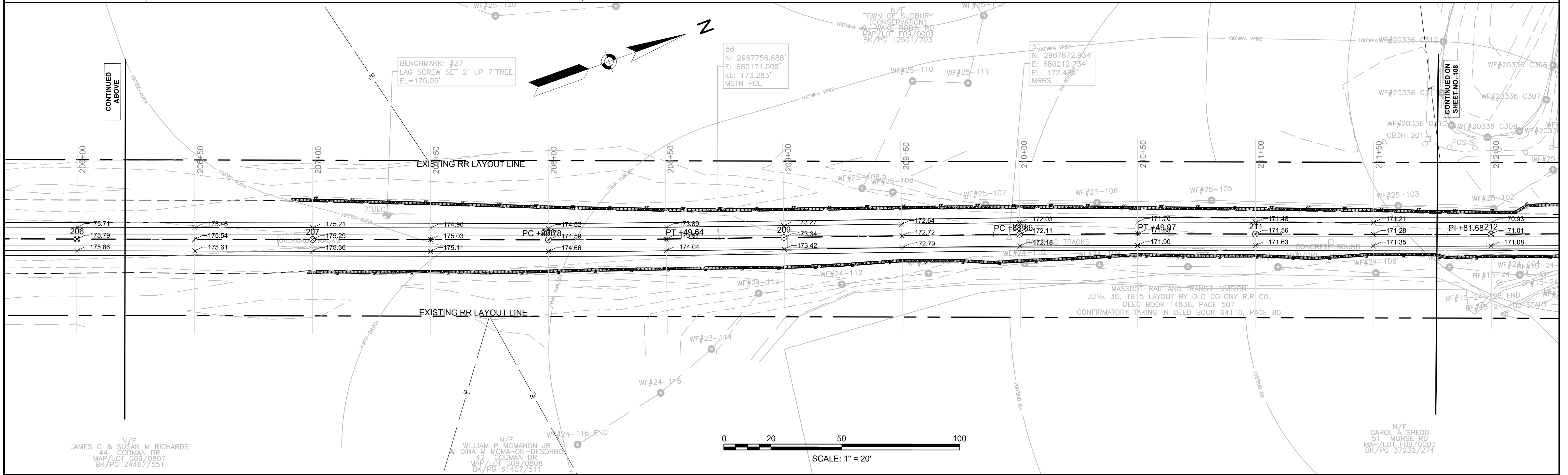
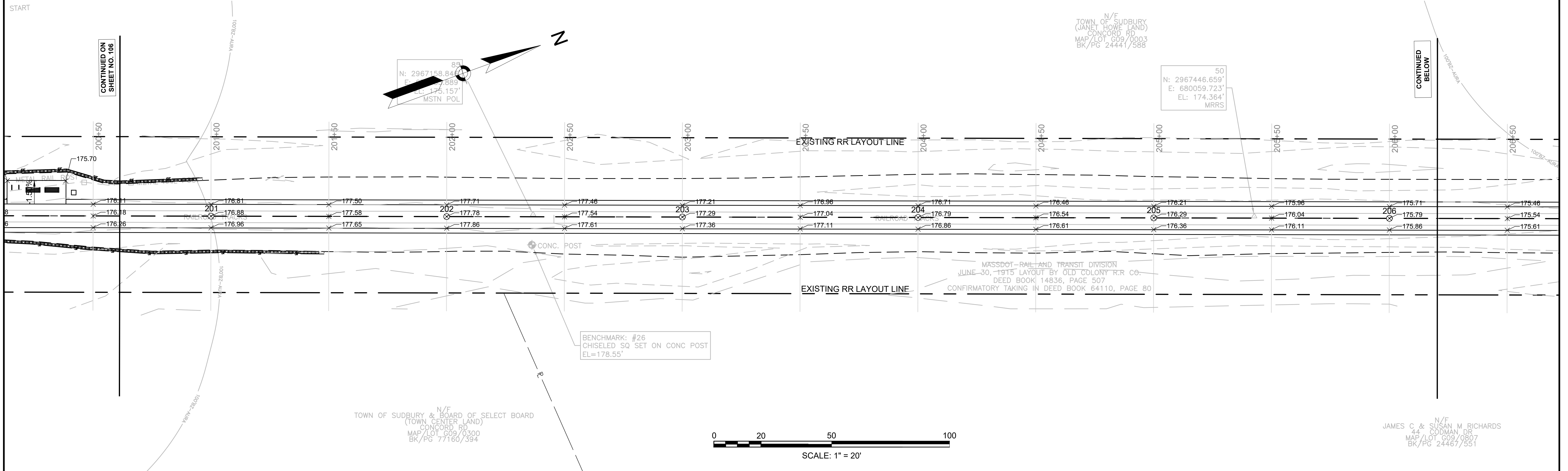


**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

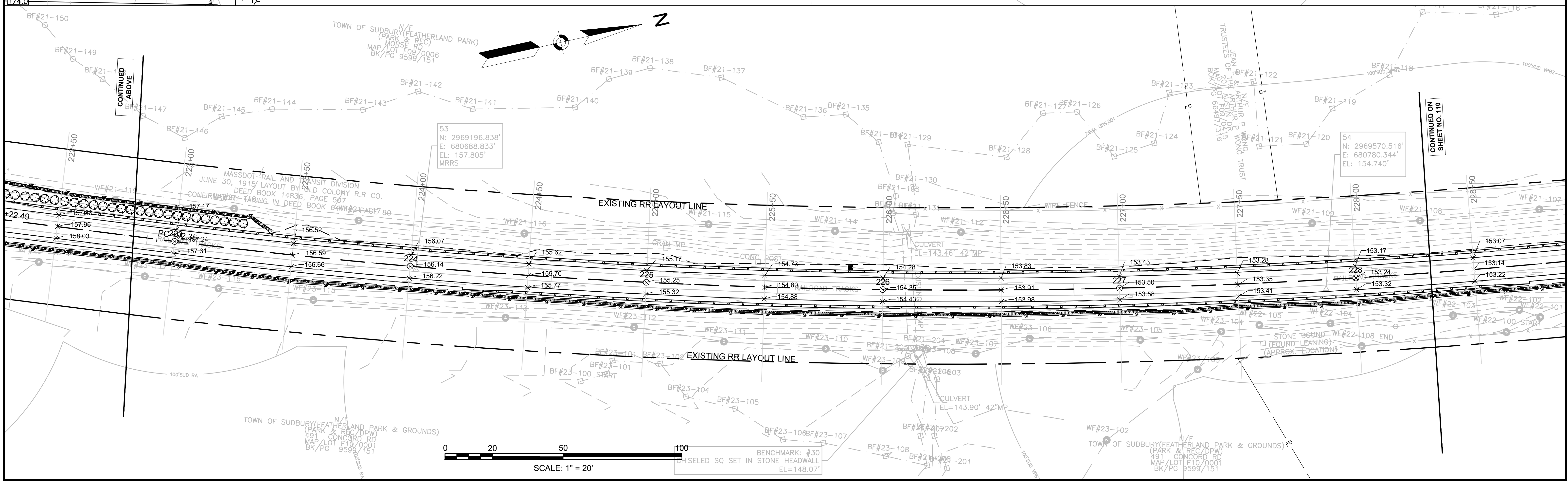
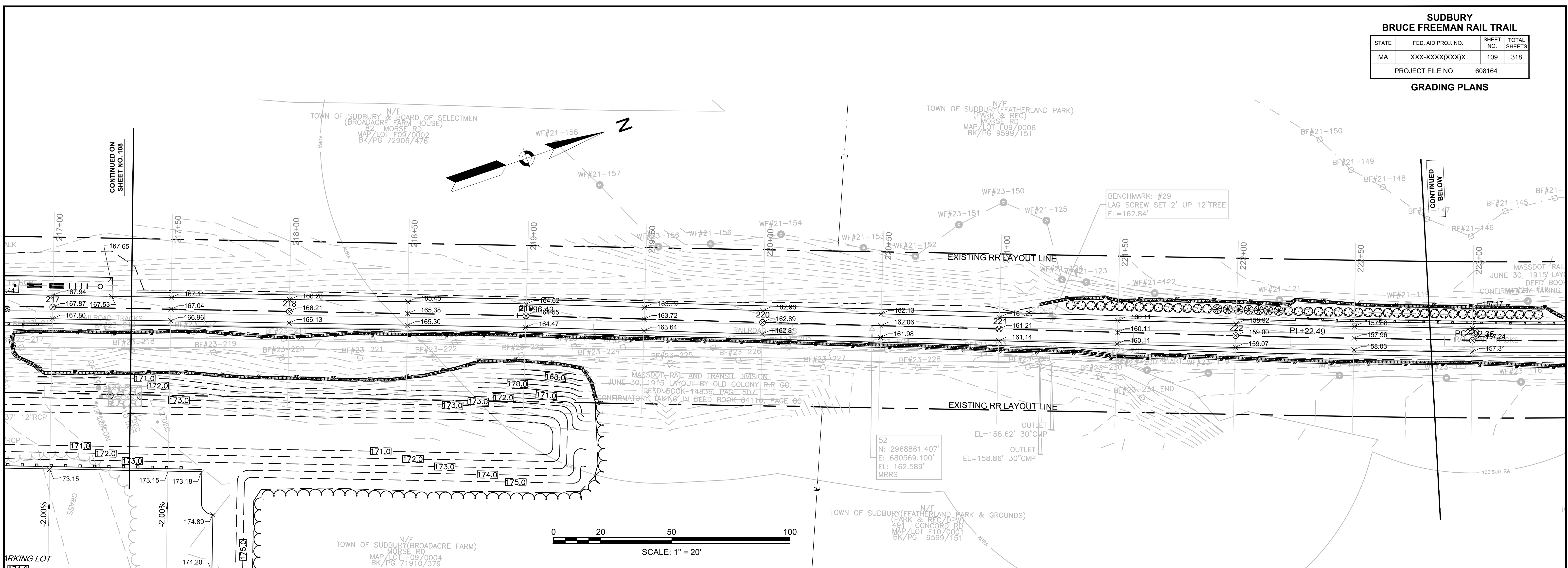
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	107	318

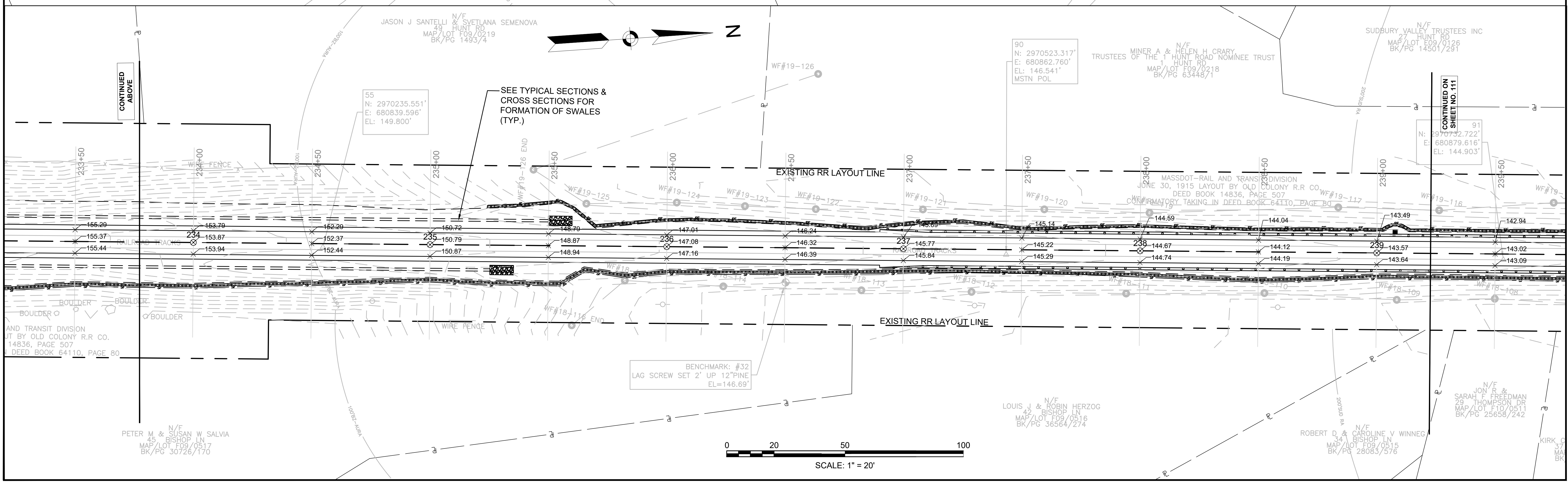
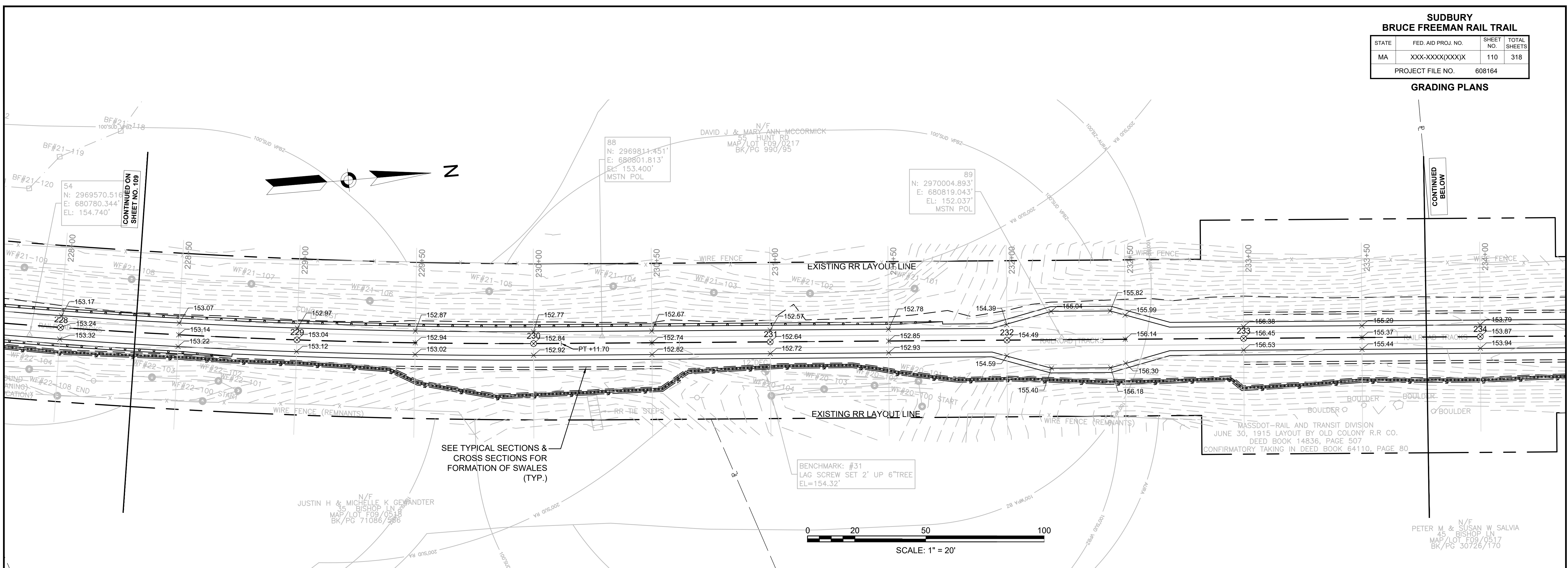
PROJECT FILE NO. 608164

**GRADING PLANS**



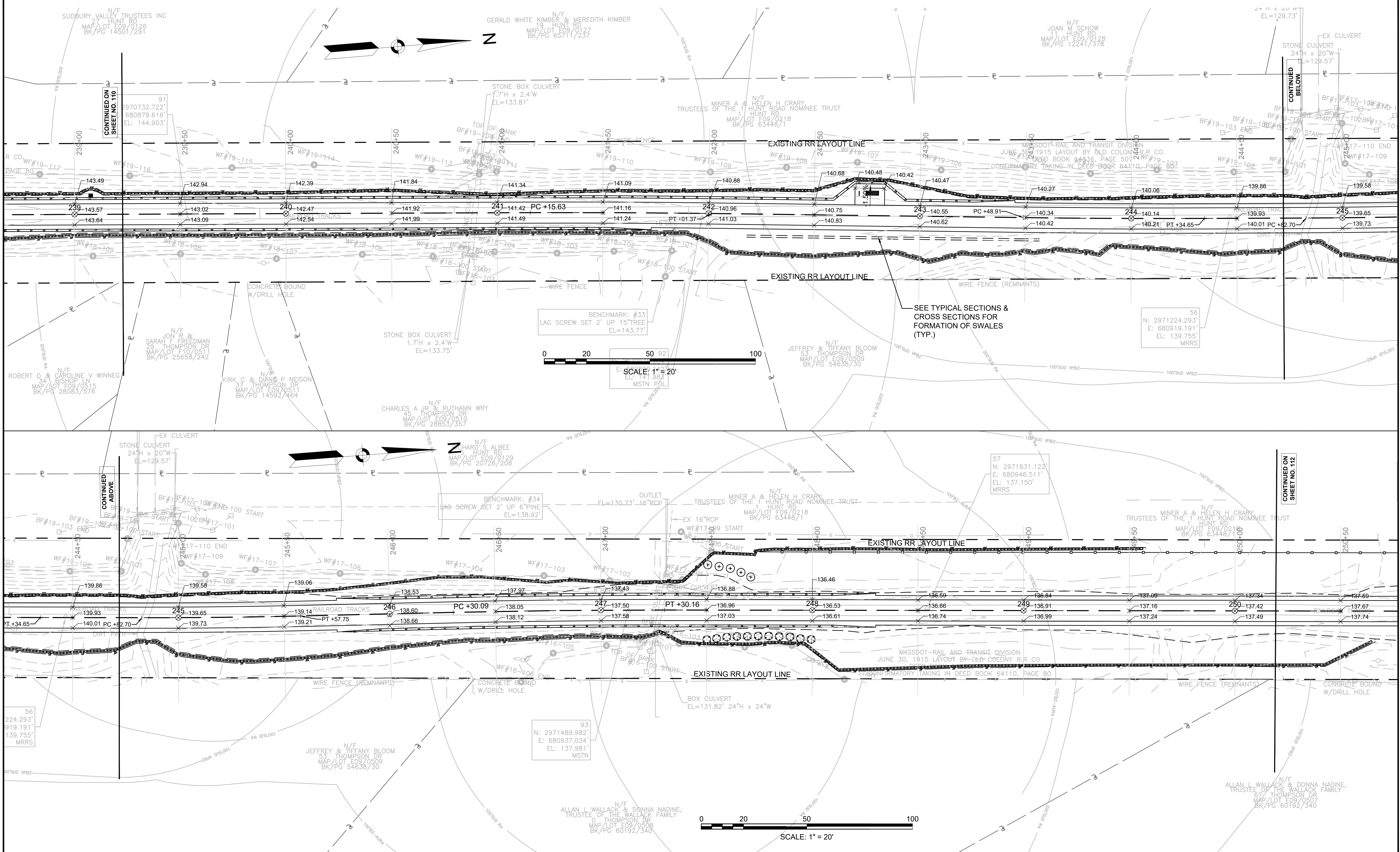


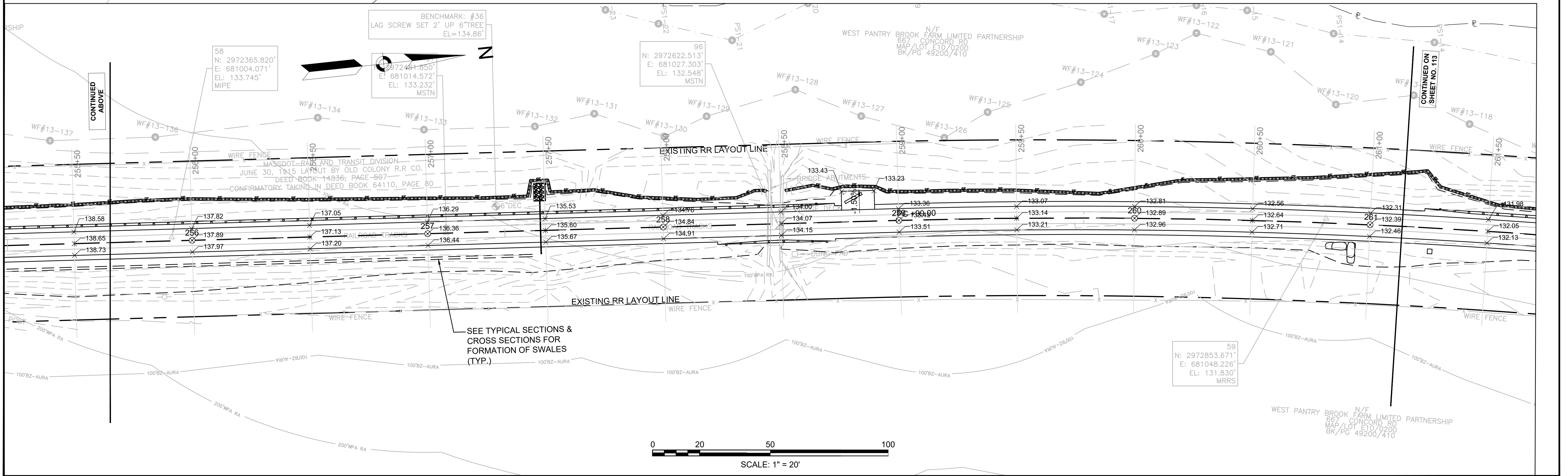
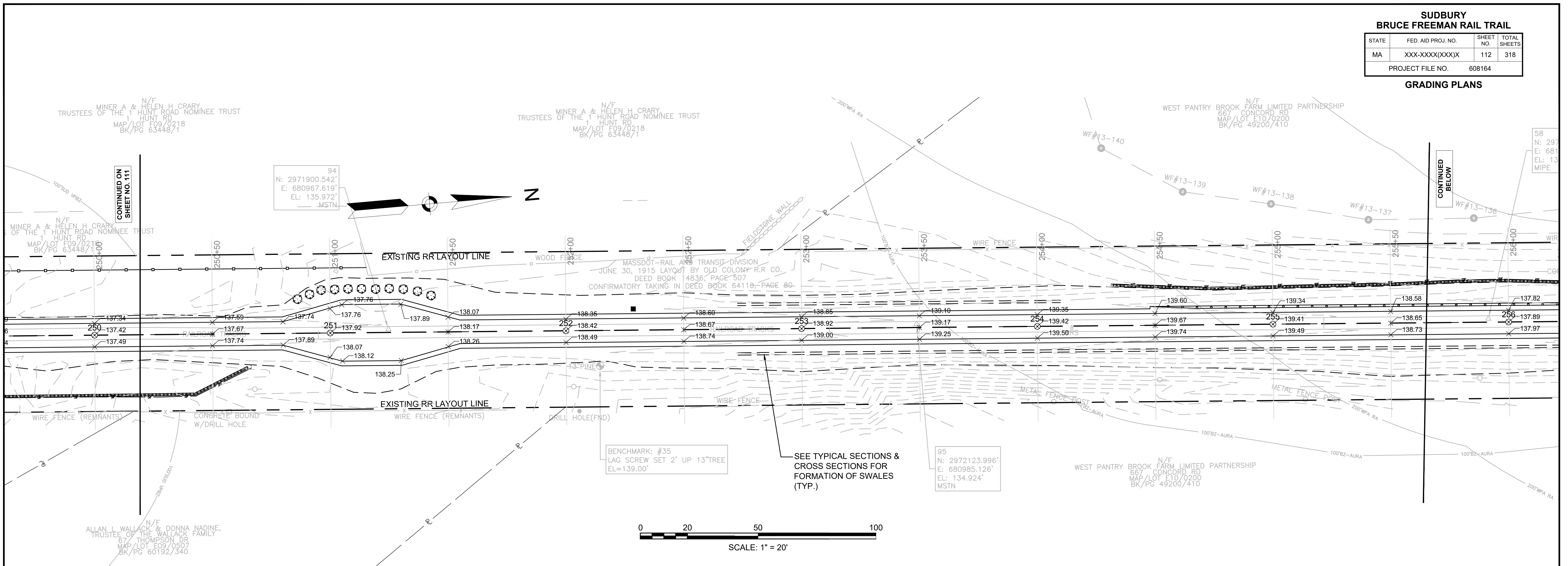




SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXX(XXX)X	111	318
PROJECT FILE NO.		608164	

**GRADING PLANS**



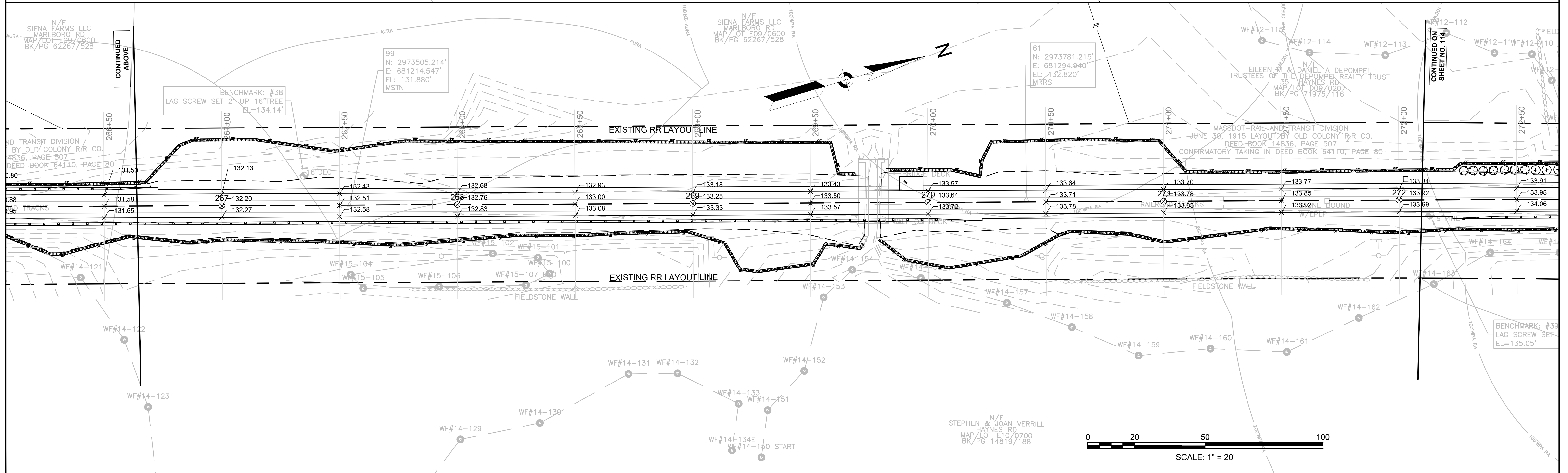
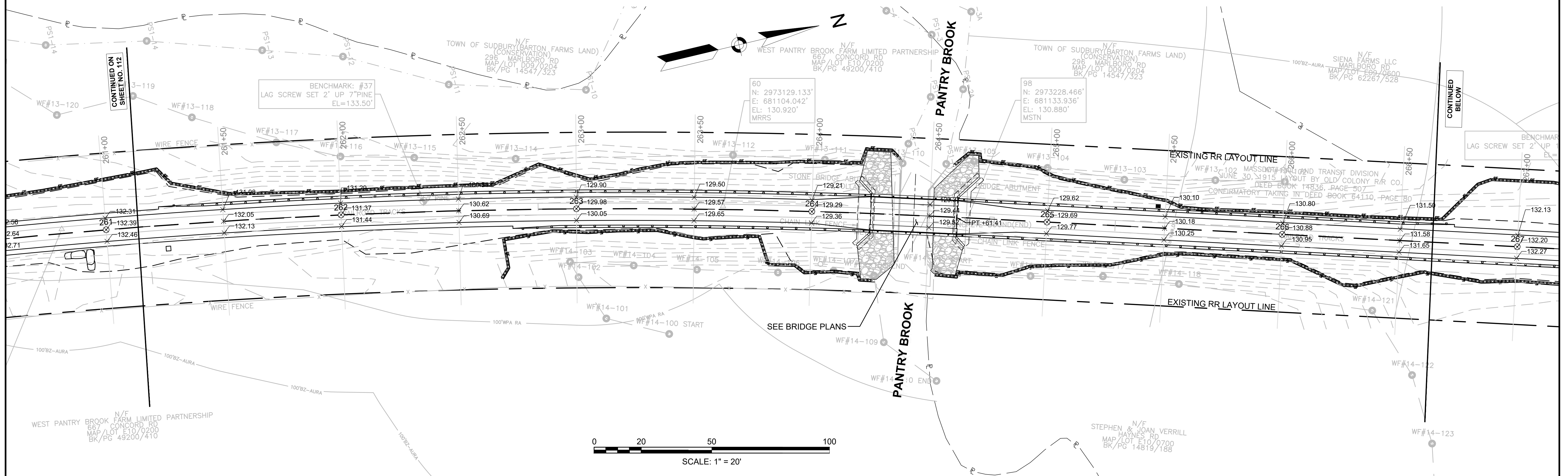




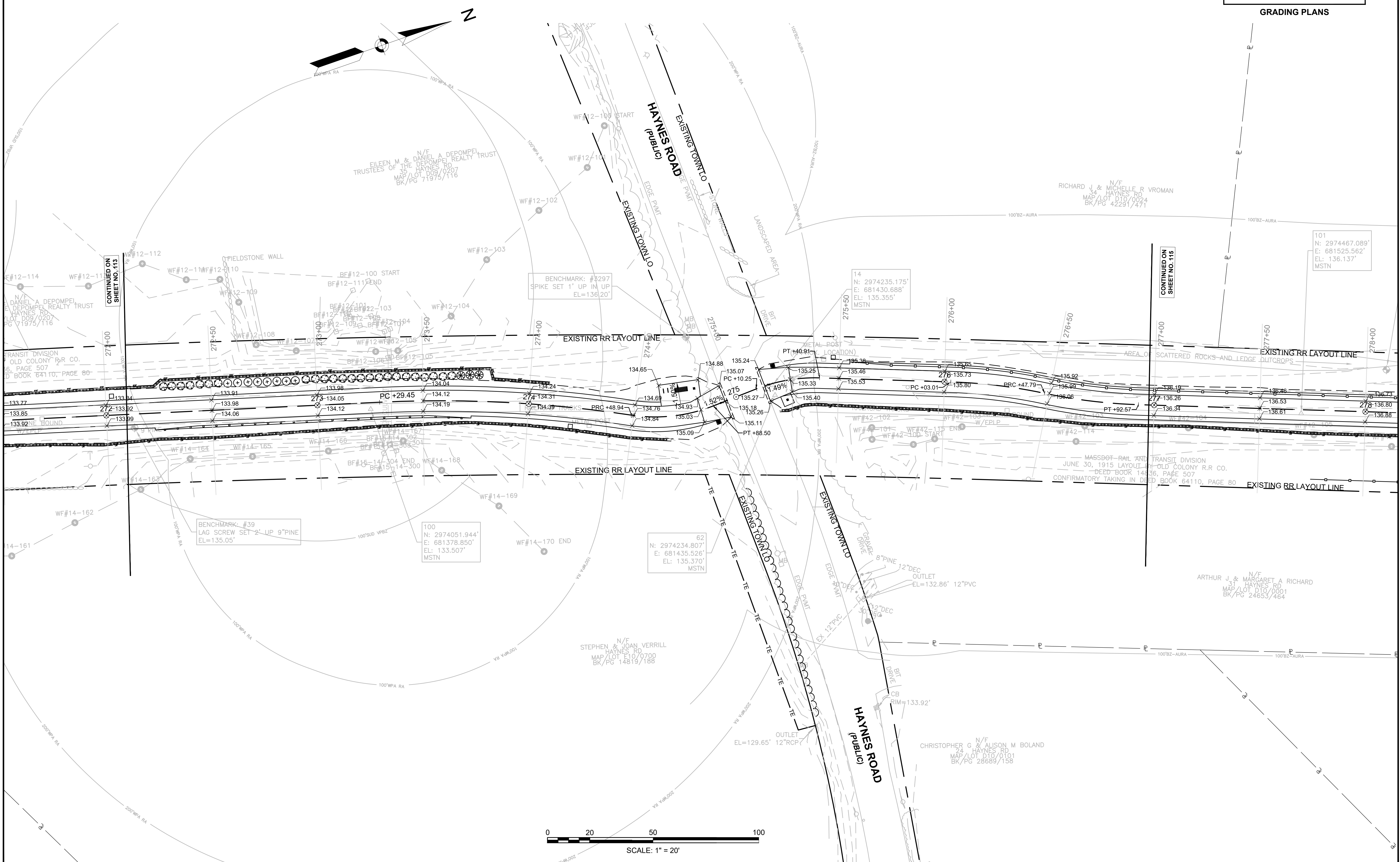
**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	113	318
PROJECT FILE NO.		608164	

**GRADING PLANS**

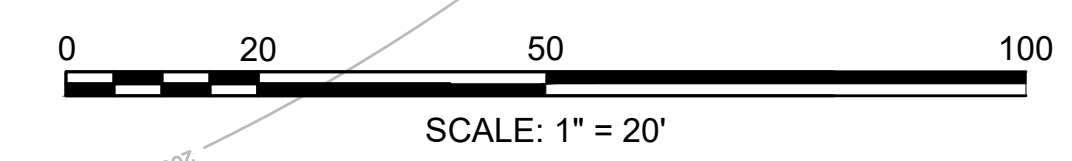


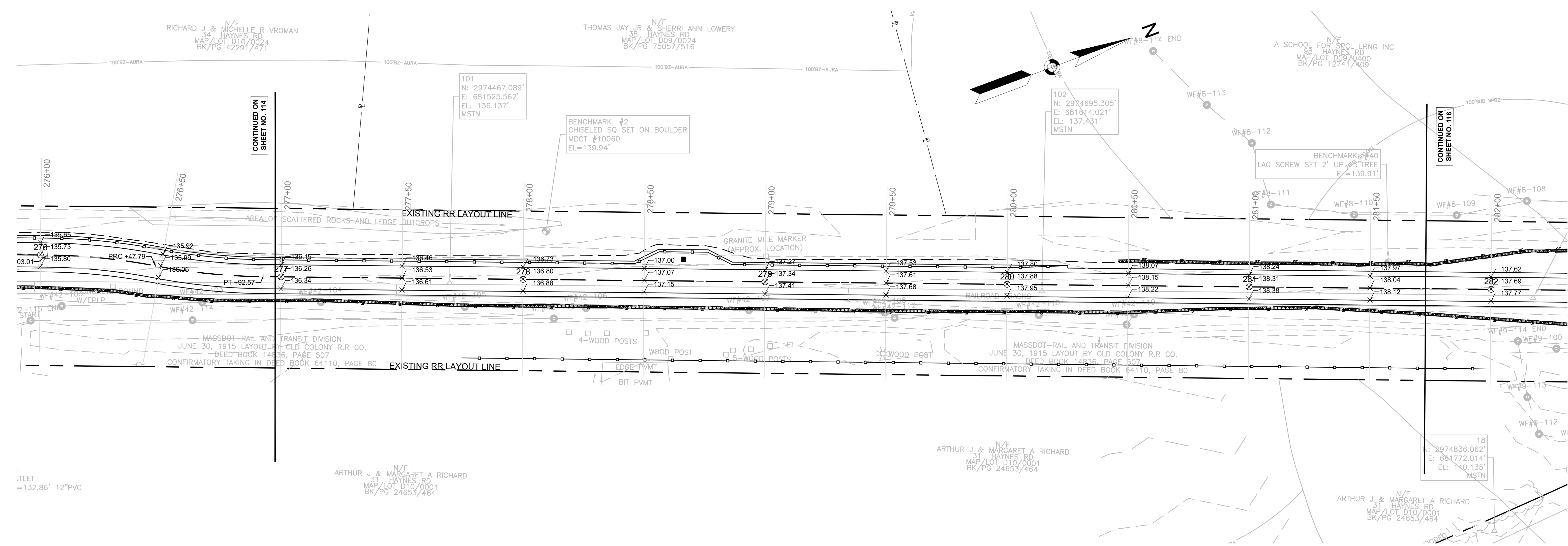
608164\_HD(Grading Plans).DWG Plotted on 12-May-2021 10:31 AM



CONTINUED ON SHEET NO. 113

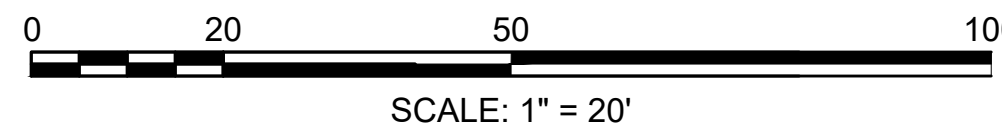
CONTINUED ON SHEET NO. 115





CONTINUED ON  
SHEET NO. 114

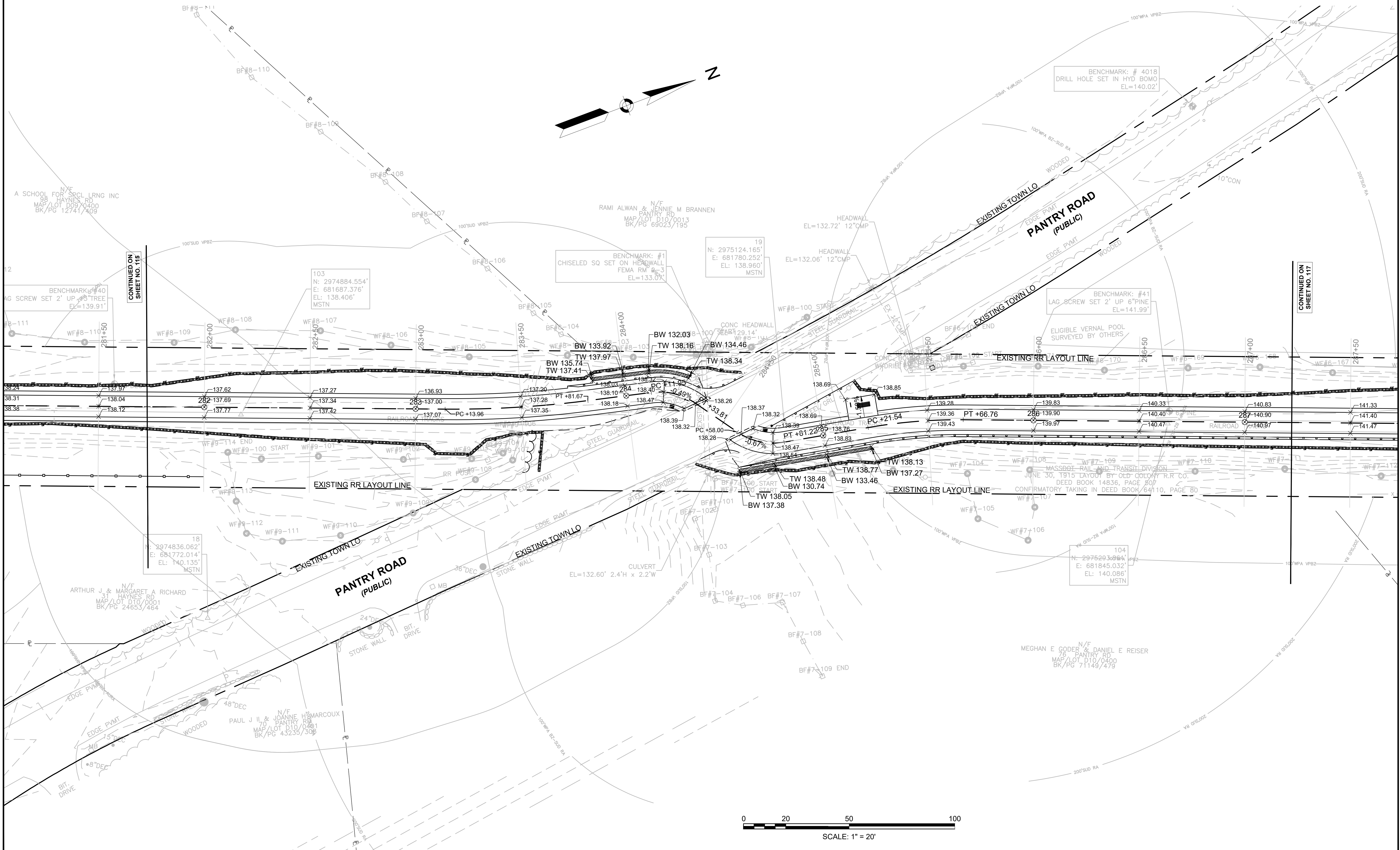
CONTINUED ON  
SHEET NO. 116



**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

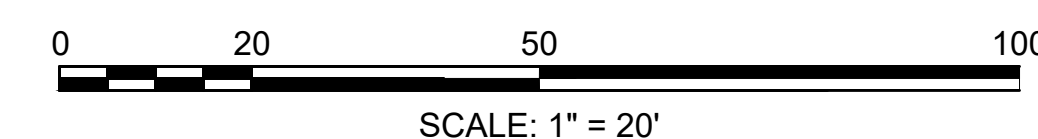
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	116	318
PROJECT FILE NO.		608164	

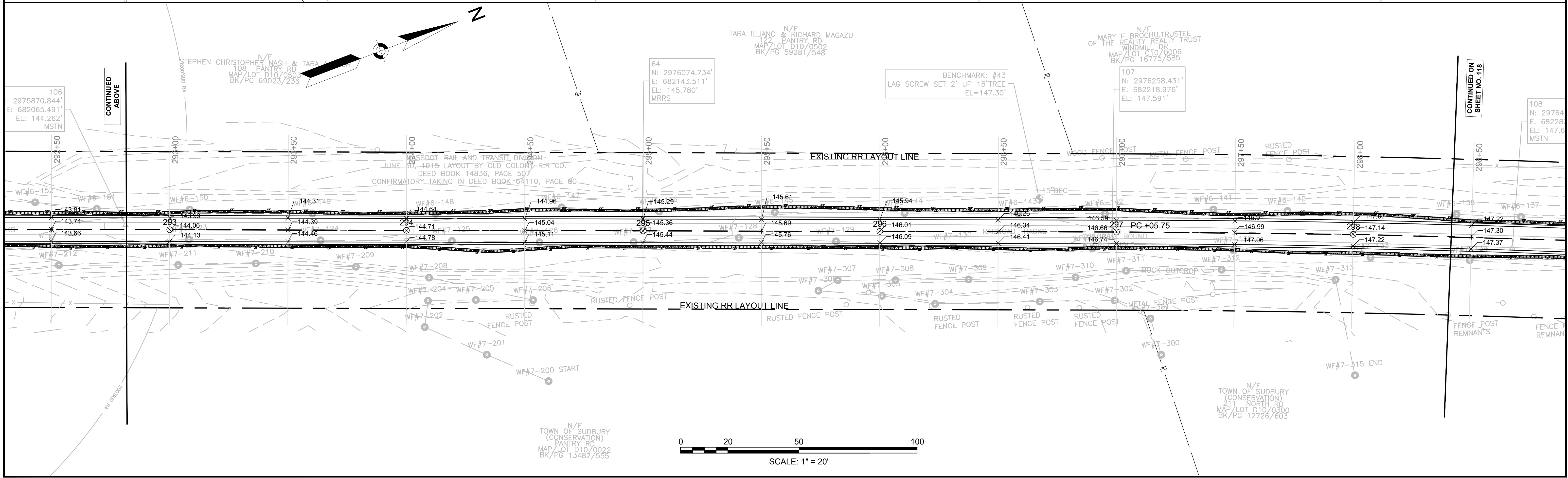
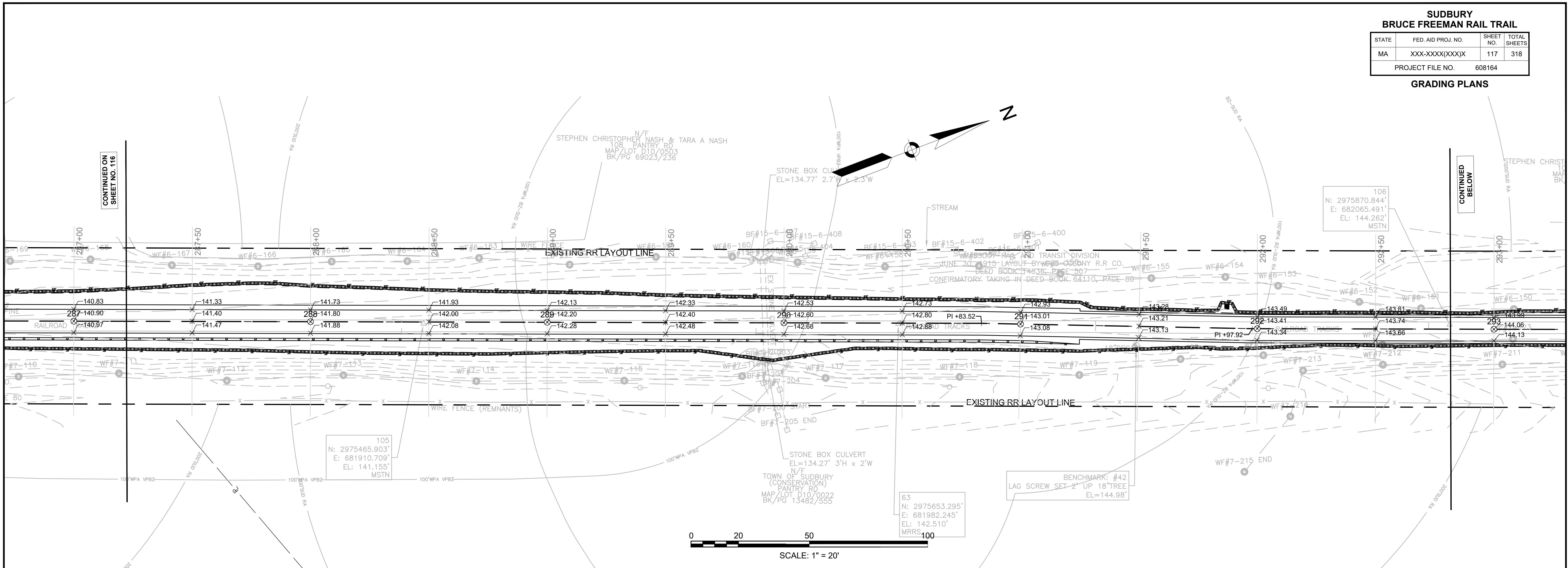
**GRADING PLANS**



CONTINUED ON  
SHEET NO. 115

CONTINUED ON  
SHEET NO. 117





CONTINUED ON  
SHEET NO. 116

CONTINUED  
BELOW

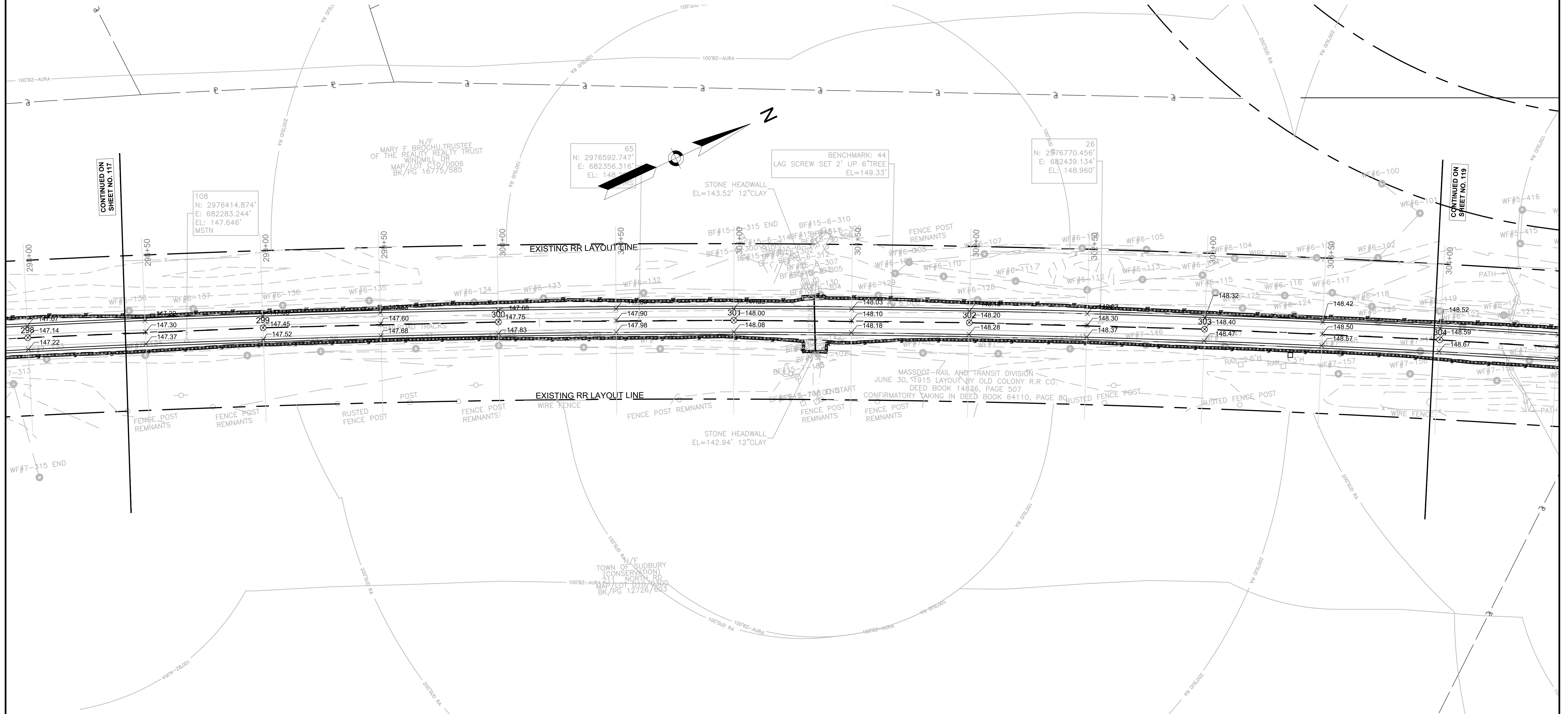
CONTINUED  
ABOVE

CONTINUED ON  
SHEET NO. 118

**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

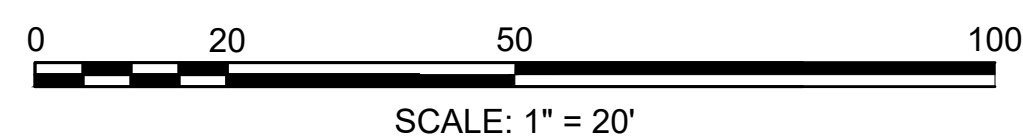
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	118	318
PROJECT FILE NO.		608164	

**GRADING PLANS**



CONTINUED ON SHEET NO. 117

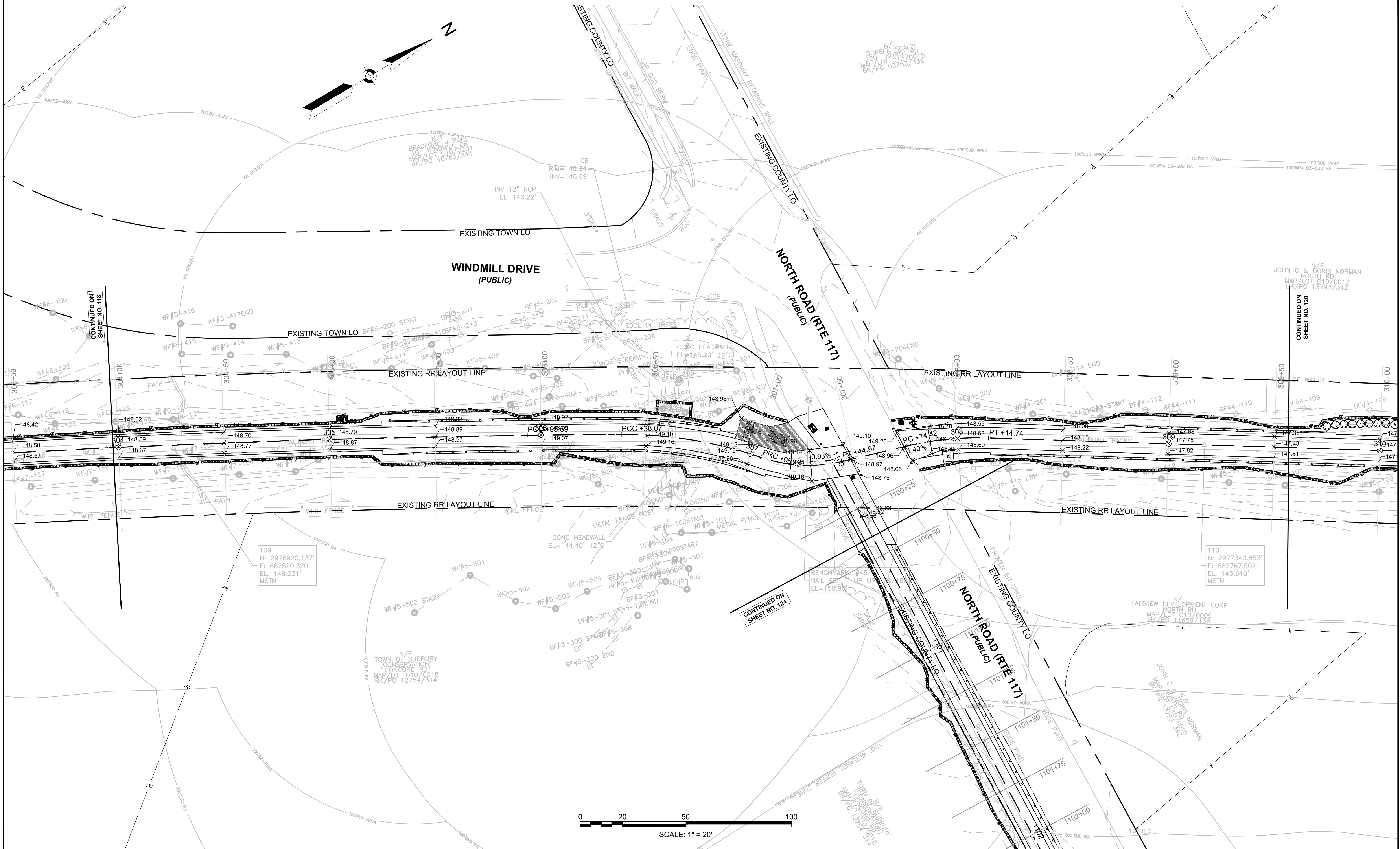
CONTINUED ON SHEET NO. 119

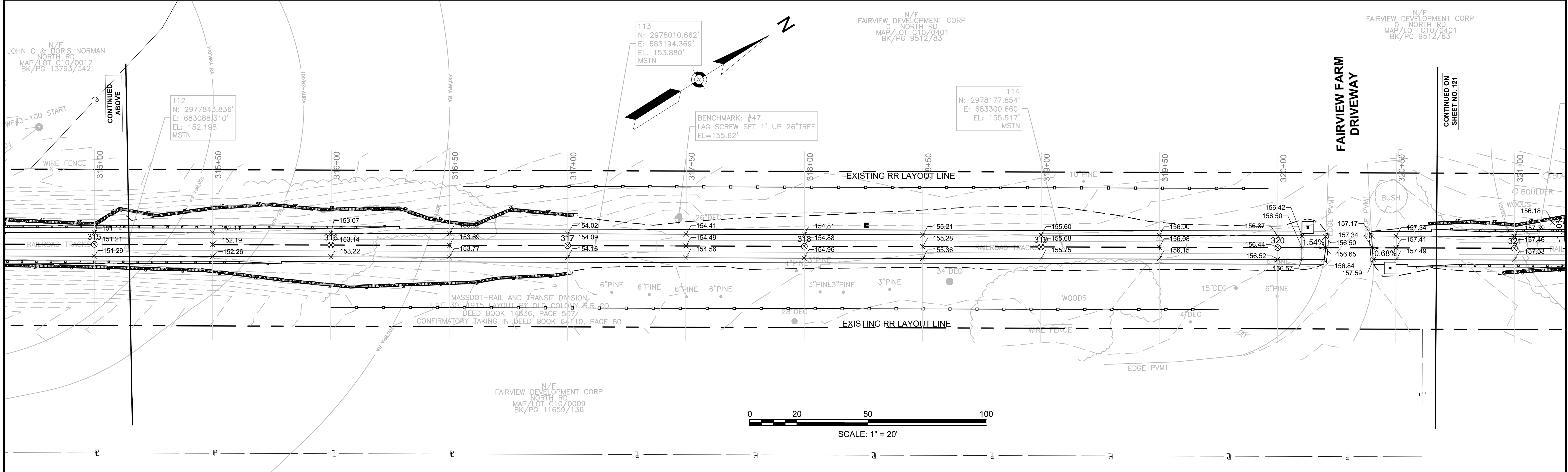
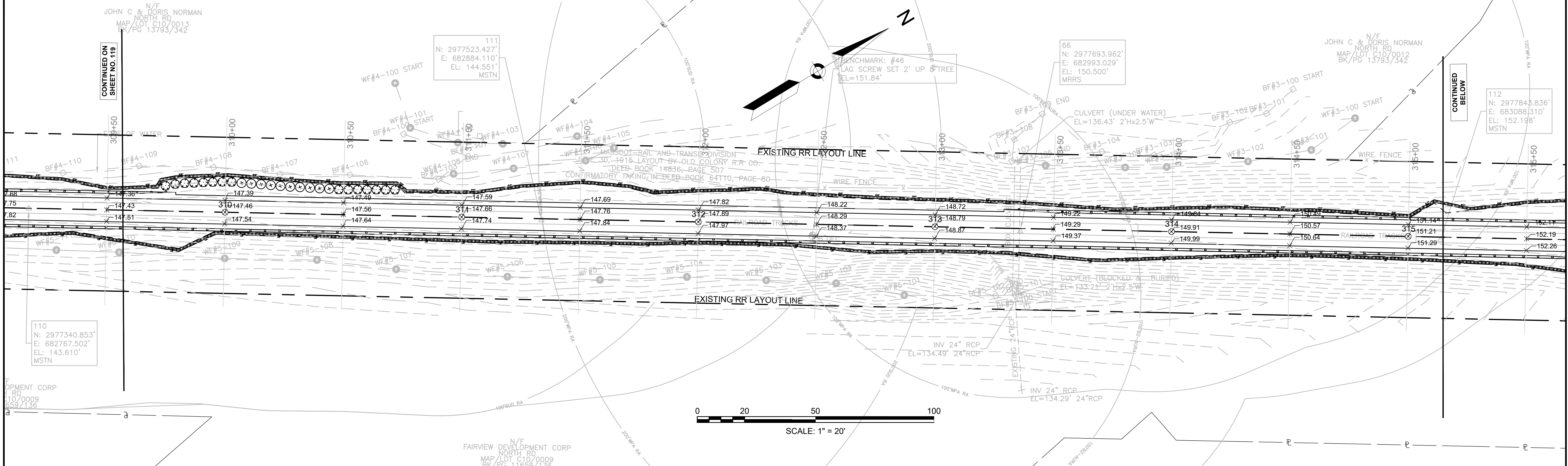


**SUDBURY**  
**BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	119	318
PROJECT FILE NO.		608164	

**GRADING PLANS**

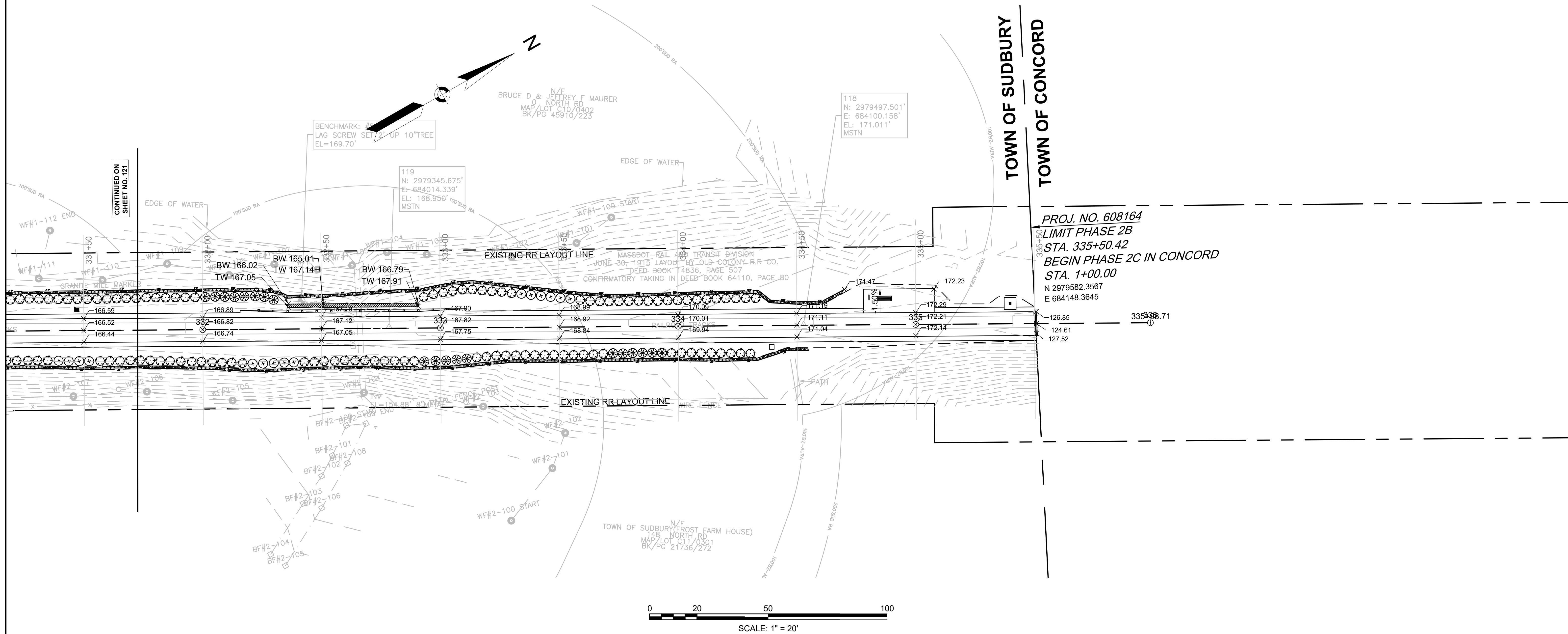


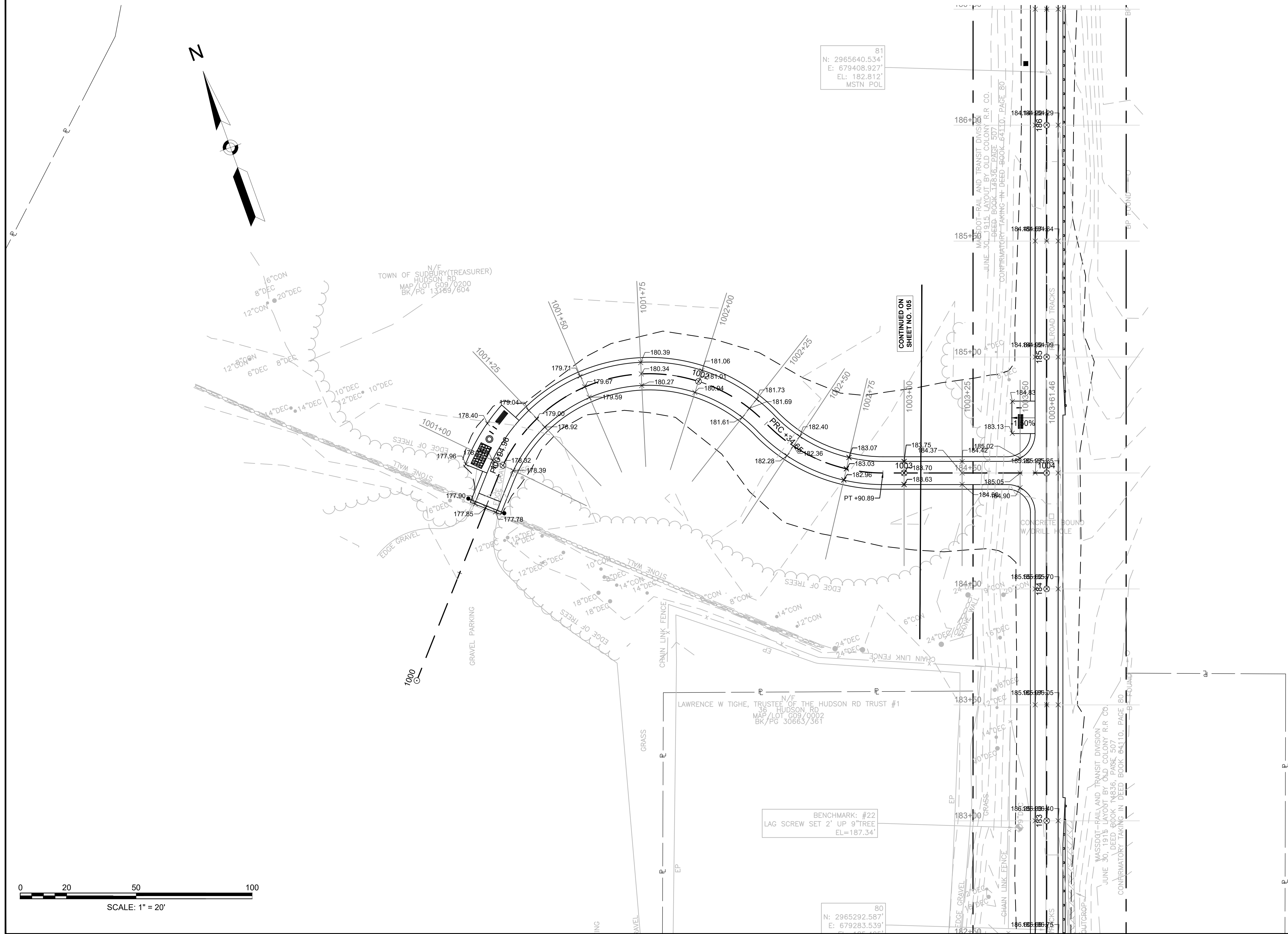






SUDBURY BRUCE FREEMAN RAIL TRAIL			
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	122	318
PROJECT FILE NO.		608164	
GRADING PLANS			

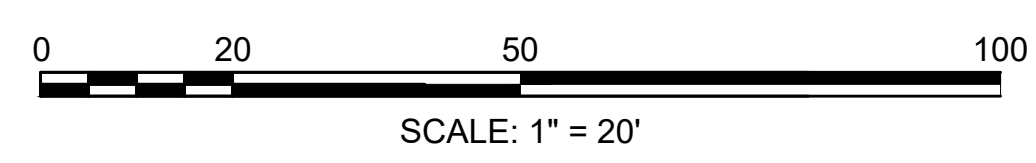




81  
 N: 2965640.534'  
 E: 679408.927'  
 EL: 182.812'  
 MSTN POL

CONTINUED ON  
 SHEET NO. 105

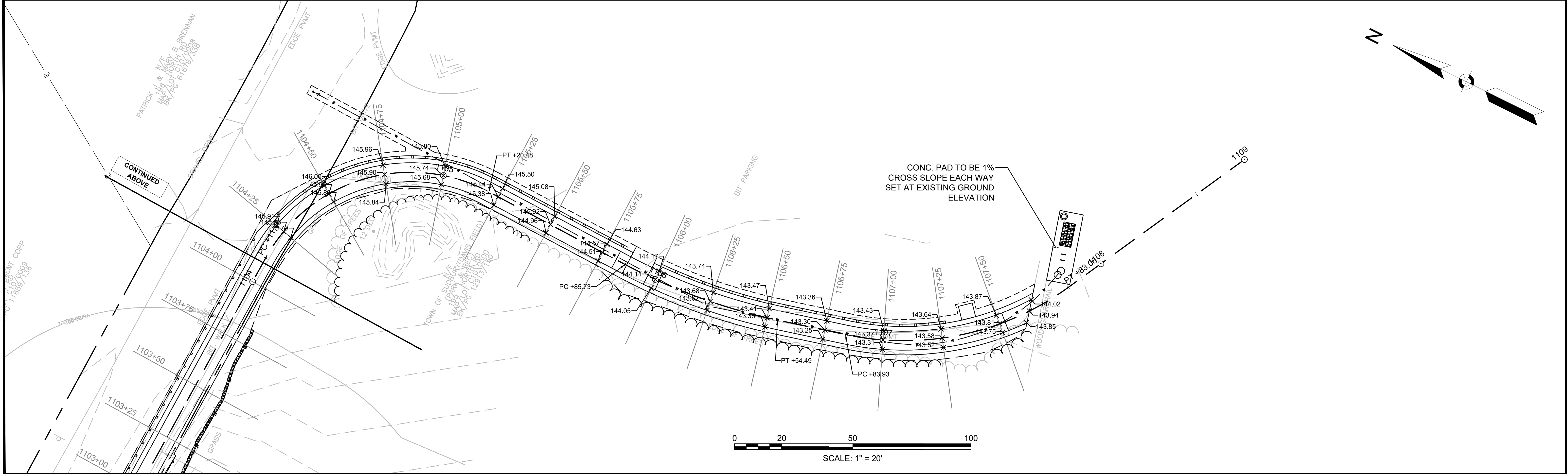
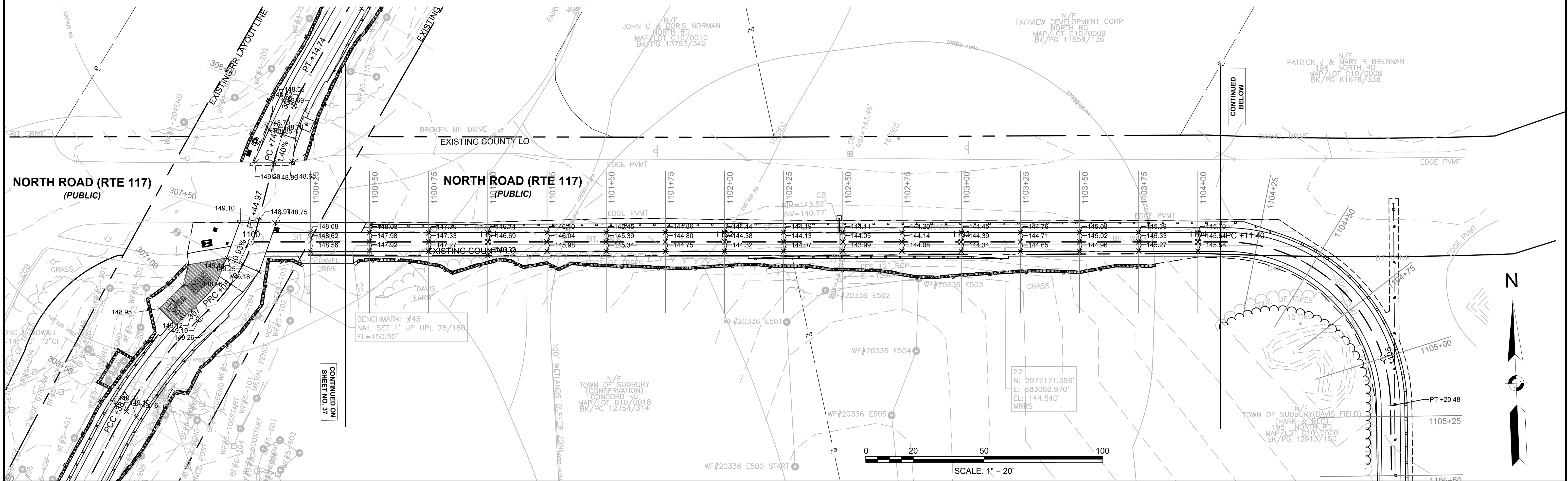
80  
 N: 2965292.587'  
 E: 679283.539'  
 EL: 187.34'



**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

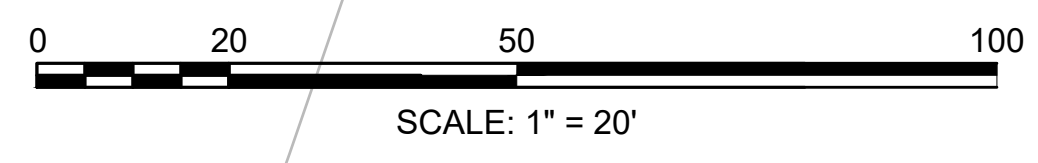
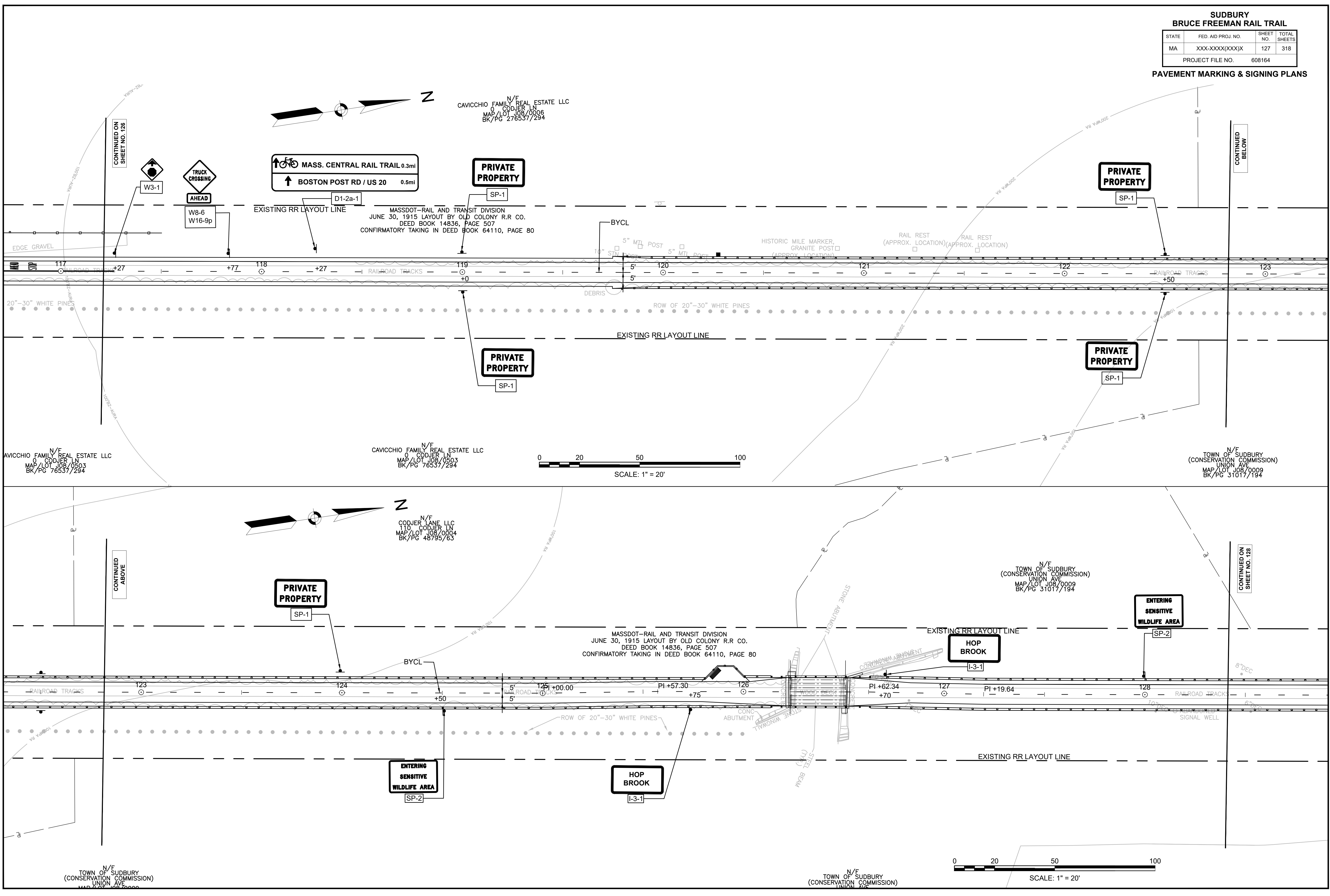
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	124	318
PROJECT FILE NO.		608164	

**GRADING PLANS**





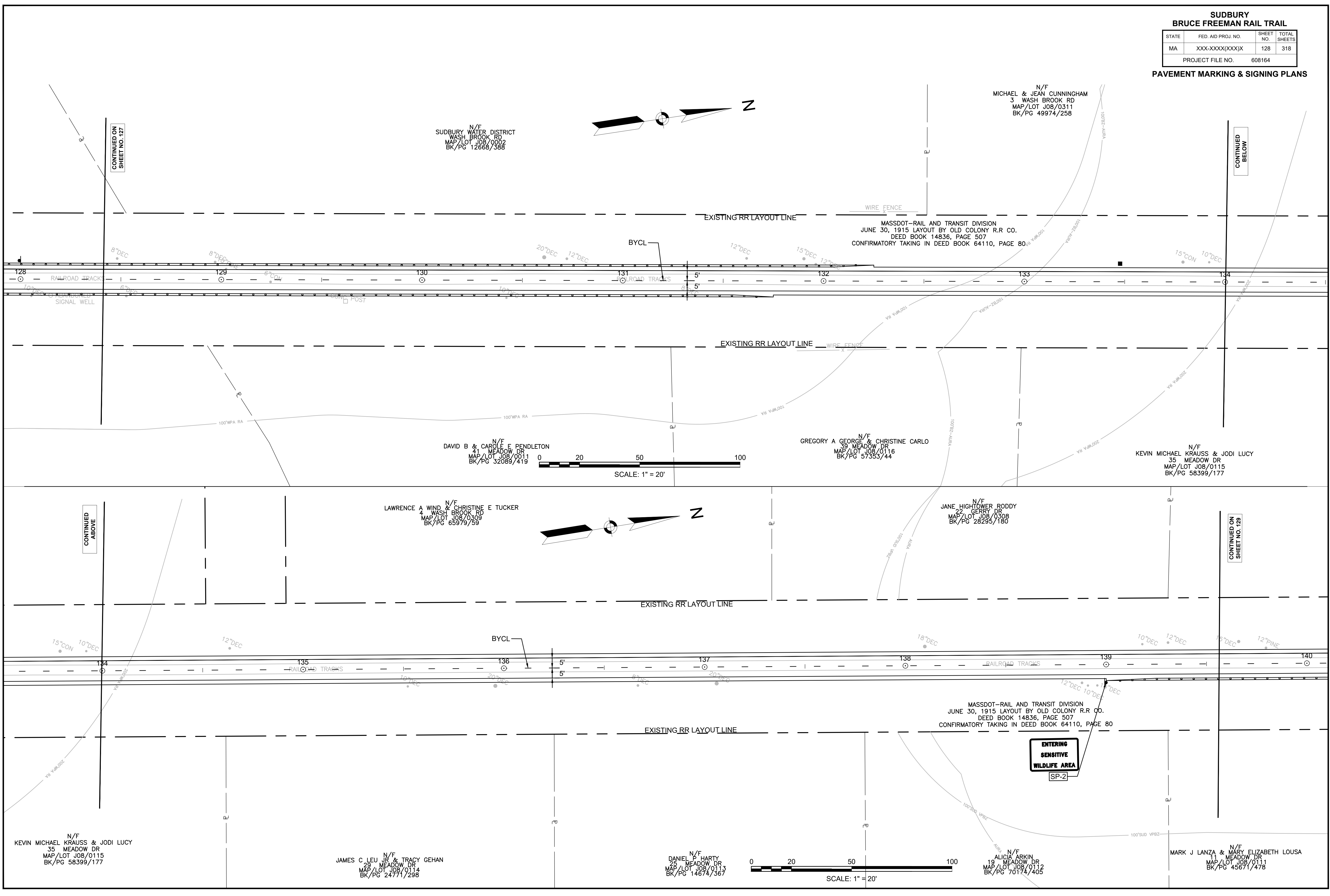




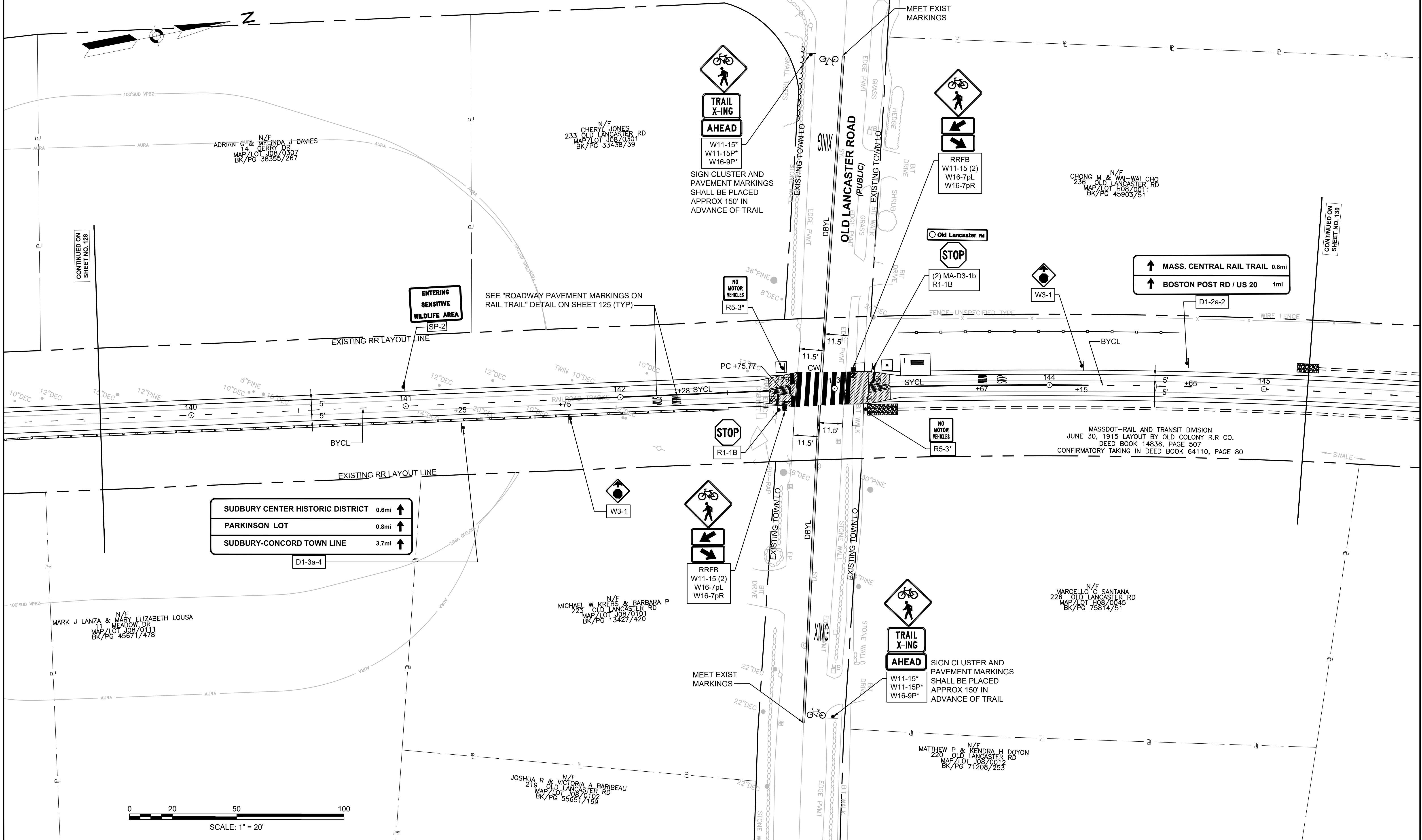
**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	128	318
PROJECT FILE NO.		608164	

**PAVEMENT MARKING & SIGNING PLANS**







CONTINUED ON SHEET NO. 128

CONTINUED ON SHEET NO. 130

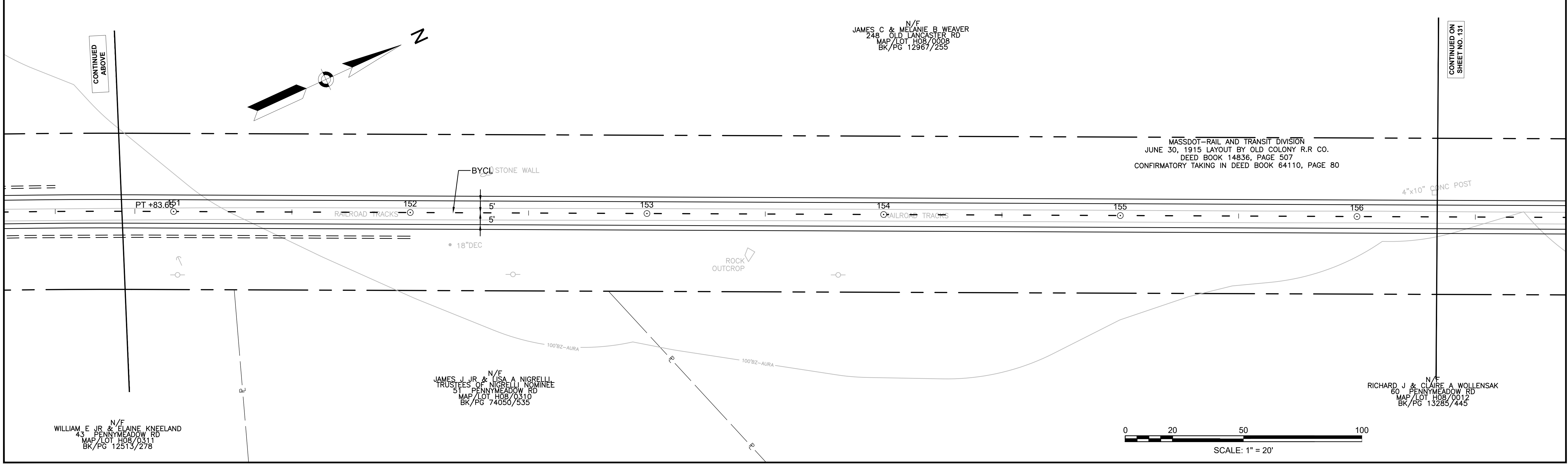
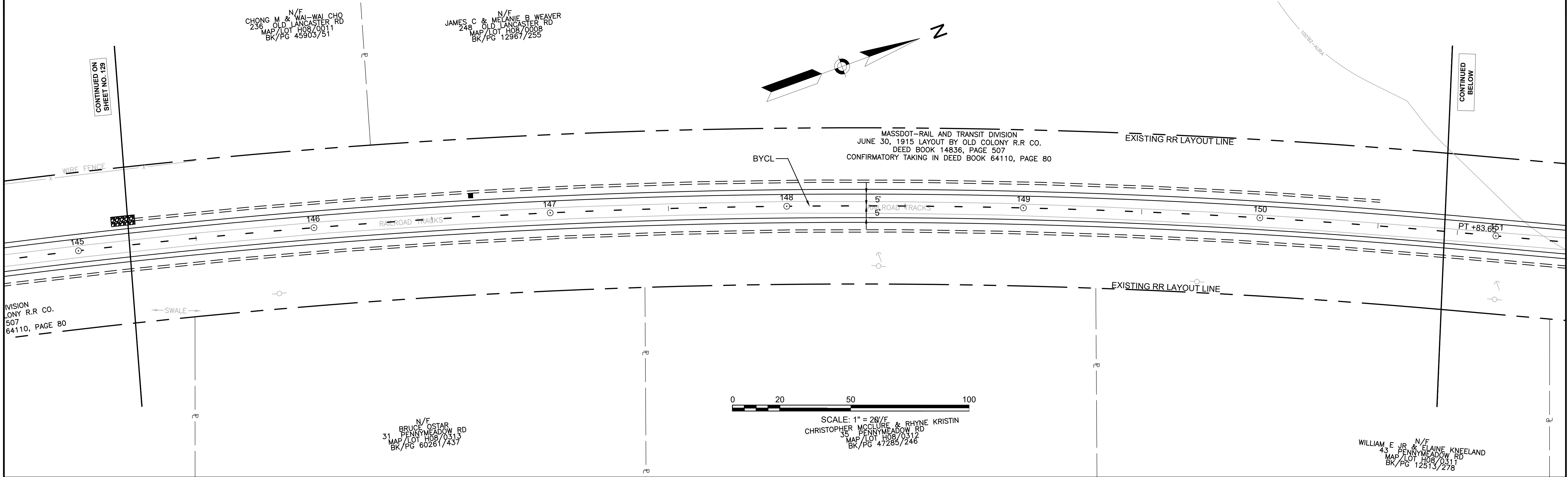
MASSDOT—RAIL AND TRANSIT DIVISION  
 JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
 DEED BOOK 14836, PAGE 507  
 CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

0 20 50 100  
 SCALE: 1" = 20'

**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	130	318
PROJECT FILE NO.		608164	

**PAVEMENT MARKING & SIGNING PLANS**



CONTINUED ON  
SHEET NO. 129

CONTINUED  
BELOW

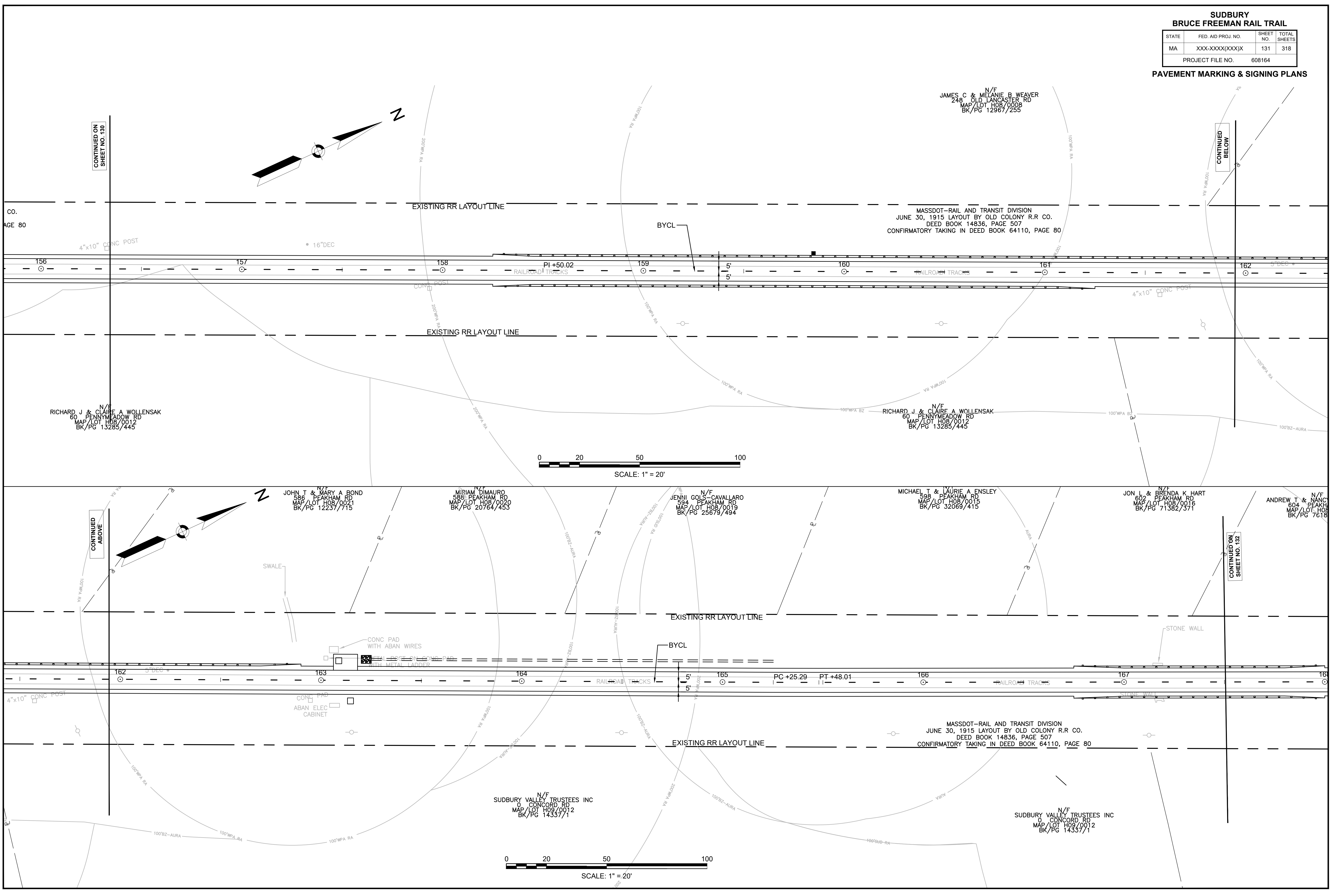
CONTINUED  
ABOVE

CONTINUED ON  
SHEET NO. 131

**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	131	318
PROJECT FILE NO.		608164	

**PAVEMENT MARKING & SIGNING PLANS**



CONTINUED ON  
SHEET NO. 130

CONTINUED  
BELOW

CONTINUED  
ABOVE

CONTINUED ON  
SHEET NO. 132

MASSDOT-RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

MASSDOT-RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

N/F  
JAMES C & MELANIE B WEAVER  
248 OLD LANCASTER RD  
MAP/LOT H08/0008  
BK/PG 12967/255

N/F  
RICHARD J & CLAIRE A WOLLENSAK  
60 PENNYMEADOW RD  
MAP/LOT H08/0012  
BK/PG 13285/445

N/F  
JOHN T & MARY A BOND  
586 PEAKHAM RD  
MAP/LOT H08/0021  
BK/PG 12237/775

N/F  
MIRIAM DIMAURO  
586 PEAKHAM RD  
MAP/LOT H08/0020  
BK/PG 20764/453

N/F  
JENNI GOLS-CAVALLARO  
594 PEAKHAM RD  
MAP/LOT H08/0019  
BK/PG 25679/494

N/F  
MICHAEL T & LAURIE A ENSLEY  
598 PEAKHAM RD  
MAP/LOT H08/0015  
BK/PG 32069/415

N/F  
JON I & BRENDA K HART  
602 PEAKHAM RD  
MAP/LOT H08/0016  
BK/PG 71382/371

N/F  
ANDREW T & NANCY  
604 PEAKHAM RD  
MAP/LOT H08/0017  
BK/PG 7618

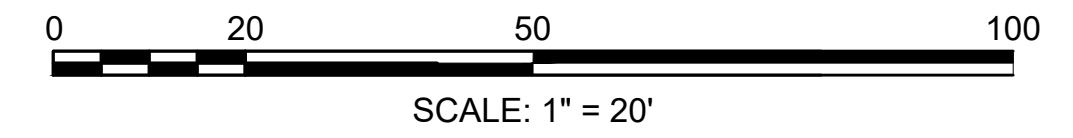
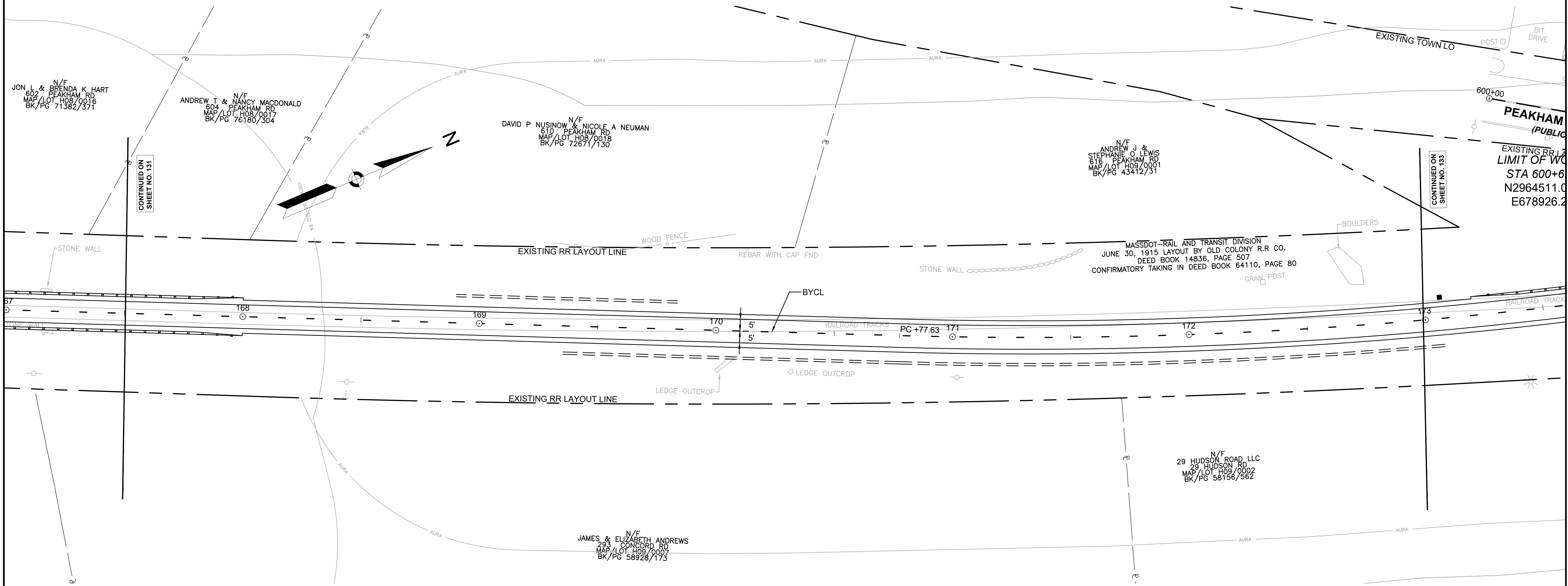
N/F  
SUDBURY VALLEY TRUSTEES INC  
0 CONCORD RD  
MAP/LOT H09/0012  
BK/PG 14337/1

N/F  
SUDBURY VALLEY TRUSTEES INC  
0 CONCORD RD  
MAP/LOT H09/0012  
BK/PG 14337/1

**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	132	318
PROJECT FILE NO.		608164	

**PAVEMENT MARKING & SIGNING PLANS**

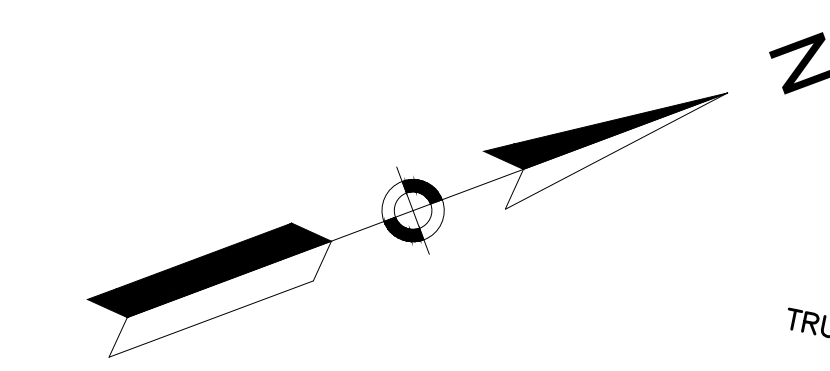


CONTINUED ON SHEET NO. 131

CONTINUED ON SHEET NO. 133

MASSDOT-RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

**PEAKHAM**  
(PUBLIC)  
EXISTING RR LAYOUT  
LIMIT OF WORK  
STA 600+60  
N2964511.0  
E678926.2



N/F  
 LINDA LOUISE MURI  
 TRUSTEE OF LINDA LOUISE MURI  
 623 PEAKHAM RD  
 MAP/LOT H09/0056  
 BK/PG 66273/460

N/F  
 LINDA LOUISE MURI  
 TRUSTEE OF LINDA LOUISE MURI  
 623 PEAKHAM RD  
 MAP/LOT H09/0056  
 BK/PG 66273/460

**LIMIT OF WORK**  
 STA 504+26.44  
 N2964724.5314  
 E678875.4345

N/F  
 NICHOLAS A TRITOS & ROANNA M LONDON  
 46 HUDSON RD  
 MAP/LOT G09/0013  
 BK/PG 45390/280

N/F  
 TOWN OF SUDBURY (TREASURER)  
 HUDSON RD  
 MAP/LOT G09/0200  
 BK/PG 13189/604

N/F  
 LAWRENCE W TIGHE  
 THE HUDSON ROAD TRUST #2  
 49 HUDSON RD  
 MAP/LOT G09/0001  
 BK/PG 33228/333

N/F  
 PENN CENTRAL TRANSPORTATION CO  
 HUDSON RD  
 MAP/LOT G09/0012  
 BK/PG 7074/339

N/F  
 MATTHEW S & MOLLY F GILMARTIN  
 50 HUDSON RD  
 MAP/LOT H09/0068  
 BK/PG 42175/386

N/F  
 MATTHEW S & MOLLY F GILMARTIN  
 50 HUDSON RD  
 MAP/LOT H09/0068  
 BK/PG 42175/386

N/F  
 TOWN OF SUDBURY & BOARD OF SELECT BOARD  
 (TOWN CENTER LAND)  
 HUDSON RD  
 MAP/LOT G09/0004  
 BK/PG 77156/463 & 77156/472

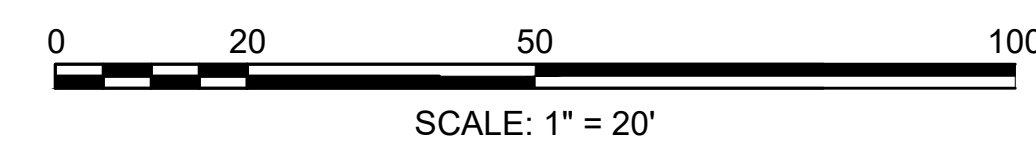
**PEAKHAM ROAD (PUBLIC)**  
 EXISTING RR LAYOUT LINE  
**LIMIT OF WORK**  
 STA 600+65.46  
 N2964511.0042  
 E678926.2687

SEE "ROADWAY PAVEMENT MARKINGS ON RAIL TRAIL" DETAIL ON SHEET 125 (TYP)

<b>PARKINSON LOT</b>	0.1mi	↑
<b>BROAD ACRES FARM LOT</b>	0.7mi	↑
<b>SUDBURY-CONCORD TOWN LINE</b>	3mi	↑

<b>SUDBURY CENTER HISTORIC DISTRICT</b>	→
<b>SUDBURY TOWN HALL</b>	→

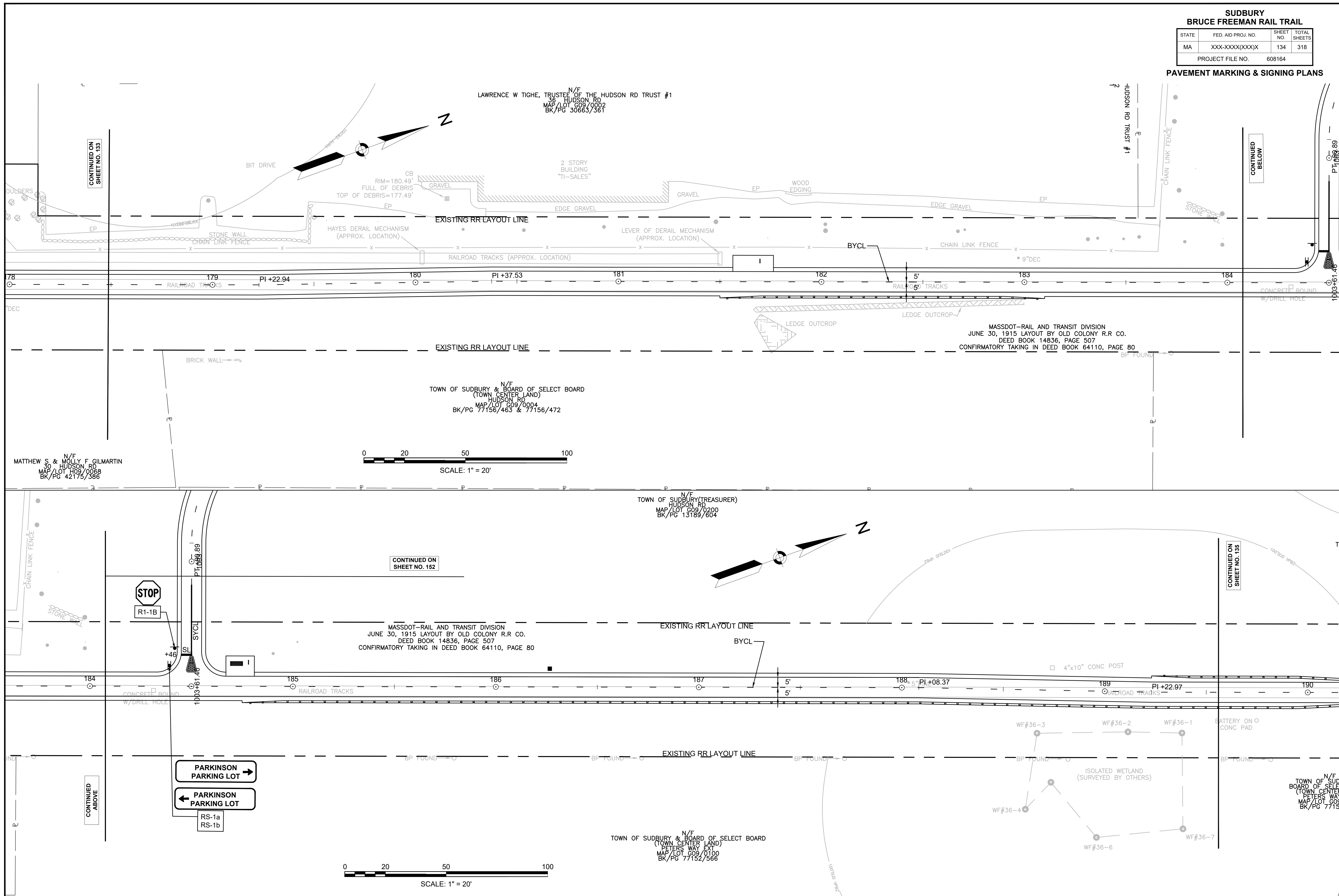
**LIMIT OF WORK**  
 STA 500+28.85  
 N2964552.0831  
 E679227.4914



**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	134	318
PROJECT FILE NO.		608164	

**PAVEMENT MARKING & SIGNING PLANS**



CONTINUED ON  
SHEET NO. 133

CONTINUED  
BELOW

CONTINUED ON  
SHEET NO. 152

CONTINUED ON  
SHEET NO. 135

CONTINUED  
ABOVE

N/F  
TOWN OF SUDBURY  
BOARD OF SELECT BOARD  
(TOWN CENTER LAND)  
PETERS WAY EXT  
MAP/LOT 609/0100  
BK/PG 77152/566



**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

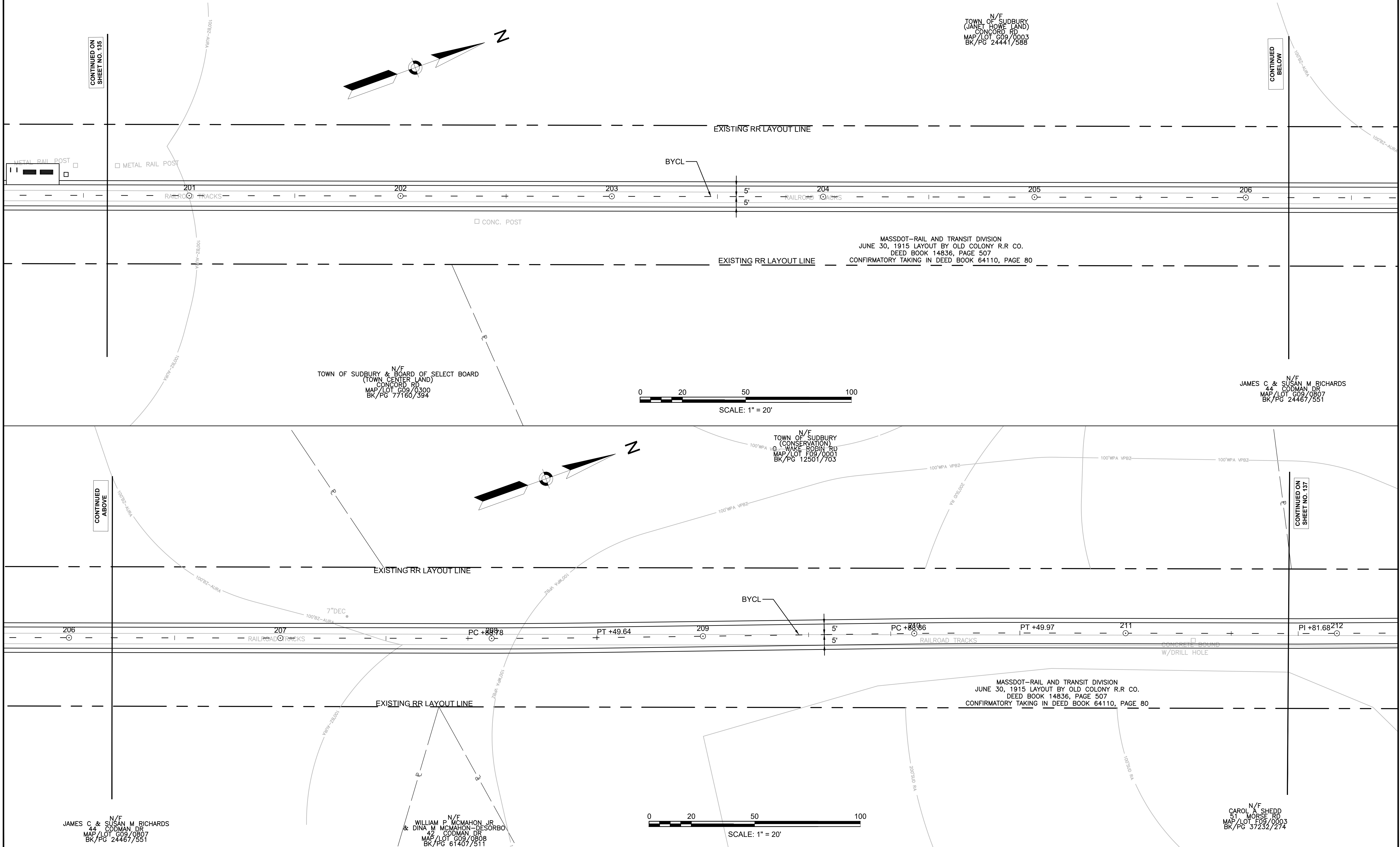
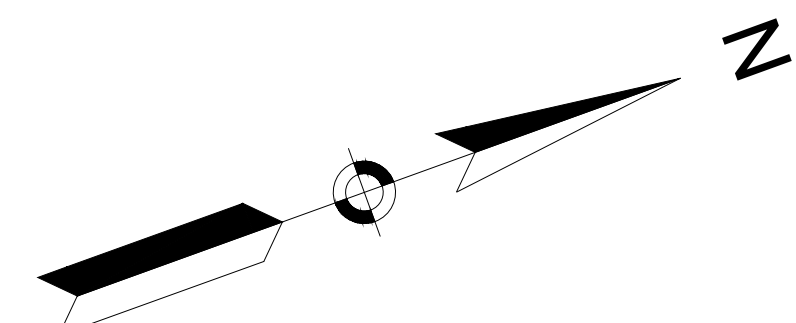
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	136	318
PROJECT FILE NO.		608164	

**PAVEMENT MARKING & SIGNING PLANS**

N/E  
TOWN OF SUDBURY  
(JANET HOWE LAND)  
CONCORD RD  
MAP/LOT G09/0003  
BK/PG 24441/588

CONTINUED ON  
SHEET NO. 135

CONTINUED  
BELOW



MASSDOT—RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

MASSDOT—RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

N/E  
TOWN OF SUDBURY & BOARD OF SELECT BOARD  
(TOWN CENTER LAND)  
CONCORD RD  
MAP/LOT G09/0300  
BK/PG 77160/394

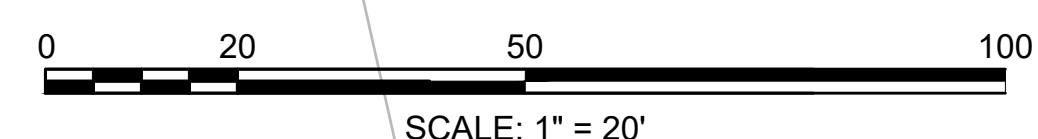
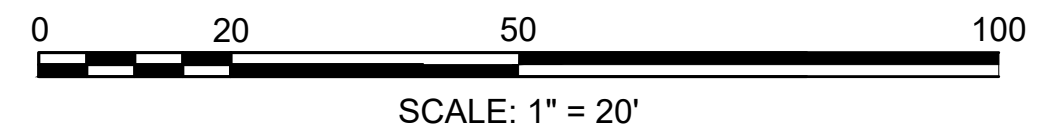
N/E  
JAMES C & SUSAN M RICHARDS  
44 CODMAN DR  
MAP/LOT G09/0807  
BK/PG 24467/551

N/E  
TOWN OF SUDBURY  
(CONSERVATION)  
WAKE ROBIN RD  
MAP/LOT F09/0001  
BK/PG 12501/703

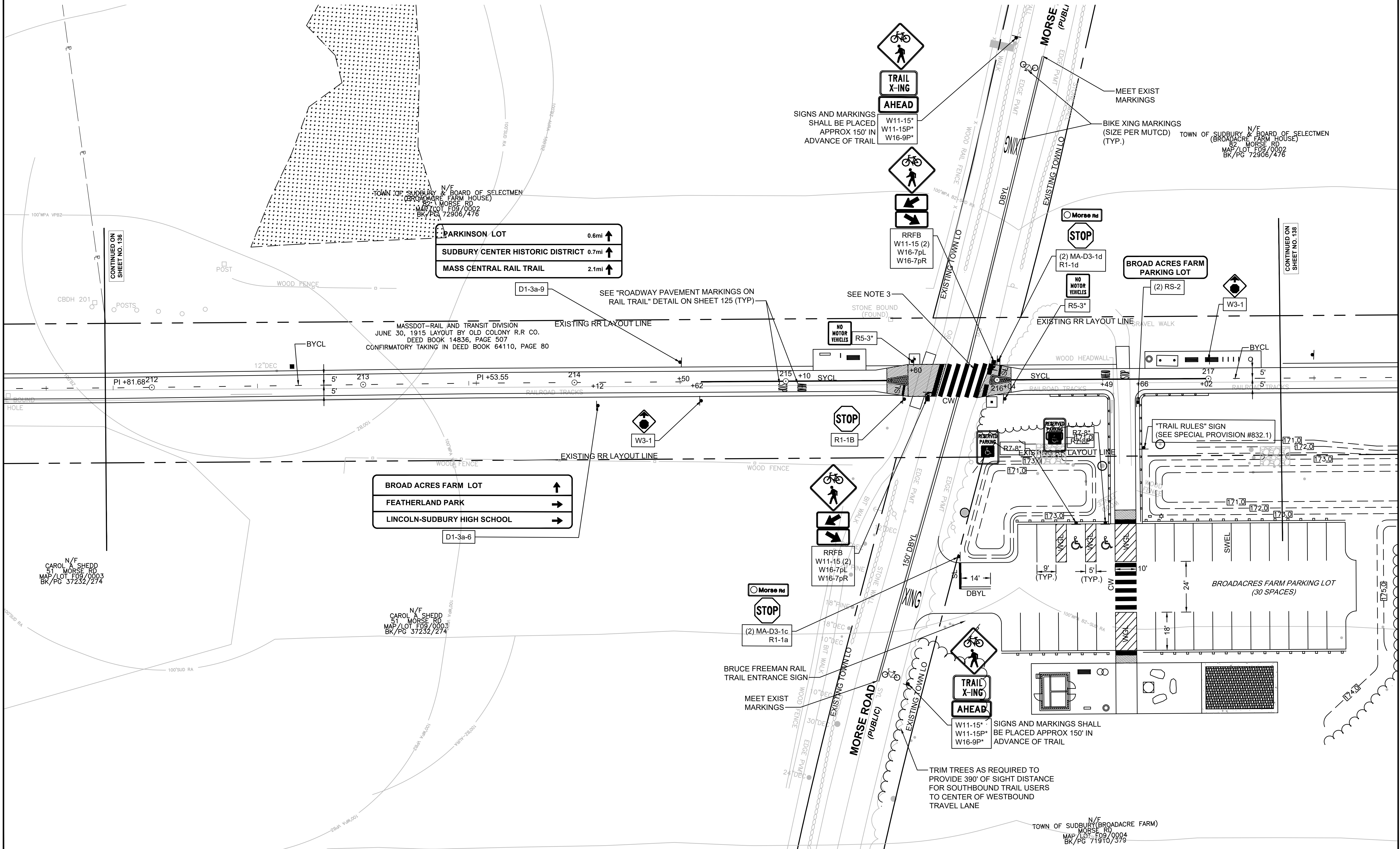
N/E  
JAMES C & SUSAN M RICHARDS  
44 CODMAN DR  
MAP/LOT G09/0807  
BK/PG 24467/551

N/E  
WILLIAM P MCMAHON JR  
& DINA M MCMAHON-DESORBO  
42 CODMAN DR  
MAP/LOT G09/0908  
BK/PG 61407/511

N/E  
CAROL A SHEDD  
51 MORSE RD  
MAP/LOT F09/0003  
BK/PG 37232/274







**PARKINSON LOT** 0.6mi ↑  
**SUDBURY CENTER HISTORIC DISTRICT** 0.7mi ↑  
**MASS CENTRAL RAIL TRAIL** 2.1mi ↑

**BROAD ACRES FARM LOT** ↑  
**FEATHERLAND PARK** →  
**LINCOLN-SUDBURY HIGH SCHOOL** →

SIGNS AND MARKINGS SHALL BE PLACED APPROX 150' IN ADVANCE OF TRAIL  
 W11-15\*  
 W11-15P\*  
 W16-9P\*

SIGNS AND MARKINGS SHALL BE PLACED APPROX 150' IN ADVANCE OF TRAIL  
 W11-15\*  
 W11-15P\*  
 W16-9P\*

TRIM TREES AS REQUIRED TO PROVIDE 390' OF SIGHT DISTANCE FOR SOUTHBOUND TRAIL USERS TO CENTER OF WESTBOUND TRAVEL LANE

MEET EXIST MARKINGS  
 BIKE XING MARKINGS (SIZE PER MUTCD) TOWN OF SUDBURY & BOARD OF SELECTMEN (BROADACRE FARM HOUSE) MAP/LOT F09/0002 BK/PG 72906/476

"TRAIL RULES" SIGN (SEE SPECIAL PROVISION #832.1)

TOWN OF SUDBURY (BROADACRE FARM) MAP/LOT F09/0004 BK/PG 71910/379

CONTINUED ON SHEET NO. 136

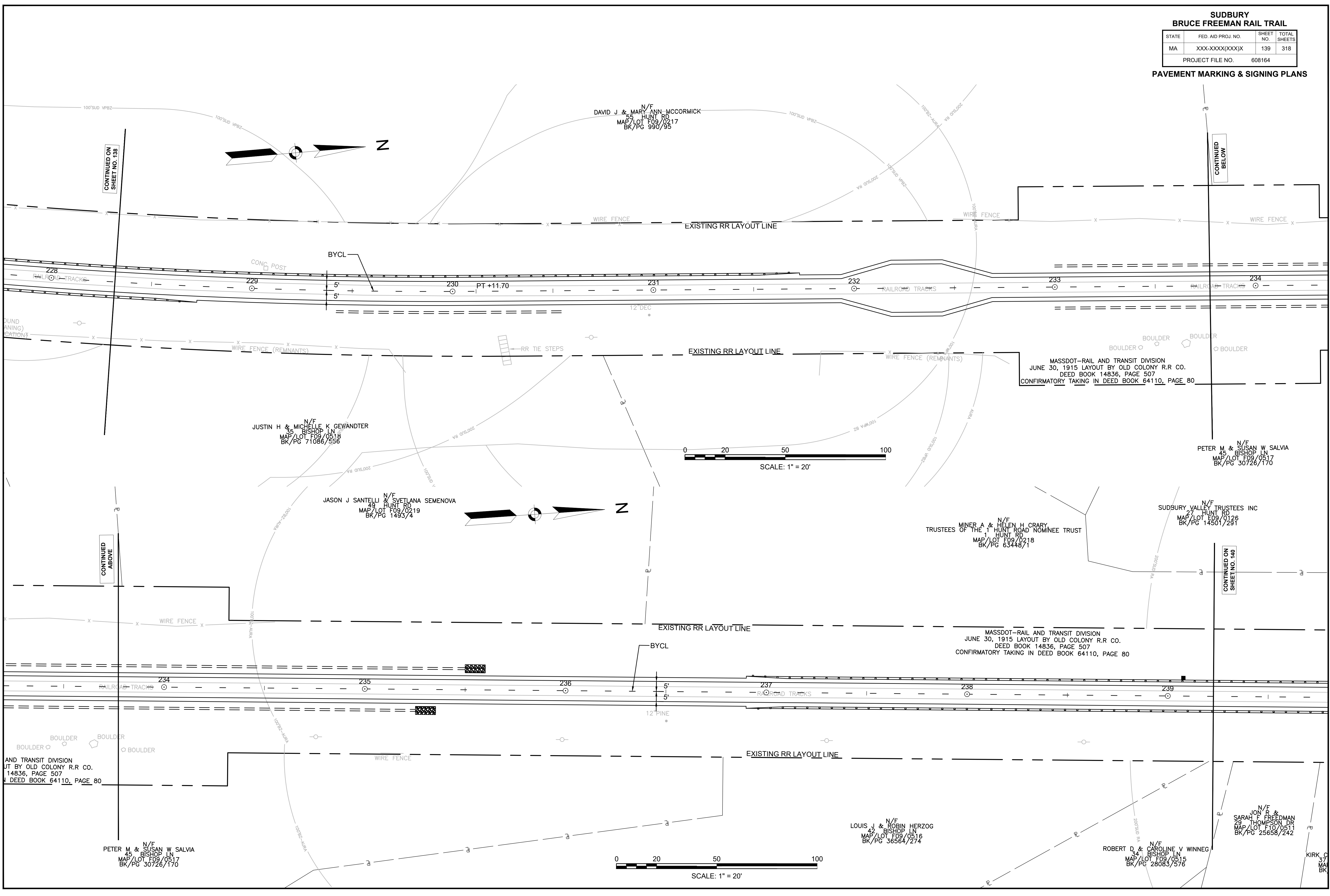
CONTINUED ON SHEET NO. 138



**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	139	318
PROJECT FILE NO.		608164	

**PAVEMENT MARKING & SIGNING PLANS**

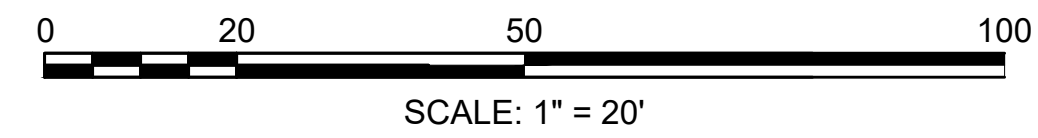
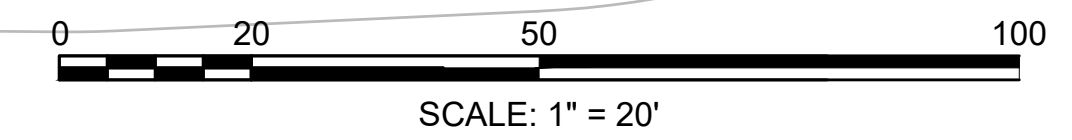
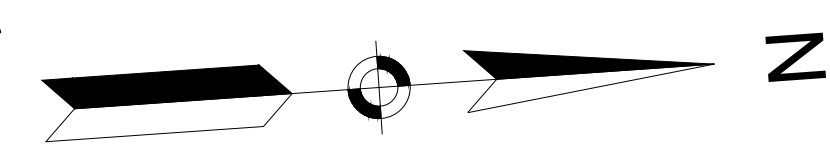
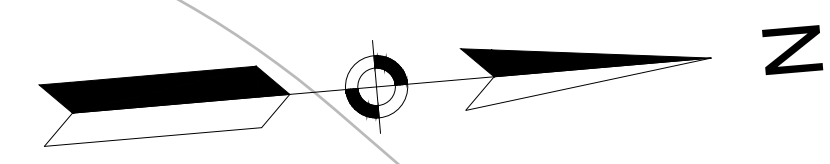


CONTINUED ON  
SHEET NO. 138

CONTINUED  
BELOW

CONTINUED  
ABOVE

CONTINUED ON  
SHEET NO. 140



N/F  
DAVID J & MARY ANN MCCORMICK  
55 HUNT RD  
MAP/LOT F09/0217  
BK/PG 990/95

N/F  
JUSTIN H & MICHELLE K GEWANDTER  
35 BISHOP LN  
MAP/LOT F09/0518  
BK/PG 71086/536

N/F  
JASON J SANTELLI & SVETLANA SEMENOVA  
49 HUNT RD  
MAP/LOT F09/0219  
BK/PG 1493/4

N/F  
MINER A & HELEN H CRAY  
TRUSTEES OF THE HUNT ROAD NOMINEE TRUST  
HUNT RD  
MAP/LOT F09/0218  
BK/PG 63448/1

N/F  
SUDBURY VALLEY TRUSTEES INC  
27 HUNT RD  
MAP/LOT F09/0126  
BK/PG 14501/291

N/F  
PETER M & SUSAN W SALVIA  
45 BISHOP LN  
MAP/LOT F09/0517  
BK/PG 30726/170

AND TRANSIT DIVISION  
UT BY OLD COLONY R.R. CO.  
14836, PAGE 507  
DEED BOOK 64110, PAGE 80

MASSDOT-RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

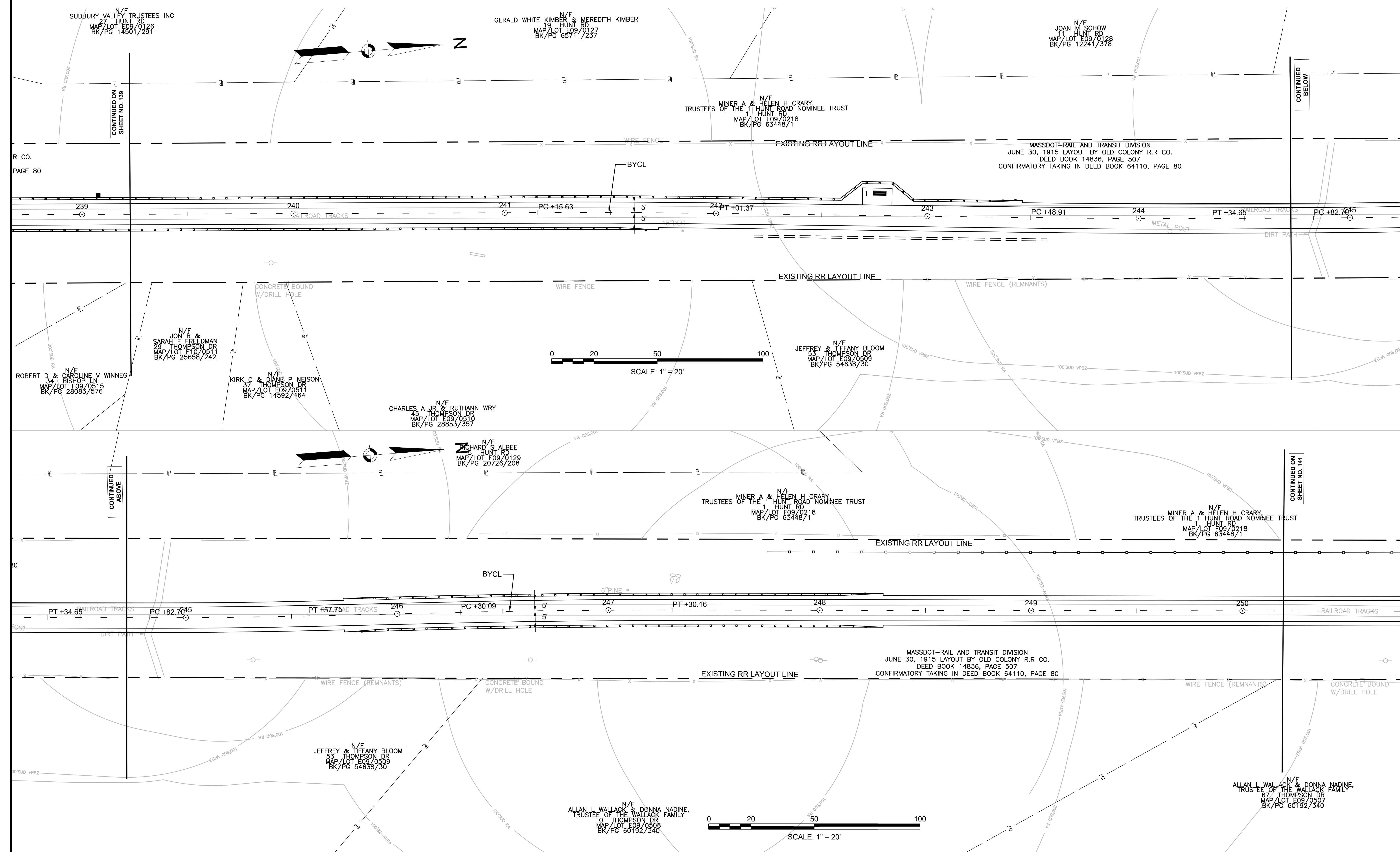
N/F  
PETER M & SUSAN W SALVIA  
45 BISHOP LN  
MAP/LOT F09/0517  
BK/PG 30726/170

N/F  
LOUIS J & ROBIN HERZOG  
42 BISHOP LN  
MAP/LOT F09/0516  
BK/PG 36564/274

N/F  
ROBERT D & CAROLINE V WINNEG  
34 BISHOP LN  
MAP/LOT F09/0515  
BK/PG 28083/576

N/F  
JON R & SARAH F FREDMAN  
29 THOMPSON DR  
MAP/LOT F10/0511  
BK/PG 25658/242

KIRK C  
37  
MA  
BK



N/F  
SUDBURY VALLEY TRUSTEES INC  
27 HUNT RD  
MAP/LOT E09/0126  
BK/PG 14501/291

N/F  
GERALD WHITE KIMBER & MEREDITH KIMBER  
19 HUNT RD  
MAP/LOT E09/0127  
BK/PG 65711/237

N/F  
JOAN M. SCHOW  
11 HUNT RD  
MAP/LOT E09/0128  
BK/PG 12241/378

N/F  
MINER A & HELEN H. CRARY  
TRUSTEES OF THE 1 HUNT ROAD NOMINEE TRUST  
1 HUNT RD  
MAP/LOT E09/0218  
BK/PG 63448/1

MASSDOT—RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

N/F  
JON R & SARAH F. FREEDMAN  
29 THOMPSON DR  
MAP/LOT F10/0511  
BK/PG 25658/242

N/F  
ROBERT D. & CAROLINE V. WINNEG  
34 BISHOP LN  
MAP/LOT F09/0515  
BK/PG 28083/376

N/F  
KIRK C & DIANE P. NEISON  
37 THOMPSON DR  
MAP/LOT E09/0511  
BK/PG 14592/464

N/F  
CHARLES A. JR. & RUTHANN WRY  
45 THOMPSON DR  
MAP/LOT E09/0510  
BK/PG 28853/357

N/F  
RICHARD S. ALBEE  
HUNT RD  
MAP/LOT E09/0129  
BK/PG 20726/208

N/F  
JEFFREY & TIFFANY BLOOM  
53 THOMPSON DR  
MAP/LOT E09/0509  
BK/PG 54636/30

N/F  
MINER A & HELEN H. CRARY  
TRUSTEES OF THE 1 HUNT ROAD NOMINEE TRUST  
HUNT RD  
MAP/LOT F09/0218  
BK/PG 63448/1

N/F  
MINER A & HELEN H. CRARY  
TRUSTEES OF THE 1 HUNT ROAD NOMINEE TRUST  
HUNT RD  
MAP/LOT F09/0218  
BK/PG 63448/1

N/F  
JEFFREY & TIFFANY BLOOM  
53 THOMPSON DR  
MAP/LOT E09/0509  
BK/PG 54636/30

N/F  
ALLAN L. WALLACK & DONNA NADINE,  
TRUSTEE OF THE WALLACK FAMILY  
0 THOMPSON DR  
MAP/LOT E09/0509  
BK/PG 60192/340

N/F  
ALLAN L. WALLACK & DONNA NADINE,  
TRUSTEE OF THE WALLACK FAMILY  
67 THOMPSON DR  
MAP/LOT E09/0509  
BK/PG 60192/340

CONTINUED ON  
SHEET NO. 139

CONTINUED  
BELOW

CONTINUED  
ABOVE

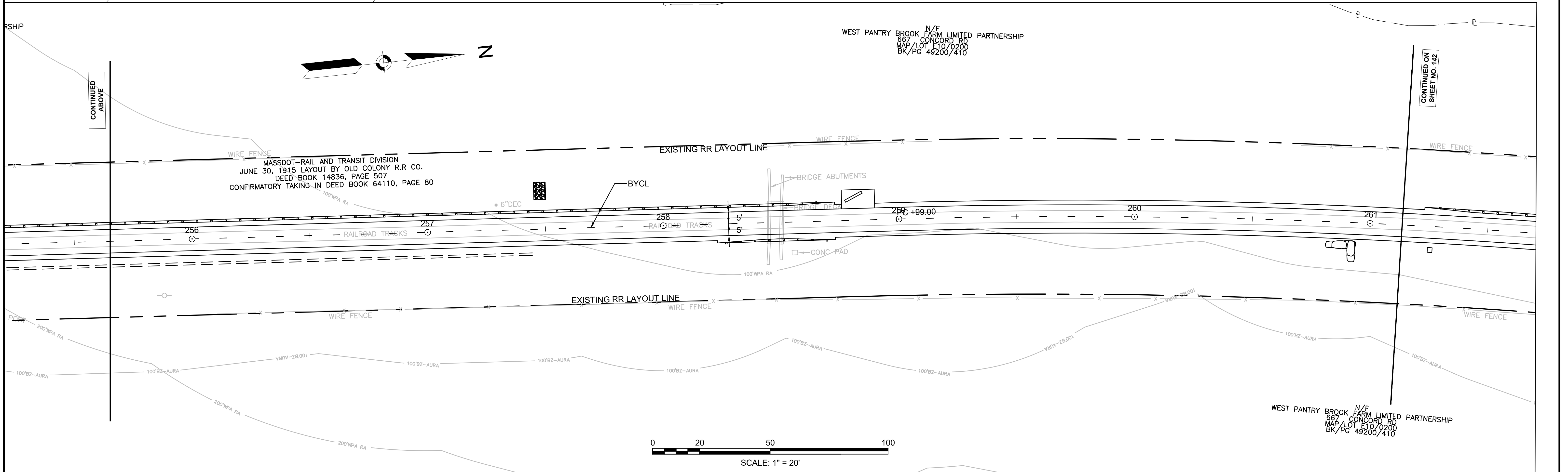
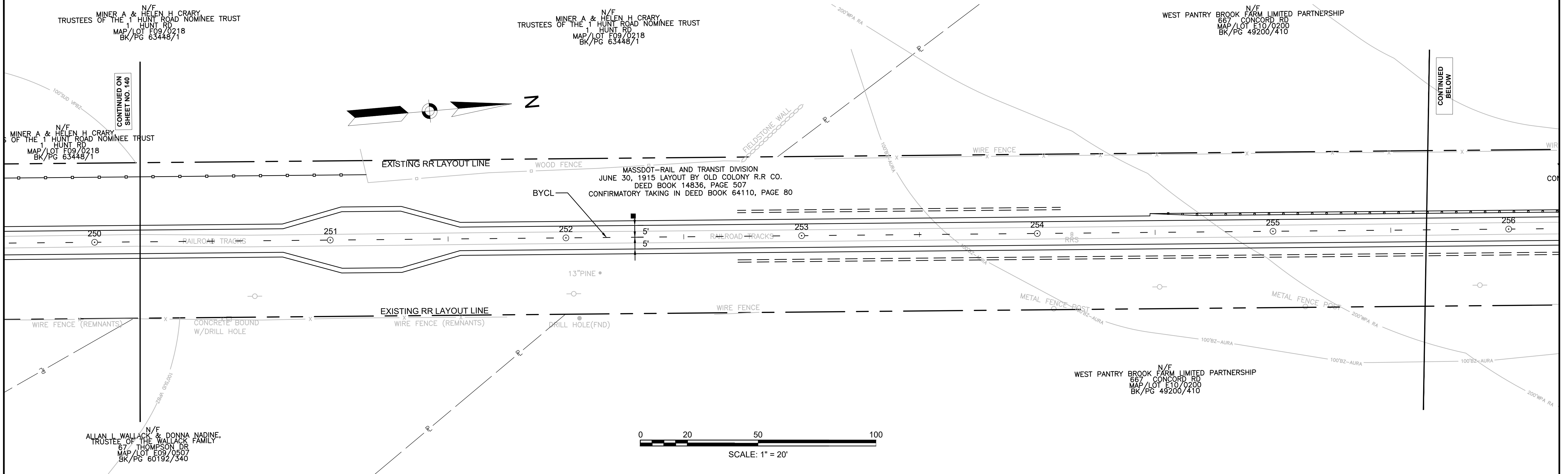
CONTINUED ON  
SHEET NO. 141

R. CO.  
PAGE 80

**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	141	318
PROJECT FILE NO.		608164	

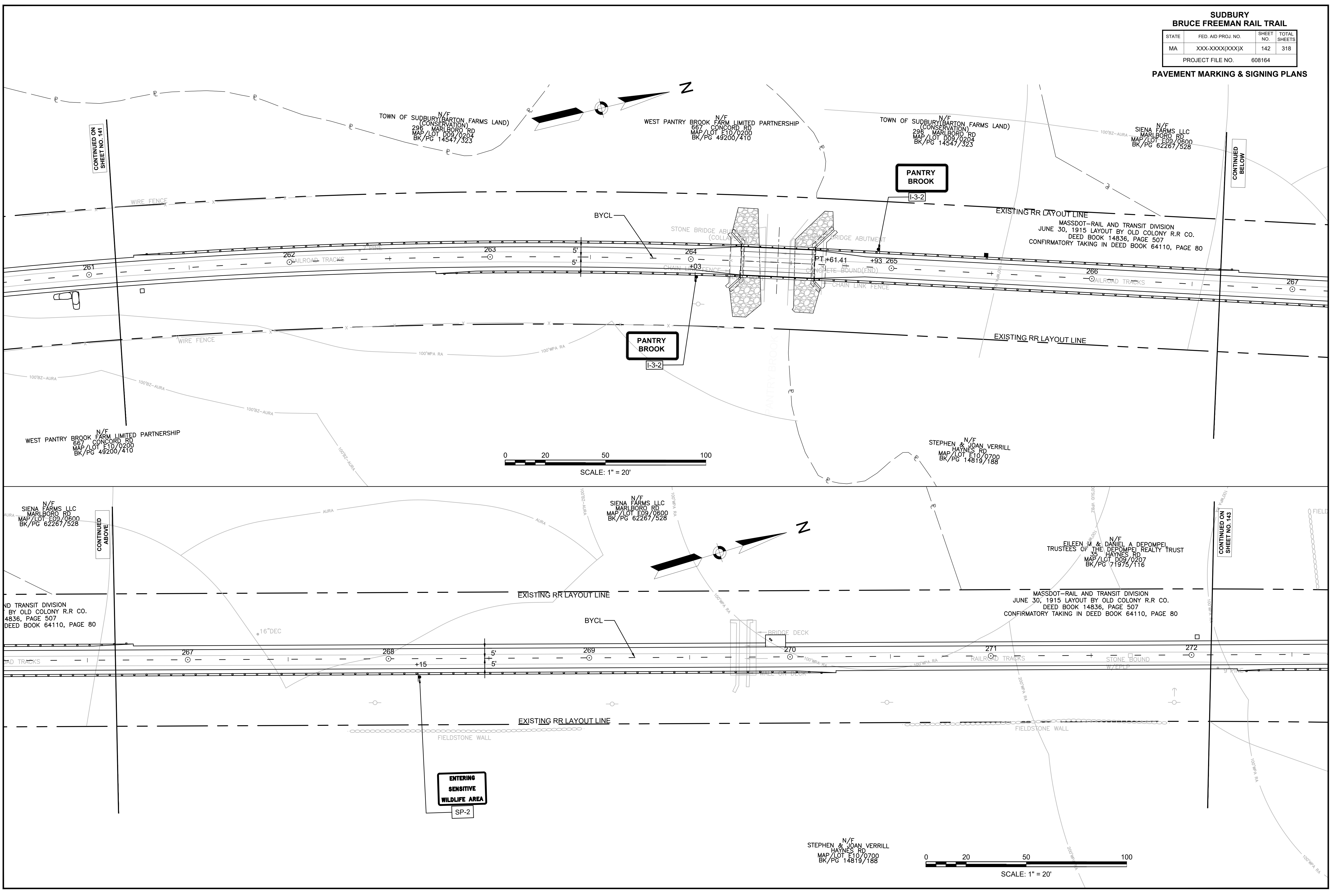
**PAVEMENT MARKING & SIGNING PLANS**



**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	142	318
PROJECT FILE NO.		608164	

**PAVEMENT MARKING & SIGNING PLANS**



CONTINUED ON  
SHEET NO. 141

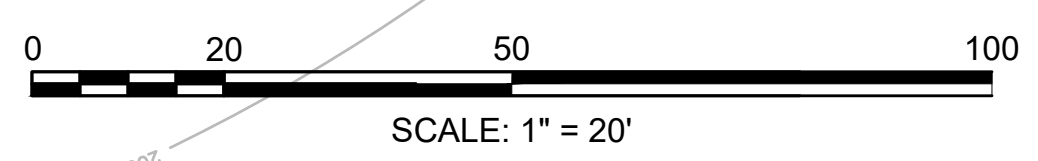
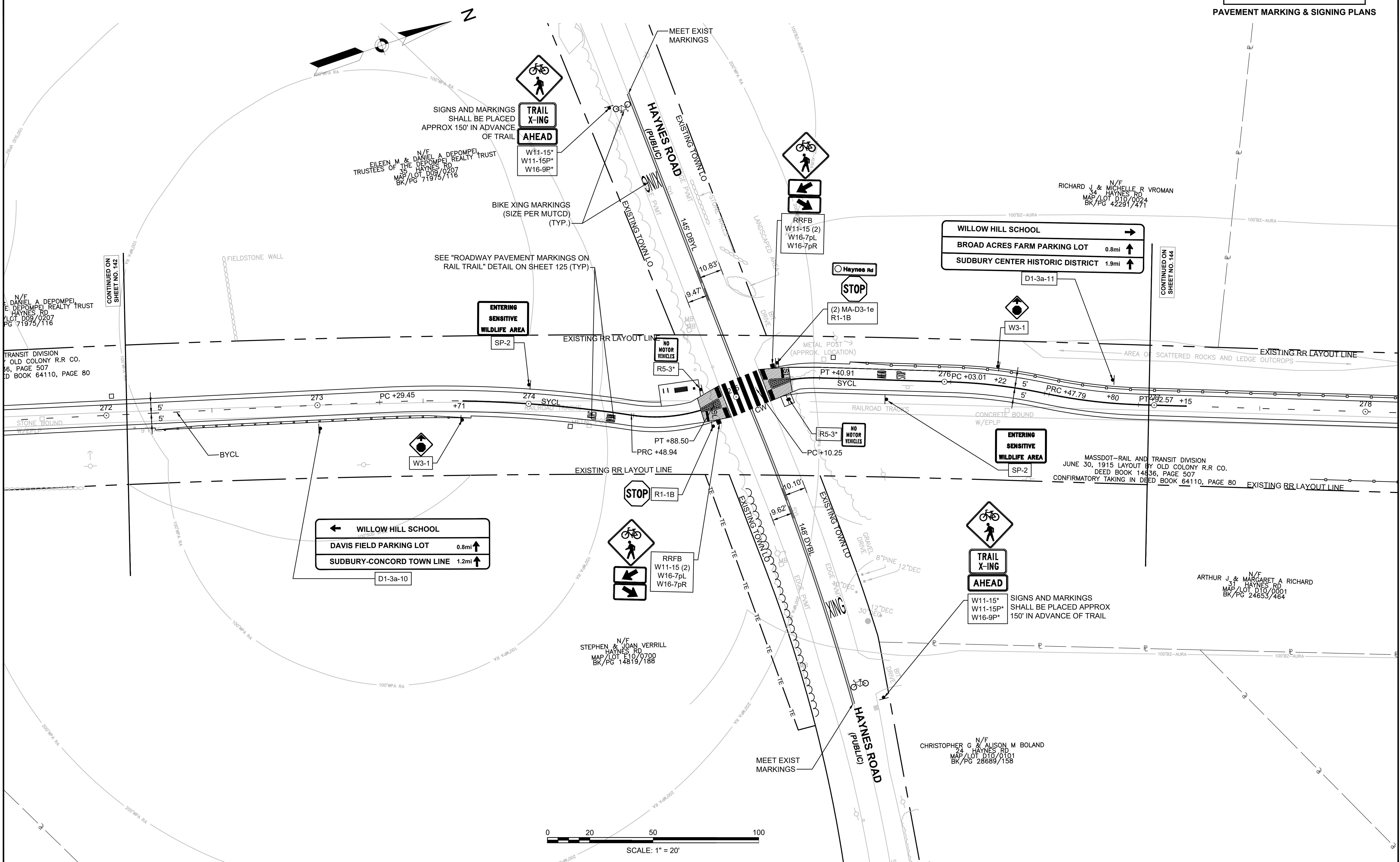
CONTINUED  
BELOW

CONTINUED  
ABOVE

CONTINUED  
ON  
SHEET NO. 143

**ENTERING  
SENSITIVE  
WILDLIFE AREA**  
SP-2

STEPHEN & JOAN VERRILL  
HAYNES RD  
MAP/LOT E10/0700  
BK/PG 14819/188



CONTINUED ON SHEET NO. 142

CONTINUED ON SHEET NO. 144

N/E DANIEL A. DEPOMPEI, E. DEPOMPEI REALTY TRUST HAYNES RD MAP/LOT D09/0207 BK/PG 71975/116

TRANSIT DIVISION OLD COLONY R.R. CO. DEED BOOK 64110, PAGE 80

N/E EILEEN M. & DANIEL A. DEPOMPEI, TRUSTEES OF THE DEPOMPEI REALTY TRUST HAYNES RD MAP/LOT D09/0207 BK/PG 71975/116

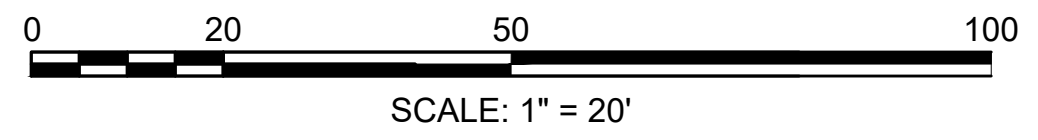
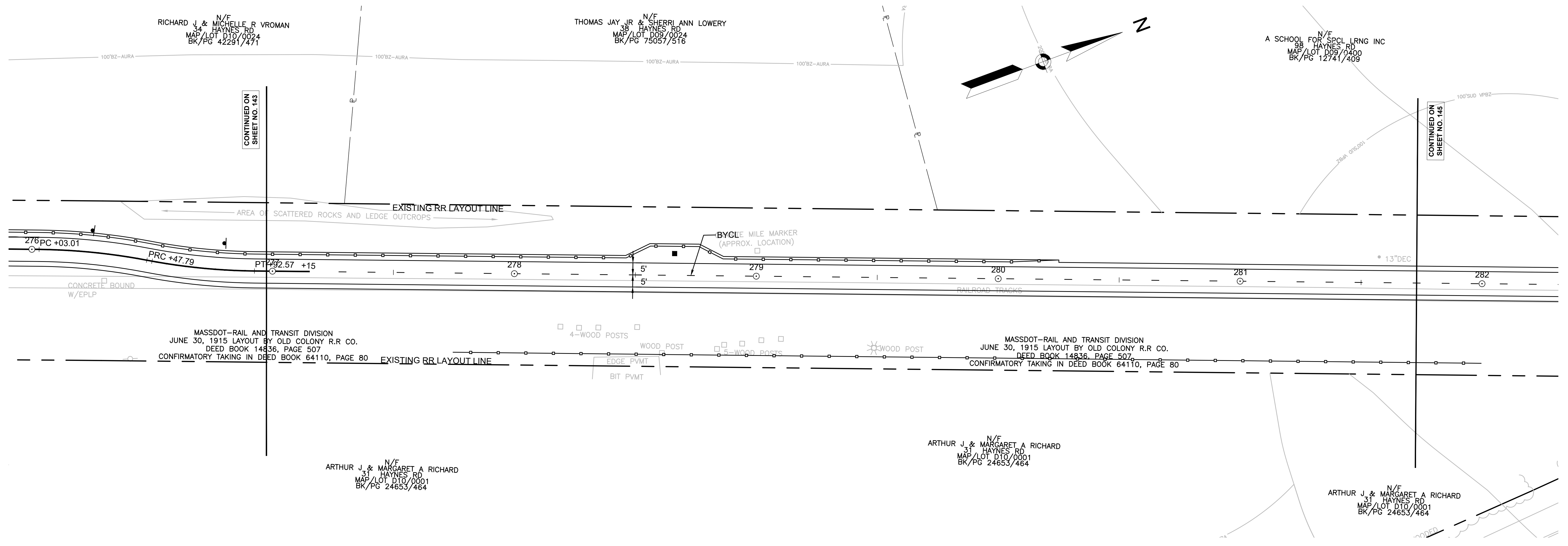
N/F RICHARD J. & MICHELLE R. VROMAN 34 HAYNES RD MAP/LOT D10/0024 BK/PG 42291/471

MASSDOT-RAIL AND TRANSIT DIVISION JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO. DEED BOOK 14836, PAGE 507 CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

N/F ARTHUR J. & MARGARET A. RICHARD 31 HAYNES RD MAP/LOT D10/0001 BK/PG 24653/464

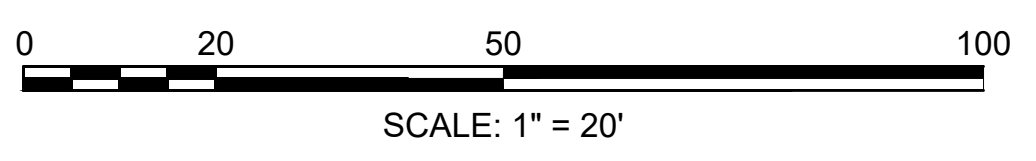
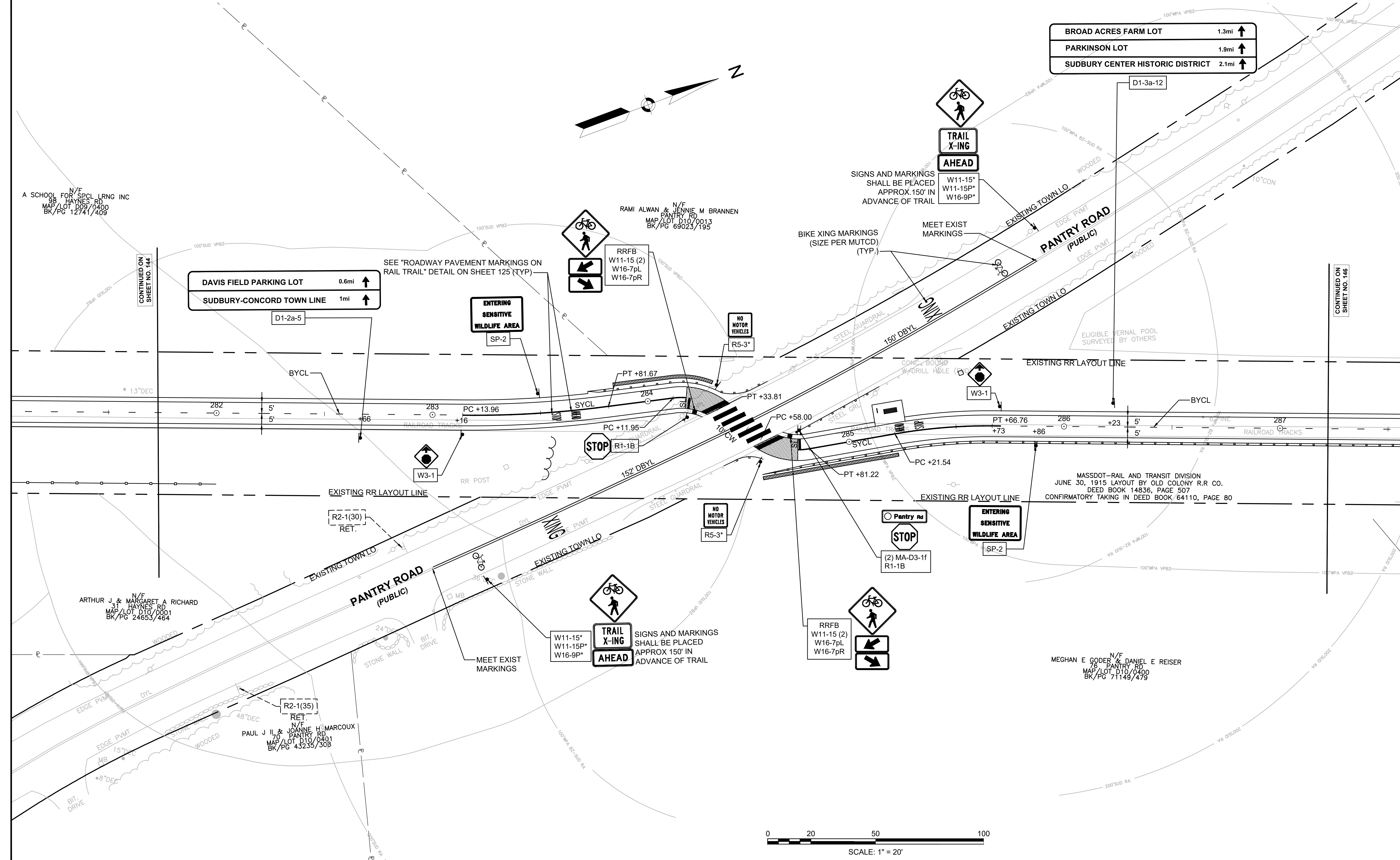
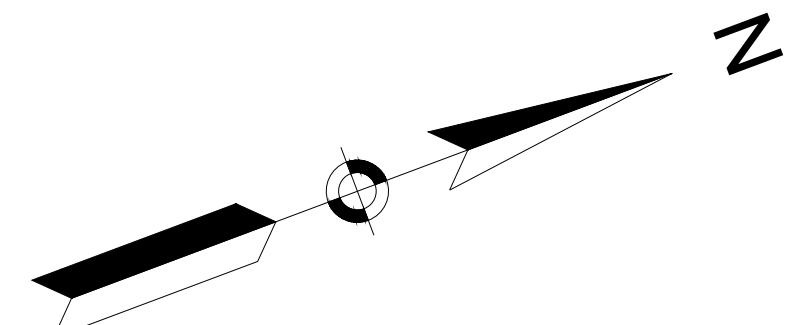
N/E STEPHEN & JOAN VERRILL HAYNES RD MAP/LOT E10/0700 BK/PG 14819/188

N/F CHRISTOPHER G. & ALISON M. BOLAND 24 HAYNES RD MAP/LOT D18/0101 BK/PG 28689/158



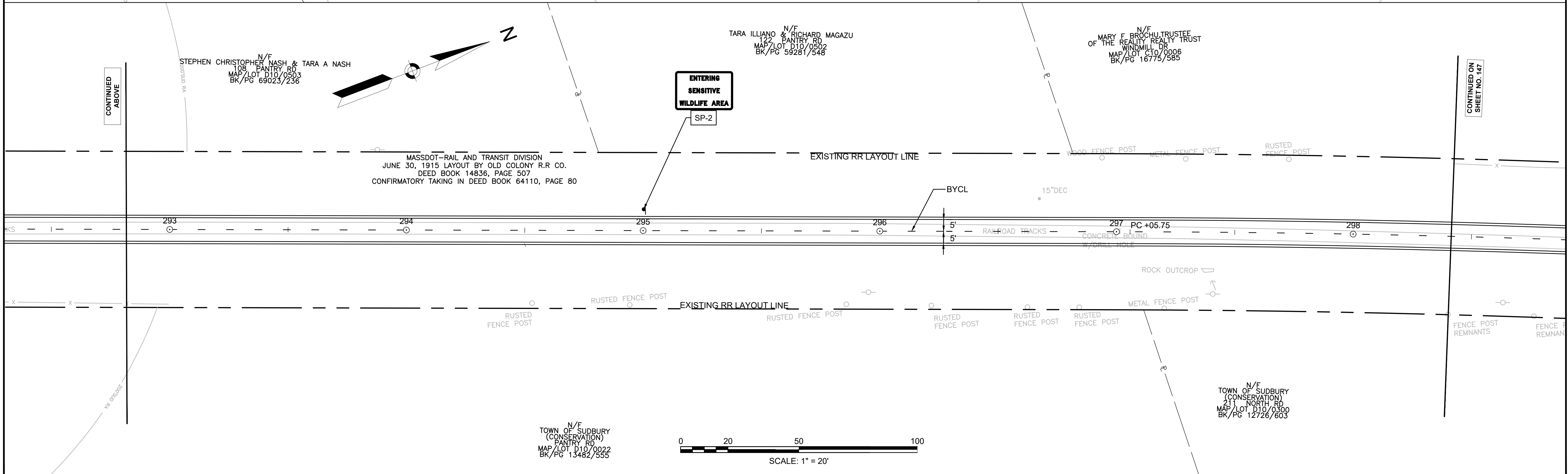
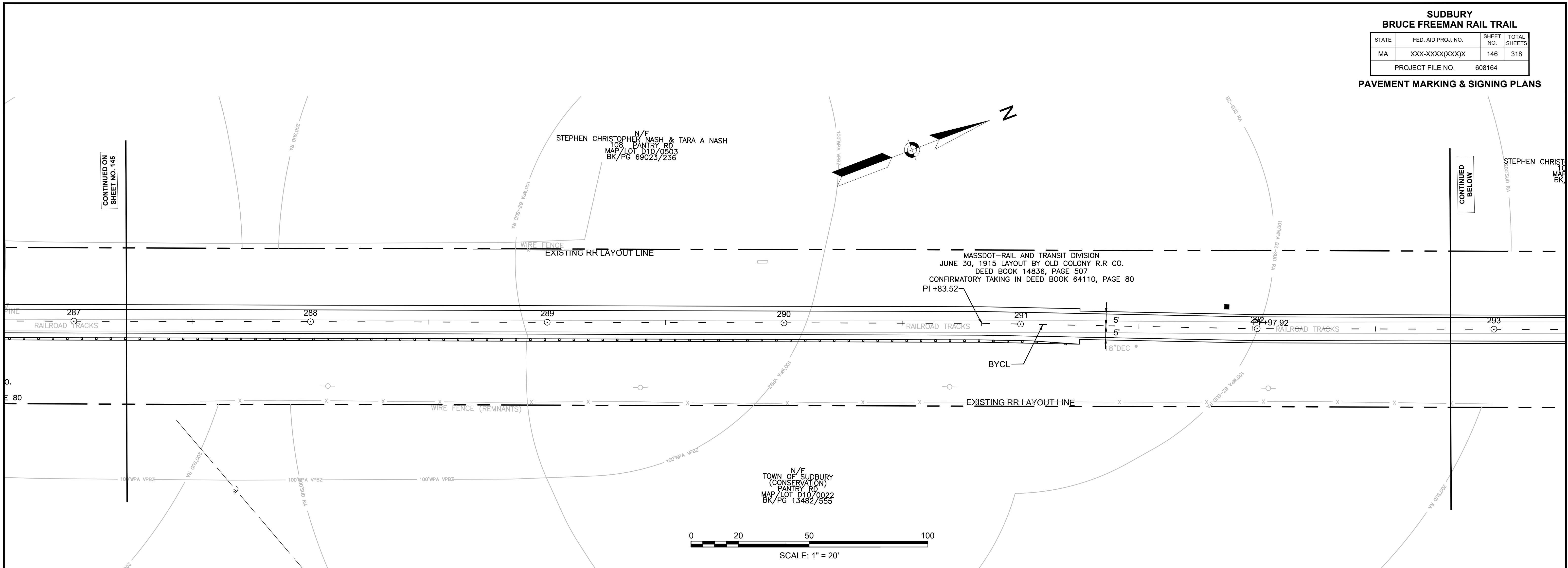


**BROAD ACRES FARM LOT** 1.3mi ↑  
**PARKINSON LOT** 1.9mi ↑  
**SUDBURY CENTER HISTORIC DISTRICT** 2.1mi ↑



CONTINUED ON SHEET NO. 144

CONTINUED ON SHEET NO. 146



CONTINUED ON  
SHEET NO. 145

CONTINUED  
BELOW

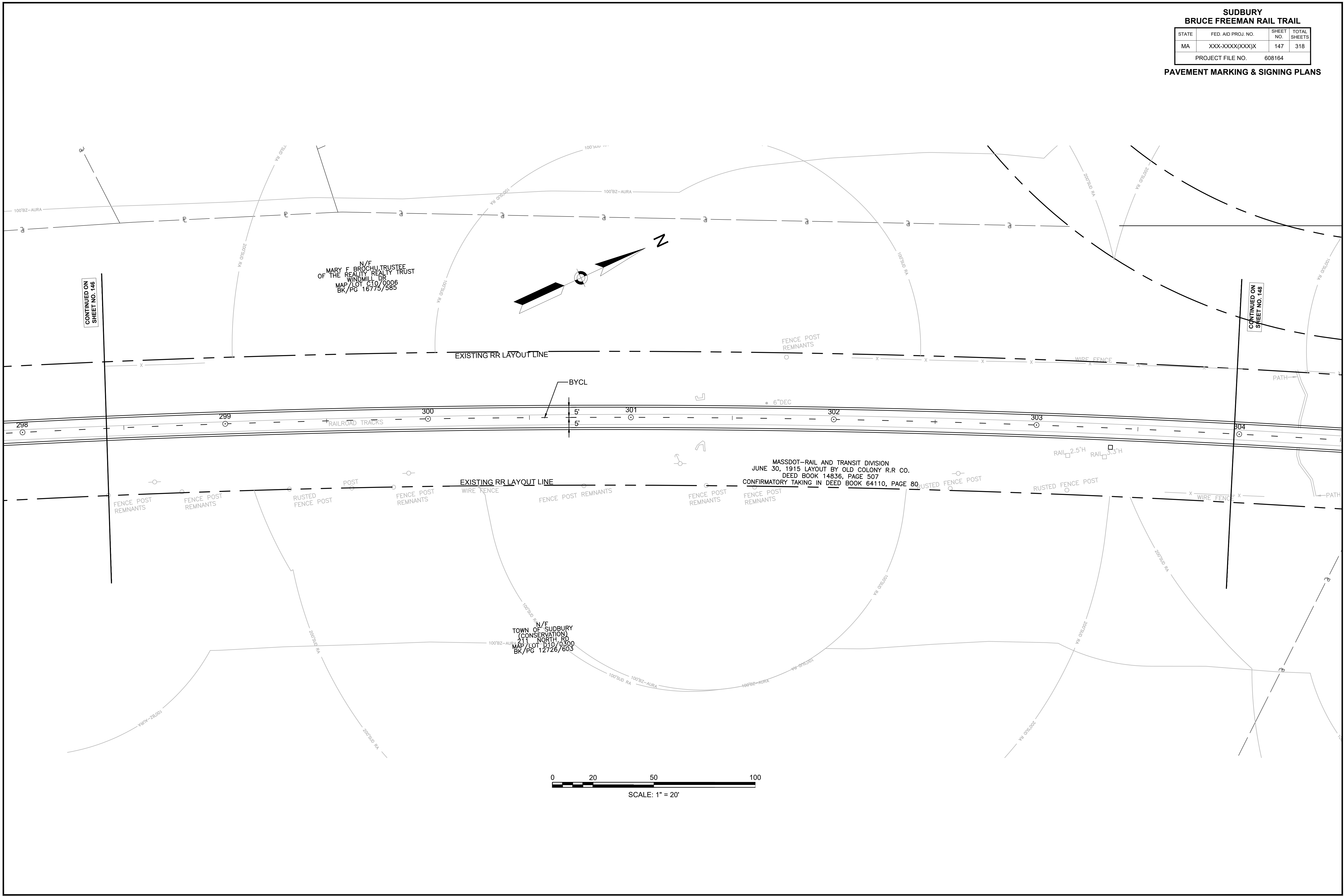
CONTINUED  
ABOVE

CONTINUED ON  
SHEET NO. 147

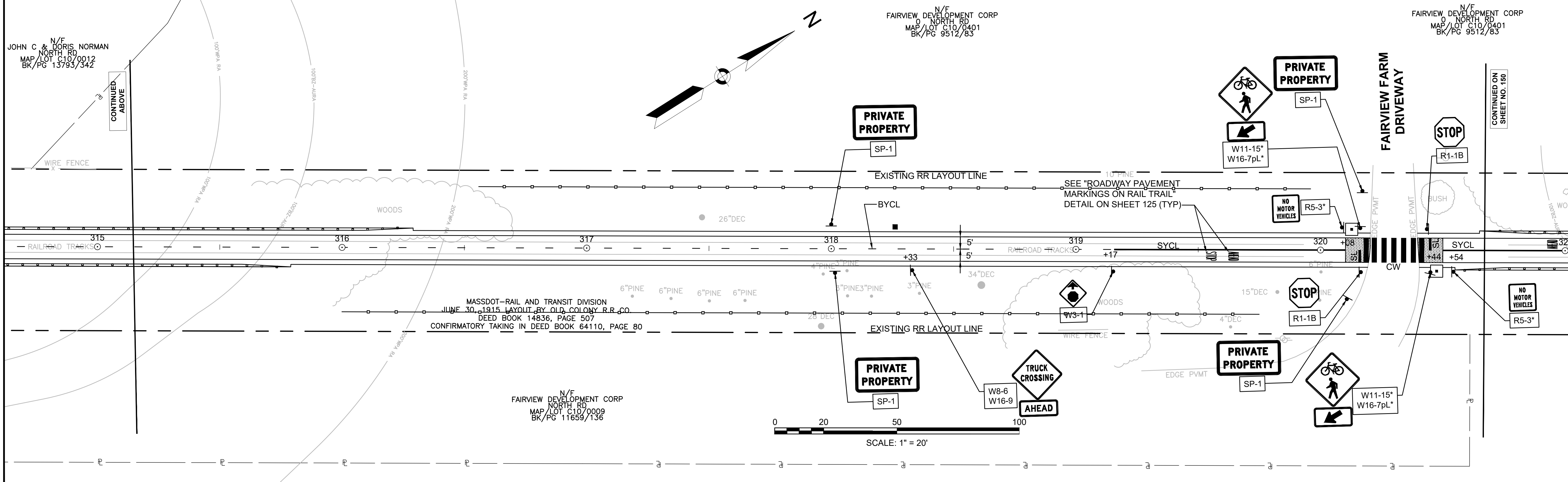
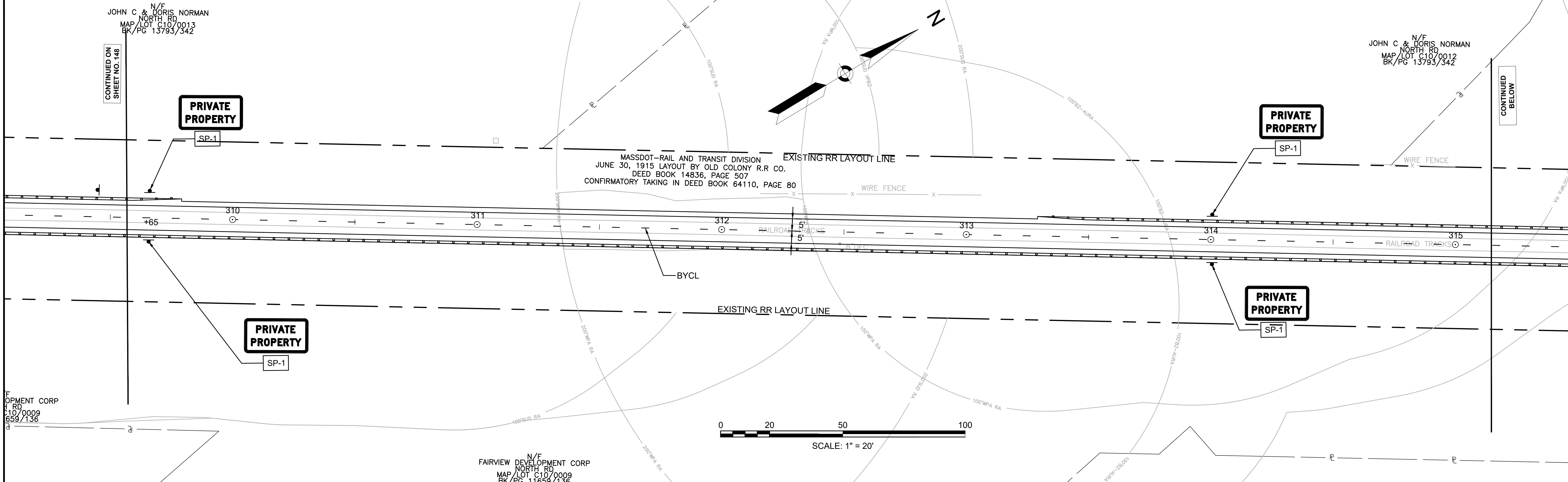
**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	147	318
PROJECT FILE NO.		608164	

**PAVEMENT MARKING & SIGNING PLANS**





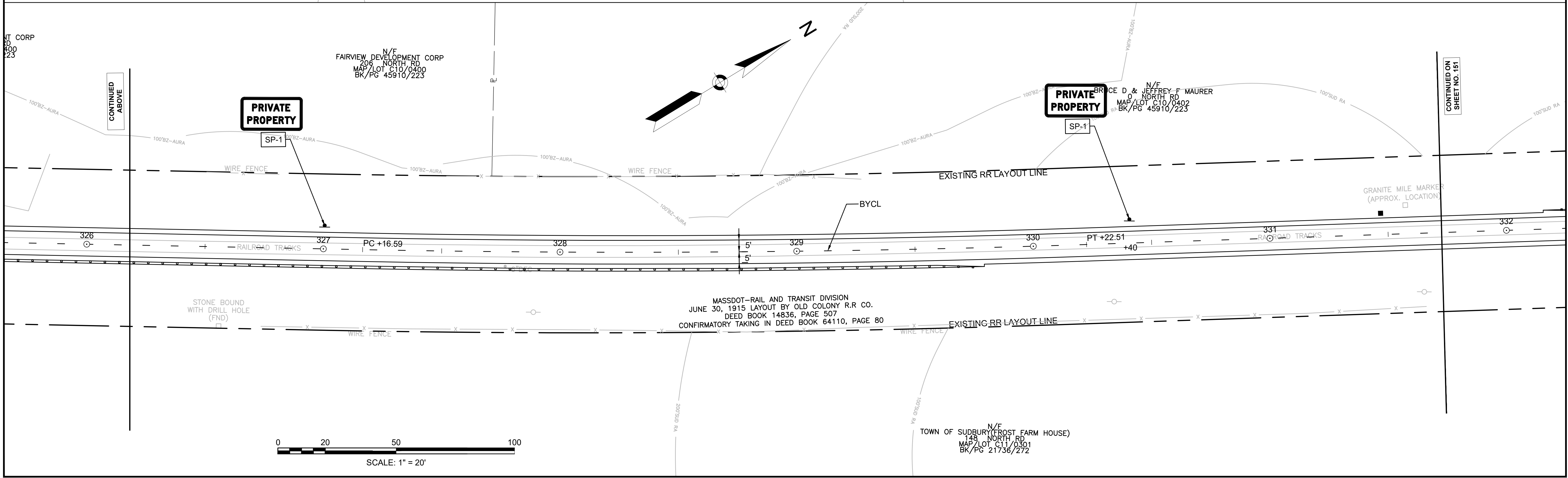
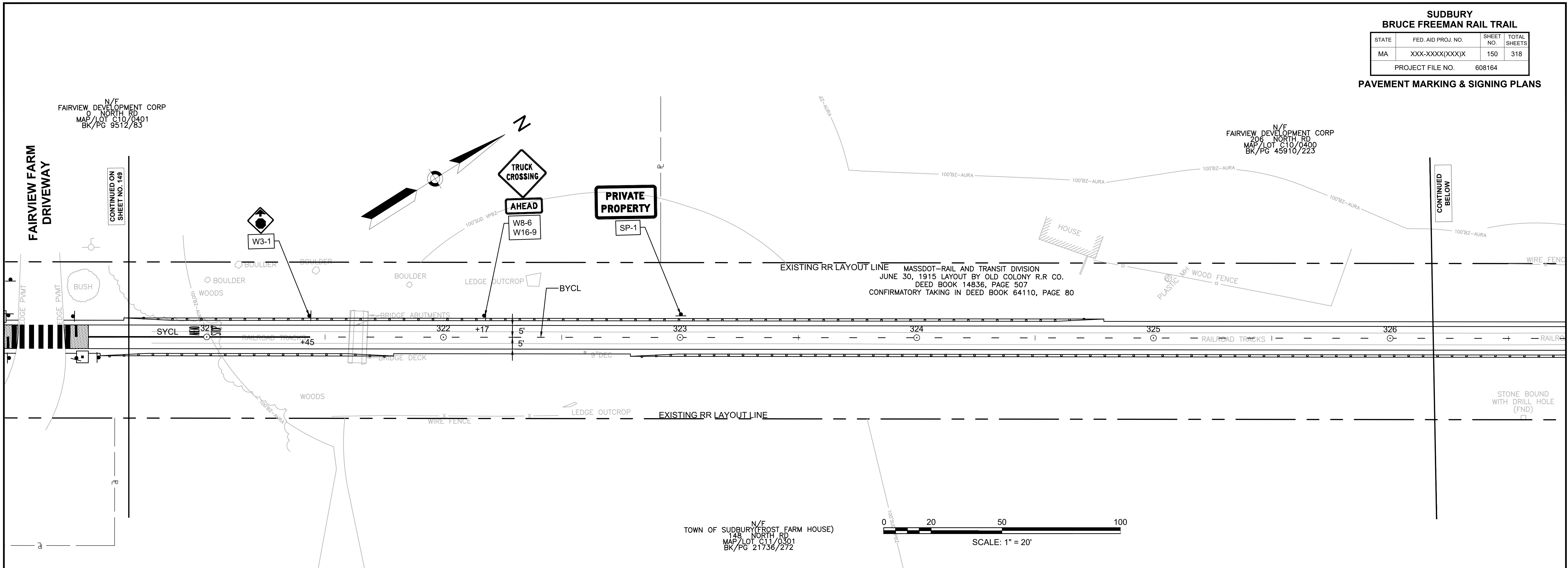


**SUDBURY  
BRUCE FREEMAN RAIL TRAIL**

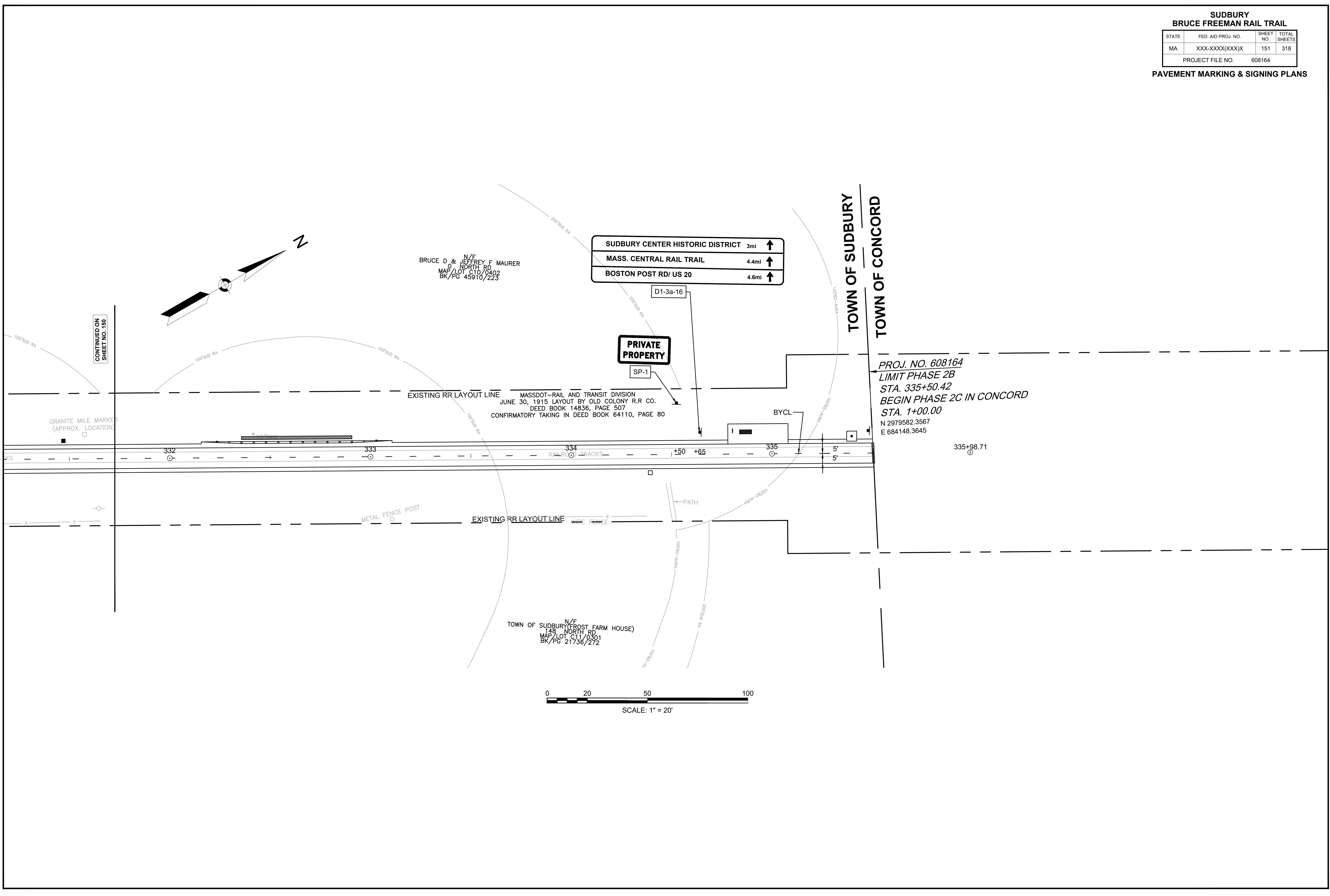
STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
MA	XXX-XXXX(XXX)X	150	318

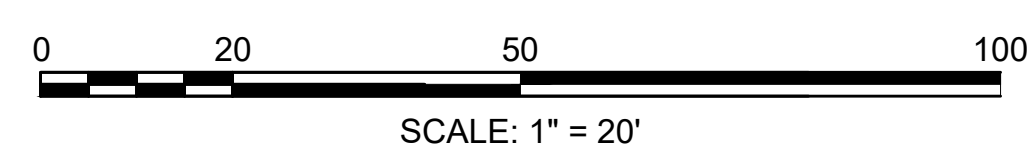
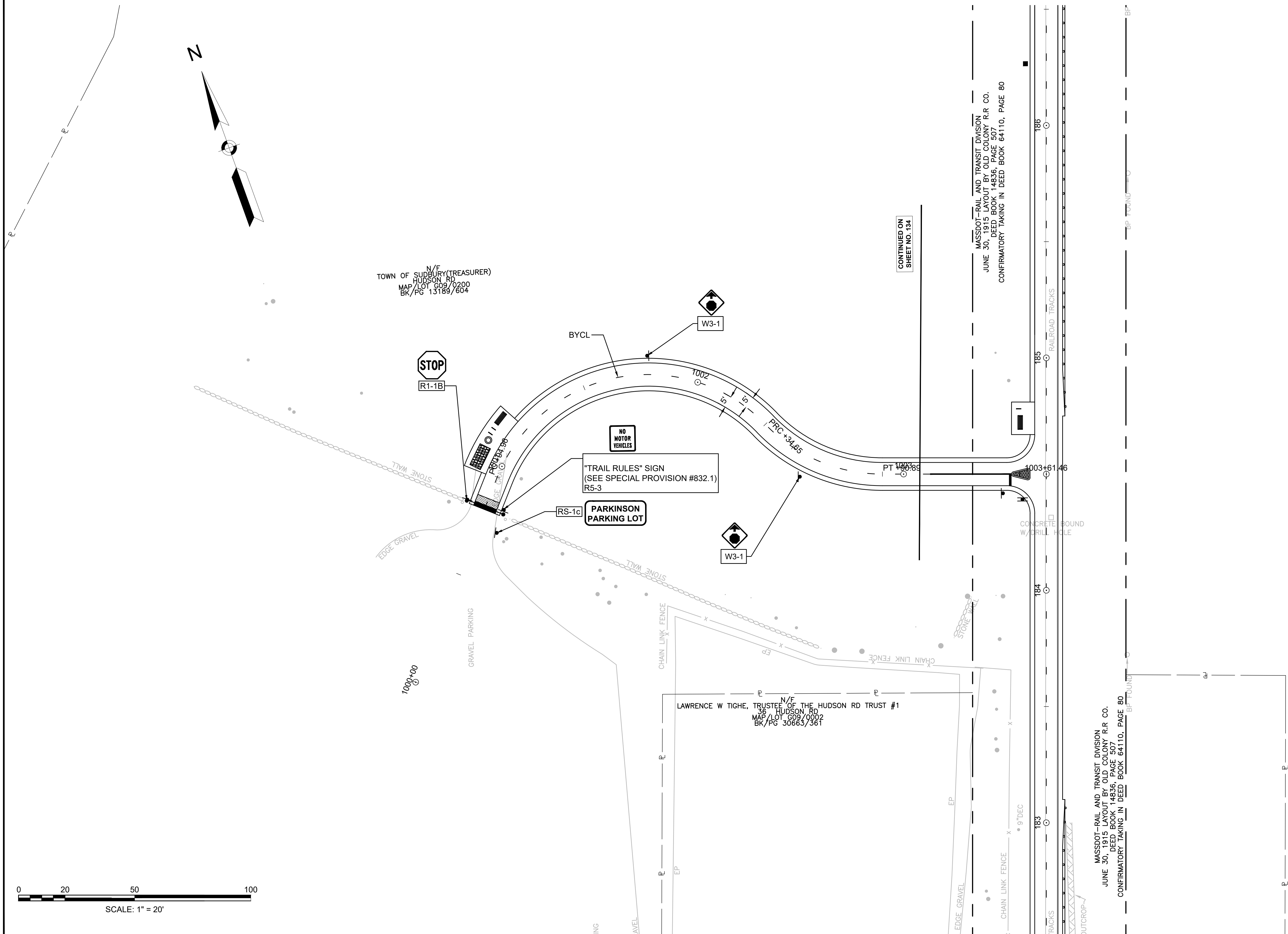
PROJECT FILE NO. 608164

**PAVEMENT MARKING & SIGNING PLANS**



608164\_HD(PAVEMENT MARKING & SIGNING PLANS).DWG Plotted on 12-May-2021 11:02 AM





CONTINUED ON  
SHEET NO. 134

MASSDOT-RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80

MASSDOT-RAIL AND TRANSIT DIVISION  
JUNE 30, 1915 LAYOUT BY OLD COLONY R.R. CO.  
DEED BOOK 14836, PAGE 507  
CONFIRMATORY TAKING IN DEED BOOK 64110, PAGE 80



